



City of Santa Clara

Meeting Agenda

Planning Commission

Wednesday, March 9, 2022

6:00 PM

Virtual Meeting

Pursuant to California Government Code section 54953(e) and City of Santa Clara Resolution 22-9051, the Planning Commission meeting will be held by teleconference only. No physical location will be available for this meeting; however, the City of Santa Clara continues to provide methods for the public to participate remotely:

- **Via Zoom:**

- o <https://santaclaraca.zoom.us/j/91729202898>

- o Webinar ID: 917 2920 2898 or

- o Phone: 1(669) 900-6833

- **Via the City's eComment (available during the meeting)**

The public may view the meetings on SantaClaraCA.gov, Santa Clara City Television (Comcast cable channel 15 or AT&T U-verse channel 99), or the livestream on the City's YouTube channel or Facebook page.

Public Comments prior to meeting may be submitted via email to PlanningPublicComment@SantaClaraCA.gov no later than noon on the day of the meeting; and also before and during the meeting via eComment. Clearly indicate the project address, meeting body, and meeting date in the email.

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- Mute all other audio before speaking. Using multiple devices can cause an audio feedback.
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- Unmute when called on to speak and mute when done speaking. If there is background noise coming from a participant, they will be muted by the host. Press *6 if you are participating by phone to unmute.
- If you no longer wish to stay in the meeting once your item has been heard, please exit the meeting.

6:00 PM REGULAR MEETING

Call to Order

Pledge of Allegiance and Statement of Values

Roll Call

DECLARATION OF COMMISSION PROCEDURES

CONTINUANCES/EXCEPTIONS

- 22-371** [Action to continue the public hearing for the Climate Action Plan, General Plan Amendment to adopt the updated Climate Action Plan 2022 and an Addendum to the General Plan Environmental Impact Report.](#)

Recommendation: Take action to open and continue the public hearing for the Climate Action Plan, General Plan Amendment to adopt the updated Climate Action Plan 2022 and Addendum to the General Plan Environmental Impact Report to April 13, 2022.

CONSENT CALENDAR

Consent Calendar items may be enacted, approved or adopted, based upon the findings prepared and provided in the written staff report, by one motion unless requested to be removed by anyone for discussion or explanation. If any member of the Planning Commission, staff, the applicant or a member of the public wishes to comment on a Consent Calendar item, or would like the item to be heard on the regular agenda, please notify Planning staff, or request this action at the Planning Commission meeting when the Chair calls for these requests during the Consent Calendar review. Items listed on the Consent Calendar with associated file numbers constitute Public Hearing items.

1.A 22-321 [Planning Commission Meeting Minutes of February 16, 2022 Meeting](#)

Recommendation: Approve the Planning Commission Minutes of the February 16, 2022 Meeting

1.B 22-130 [Action on a Vesting Tentative Subdivision Map for 1530 and 1540 Pomeroy Avenue](#)

Recommendation: Adopt a resolution to recommend the City Council approve the Vesting Tentative Subdivision Map to subdivide the two residential parcels into eight condominium lots and a common parcel at the property located at 1530 and 1540 Pomeroy Avenue.

PUBLIC PRESENTATIONS

Members of the public may briefly address the Commission on any item not on the agenda.

PUBLIC HEARING

Items listed above under Items for Council Action will be scheduled for Council review following the conclusion of hearings and recommendations by the Planning Commission. Due to timing of notices for Council hearings and the preparation of Council agenda reports, these items will not necessarily be heard on the date the minutes from this meeting are forwarded to the Council. Please contact the Planning Division office for information on the schedule of hearings for these items.

2. 22-374 [Study Session: Climate Action Plan \(CAP\) Update](#)

REPORTS OF COMMISSION/BOARD LIAISON AND COMMITTEE:

1. Announcements/Other Items
2. Commissioner Travel and Training Reports, Requests to attend Trainings

DIRECTOR OF COMMUNITY DEVELOPMENT REPORTS:

1. Planning Commission Budget Update
2. Upcoming Agenda Items

3. City Council Actions

ADJOURNMENT:

The next regular scheduled meeting is on April 13, 2022

The time limit within which to commence any lawsuit or legal challenge to any quasi-adjudicative decision made by the City is governed by Section 1094.6 of the Code of Civil Procedure, unless a shorter limitation period is specified by any other provision. Under Section 1094.6, any lawsuit or legal challenge to any quasi-adjudicative decision made by the City must be filed no later than the 90th day following the date on which such decision becomes final. Any lawsuit or legal challenge, which is not filed within that 90-day period, will be barred. If a person wishes to challenge the nature of the above section in court, they may be limited to raising only those issues they or someone else raised at the meeting described in this notice, or in written correspondence delivered to the City of Santa Clara, at or prior to the meeting. In addition, judicial challenge may be limited or barred where the interested party has not sought and exhausted all available administrative remedies.

If a member of the public submits a speaker card for any agenda items, their name will appear in the Minutes. If no speaker card is submitted, the Minutes will reflect "Public Speaker."

In accordance with the requirements of Title II of the Americans with Disabilities Act of 1990 ("ADA"), the City of Santa Clara will not discriminate against qualified individuals with disabilities on the basis of disability in its services, programs, or activities, and will ensure that all existing facilities will be made accessible to the maximum extent feasible. The City of Santa Clara will generally, upon request, provide appropriate aids and services leading to effective communication for qualified persons with disabilities including those with speech, hearing, or vision impairments so they can participate equally in the City's programs, services, and activities. The City of Santa Clara will make all reasonable modifications to policies and programs to ensure that people with disabilities have an equal opportunity to enjoy all of its programs, services, and activities.

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Agenda Report

22-371

Agenda Date: 3/9/2022

REPORT TO PLANNING COMMISSION

SUBJECT

Action to continue the public hearing for the Climate Action Plan, General Plan Amendment to adopt the updated Climate Action Plan 2022 and an Addendum to the General Plan Environmental Impact Report.

BACKGROUND

The Public Hearing item for the review and recommendation on the adoption of the Climate Action Plan, General Plan Amendment to adopt the updated Climate Action Plan 2022 and an Addendum to the General Plan Environmental Impact Report was noticed to be heard by the Planning Commission on March 9, 2022, but will not be considered for action on that day.

Notice of this item being heard by the Planning Commission on March 9, 2022 was made by publishing a hearing notice in the Santa Clara Weekly on February 23, 2022.

DISCUSSION

After notice of this hearing on the Climate Action Plan was made, it was determined that since the Climate Action Plan is adopted as part of the City's General Plan it is subject to Senate Bill 18 (SB 18) (2004), which obligates the City to consult with California Native American tribes for the purpose of identifying, protecting, and preserving Native American places, features, and objects.

To allow time for the required tribal notification to occur prior to the public hearing, staff is now requesting that the Planning Commission continue this item to April 13, 2022.

ENVIRONMENTAL REVIEW

The action being considered does not constitute a "project" within the meaning of the California Environmental Quality Act ("CEQA") pursuant to CEQA Guidelines Section 15378(a) as it has no potential for resulting in either a direct physical change in the environment, or a reasonably foreseeable indirect physical change in the environment.

FISCAL IMPACT

There is no impact to the City for preparing this report.

PUBLIC CONTACT

Public contact was made by posting the Council agenda on the City's official-notice bulletin board outside City Hall Council Chambers. A complete agenda packet is available on the City's website and in the City Clerk's Office at least 72 hours prior to a Regular Meeting and 24 hours prior to a Special Meeting. A hard copy of any agenda report may be requested by contacting the City Clerk's Office at (408) 615-2220, email clerk@santaclaraca.gov.

RECOMMENDATION

Take action to open and continue the public hearing for the Climate Action Plan, General Plan Amendment to adopt the updated Climate Action Plan 2022 and Addendum to the General Plan Environmental Impact Report to April 13, 2022.

Reviewed by: Alexander Abbe, Assistant City Attorney

Approved by: Lesley Xavier, Planning Manager



City of Santa Clara

1500 Warburton Avenue
Santa Clara, CA 95050
santaclaraca.gov
@SantaClaraCity

Agenda Report

22-321

Agenda Date: 3/9/2022

REPORT TO PLANNING COMMISSION

SUBJECT

Planning Commission Meeting Minutes of February 16, 2022 Meeting

RECOMMENDATION

Approve the Planning Commission Minutes of the February 16, 2022 Meeting



City of Santa Clara

Meeting Minutes

Planning Commission

02/16/2022

6:00 PM

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6:00 PM REGULAR MEETING

Call to Order

Chair Biagini called the meeting to order at 6:01 p.m.

Pledge of Allegiance and Statement of Values

Roll Call

Chair Biagini announced that **Commissioner Cherukuru** would be joining a few minutes late and that **Commissioner Ikezi** would be joining the meeting late due to work commitments. **Commissioner Cherukuru** joined the meeting at 6:06 p.m. and **Commissioner Ikezi** joined at 6:56 p.m.

Present 5 - Chair Nancy A. Biagini, Commissioner Ricci Herro, Commissioner Qian Huang, Commissioner Lance Saleme, and Commissioner Yashraj Bhatnagar

Absent 2 - Vice Chair Priya Cherukuru, and Commissioner Yuki Ikezi

DECLARATION OF COMMISSION PROCEDURES

Secretary Herro read the Declaration of Commission Procedures.

CONTINUANCES/EXCEPTIONS

None.

CONSENT CALENDAR

A motion was made by Commissioner Saleme, seconded by Commissioner Huang to approve the consent calendar.

Aye: 6 - Chair Biagini, Vice Chair Cherukuru, Commissioner Herro, Commissioner Huang, Commissioner Saleme, and Commissioner Bhatnagar

Absent: 1 - Commissioner Ikezi

1.A [22-261](#) Planning Commission Meeting Minutes of January 26, 2022 Meeting

Recommendation: Approve the Planning Commission Minutes of the January 26, 2022 Meeting

PUBLIC PRESENTATIONS

None.

PUBLIC HEARING - 1 Hour

2. Discussion of Land Use Topics to Recommend to Council - Topics may include: electrification standards, increased density, architectural review, senior housing, homelessness, historic preservation, meeting procedures, and interaction with other commissions

Chair Biagini, Commissioner Saleme, and Commissioner Bhatnagar submitted a list of topics for this item and a discussion took place on their submitted items.

Commissioners discussed long-term planning goals and discussed that if the items are still of concern at the conclusion of 2022, the Planning Commission will submit them to the Council for consideration during the Council's 2023 goal-setting session.

Topics discussed included:

- The impact of ADU's and SB9 on single family homes possibly resulting in a strain on the City's infrastructure, specifically electricity.
- Creation of Objective Development Standards
- Communication between Planning Commission and Other Government Bodies
- Dealing with Post-Entitlement Requests from Developers to Make Changes to Conditions of Approval
- Return to In-Person Planning Commission Meetings - staff commented that this direction to return to in person meeting would take place via City Council direction
- Request for Speakers from Other Cities who are dealing with similar issues as the City of Santa Clara
- Transportation Demand Measures (TDM) - a request to have this information available on the City's website

Assistant Director of Community Development Reena Brilliot stated she would contact Silicon Valley Power for a possible upcoming Study Session

The meeting went into recess at 7:26 p.m. and reconvened at 7:36 p.m.

Public Speaker: Patricia Leung spoke in favor of the creation of Objective Development Standards and continuity of knowledge

3. Ad Hoc Committee Discussion: Ideas for topics that committees would cover, responses to process questions about creating and running committees, and discussion of when the use of a committee would be appropriate

Assistant City Attorney Alexander Abbe provided clarification on the difference between a standing committee versus and ad-hoc committee. There was no action taken by the Commission to form an ad-hoc committee.

REPORTS OF COMMISSION/BOARD LIAISON AND COMMITTEE:

1. Announcements/Other Items

Assistant Director of Community Development Reena Brilliot announced that **Planning Manager Lesley Xavier** would be assisting with Planning Commission items and that **Associate Planner Jeff Schwilk** will be retiring in early March. (The Commission congratulated the newly promoted staff members and thanked Jeff for his work.)

2. Commissioner Travel and Training Reports, Requests to attend Trainings

Commissioners were reminded that each of them had been registered for the State of the Valley Conference taking place on Friday, February 18, 2022.

It was announced that the APA California Conference is taking place in San Diego, April 30 - May 3, 2022 and that staff would provide detailed information to the Commission on this conference.

DIRECTOR OF COMMUNITY DEVELOPMENT REPORTS:

1. Planning Commission Budget Update

Office Specialist IV Elizabeth Elliott shared that there were no updates to the budget. **Assistant Director of Community Development Reena Brilliot** asked the Commission to notify staff of any trainings they would like to take place before the end of the fiscal year at the next Planning Commission meeting. (discussion of using current budget's funds for next conference in fall)

2. Upcoming Agenda Items

Assistant Director of Community Development Reena Brilliot provided updates.

3. City Council Actions

Assistant Director of Community Development Reena Brilliot provided updates.

ADJOURNMENT:

A motion was made by Commissioner Bhatnagar, seconded by Commissioner Saleme to adjourn the meeting.

The meeting adjourned at 9:18 p.m.

The next regular scheduled meeting is on Wednesday, March 9, 2022.

Aye: 7 - Chair Biagini, Vice Chair Cherukuru, Commissioner Herro, Commissioner Huang, Commissioner Ikezi, Commissioner Saleme, and Commissioner Bhatnagar

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Agenda Report

22-130

Agenda Date: 3/9/2022

REPORT TO PLANNING COMMISSION

SUBJECT

Action on a Vesting Tentative Subdivision Map for 1530 and 1540 Pomeroy Avenue

BACKGROUND

The Applicant, Ridgecrest Group Inc., is requesting approval of a Vesting Tentative Subdivision Map (PLN21-15320) to subdivide two residential parcels with a total lot area of approximately 21,000 square feet into eight condominium lots and a common parcel at 1530 and 1540 Pomeroy Avenue for the development of eight condominium units. The project site is located on the west side of Pomeroy Avenue between El Camino Real and Granada Ave. The project would construct two detached single-family residences on the southern parcel (1530 Pomeroy Avenue) that borders single-family residences to the west and to the south. The parcel on the north (1540 Pomeroy Avenue) that borders commercial uses to the north and the west, would be occupied with six, three-story attached townhomes.

Currently, the property contains two parcels. The parcel at 1530 Pomeroy Avenue is zoned Low Density Multiple Dwelling (R3-18D) and has a General Plan land use designation of Very Low Density Residential (up to 10 dwelling units per acre). The proposed two single-family homes are permitted uses per the zoning code and would result in a density of 8.7 du/acre based on the lot size of 10,080 square feet (0.23 acre) and is therefore, consistent with density of up to 10 units per acre allowed for the Very Low Density Residential land use designation.

For the parcel at 1540 Pomeroy Avenue, the project utilizes Assembly Bill (AB) 3194 (2019), which prohibits a local agency from requiring rezoning if a proposed project is consistent with the General Plan, but the zoning for the project site is inconsistent with the General Plan. Further, a local agency may require the proposed housing development project to comply with the objective standards and criteria of the zoning code that is consistent with the General Plan; however, the standards and criteria are to be applied to facilitate and accommodate development at the density allowed on the site by the General Plan and proposed by the housing development project.

For 1540 Pomeroy Avenue, the zoning is Agricultural (A) and the General Plan land use designation is Community Mixed Use (CMU). Pursuant to AB 3194, the project will employ the objective standards of the Medium Density Multiple Dwelling (R3-36D) zoning district to construct six attached townhomes, which is a permitted use in this zoning district. The prescribed density range for Community Mixed Use designation of 20-36 du/acre would allow construction of 5-9 units on this site. Based on the lot size of 10,920 square feet (0.25 acres) the density for the proposal to construct the six units is 24 du/acre and within the allowable density range.

The Architectural Review (PLN2019-13924) for the proposed six, three-story townhomes and two,

detached two-story single-family residences was approved at the Development Review Hearing on August 18, 2021, finding the project consistent with the applicable requirements of Santa Clara City Code (SCCC) chapters 18.16 (R3-18D) and 18.20 (R3-36D). Planning Commission review, and a recommendation to the City Council, is required under (SCCC) section 17.05.300(g) for Subdivision Map applications.

DISCUSSION

Vesting Tentative Subdivision Map applications are reviewed for General Plan and Zoning Code consistency and conformance with the Subdivision Map Act. The proposed subdivision is consistent with the approved site plan, improvements, private street, surface parking, landscape open space areas, and associated utilities on the project site for the development of eight residential units with two detached single-family residences on 1530 Pomeroy Avenue and six attached townhomes. The proposed Vesting Tentative Subdivision Map was reviewed by the City's Subdivision Clearance Committee and determined to be complete on January 11, 2022. Staff has confirmed that the proposed subdivision will not result in any inconsistencies with the General Plan and zoning designations for the property, Building Code, or other applicable requirements. Conditions of Approval have been prepared and are provided as Attachment 3. The conditions include a requirement for recordation of Covenants, Conditions and Restrictions (CC&R's) governing the shared use and maintenance of building structures and private on-site improvements (e.g. private street, surface parking, utilities, landscaping, fencing) as well public right-off-way improvements (complete street landscaping).

ENVIRONMENTAL REVIEW

A Mitigated Negative Declaration (MND) was prepared for the project in accordance with the California Environmental Quality Act (CEQA), for the previously approved 8-unit residential development. The MND and Mitigation Monitoring and Reporting Program were approved at the Development Review Hearing on August 18, 2021, at a publicly noticed meeting. The proposed Vesting Tentative Subdivision Map is consistent with the approved project and would not result in a new impact on the environment or significantly increase the severity of any previously identified impact.

FISCAL IMPACT

There is no cost to the City other than administrative staff time and expense for the processing of this application.

COORDINATION

This report has been coordinated with the City Attorney's Office.

PUBLIC CONTACT

On February 23, 2022, a notice of public hearing was published in *The Weekly*, a newspaper of general circulation, and on February 24, 2022, a notice of this item was posted in three conspicuous locations within 300 feet of the project site and mailed to property owners within 500 feet of the project site. At the time of this report, the Planning staff has not received any public comments regarding the proposed project.

Public contact was also made by posting the Commission agenda on the City's official-notice bulletin board outside City Hall Council Chambers. A complete agenda packet is available on the City's website and in the City Clerk's Office at least 72 hours prior to a Regular Meeting and 24

hours prior to a Special Meeting. A hard copy of any agenda report may be requested by contacting the City Clerk's Office at (408) 615-2220, email clerk@santaclaraca.gov or at the public information desk at any City of Santa Clara public library.

RECOMMENDATION

Adopt a resolution to recommend the City Council approve the Vesting Tentative Subdivision Map to subdivide the two residential parcels into eight condominium lots and a common parcel at the property located at 1530 and 1540 Pomeroy Avenue.

Reviewed by: Alexander Abbe, Assistant City Attorney

Approved by: Lesley Xavier, Planning Manager

ATTACHMENTS

1. Project Data Sheet
2. Resolution Recommending Council Approval of the Vesting Tentative Subdivision Map
3. Conditions of Vesting Tentative Subdivision Map Approval
4. DRH Staff Report 08.18.2021
5. Vesting Tentative Subdivision Map

Project Data

File No.(s)
Location

PLN21-15320

1530 and 1540 Pomeroy Ave; two parcels with a total lot area of 0.48 acres, located on the west side of Pomeroy Avenue between El Camino Real and Granada Ave; APN: 290-02-096, 290-02-097; Property at 1530 Pomeroy is zoned Low Density Multiple Dwelling (R3-18D) and 1540 Pomeroy Avenue zoned Agricultural (A).

Applicant:

The Ridgecrest Group Inc./Omid Shakeri

Owner:

Masud Maesumi and Valm Pascual

CEQA Determination:

Adopted Mitigated Negative Declaration

Project Planner:

Nimisha Agrawal, Associate Planner

	Existing	Proposed	Zoning/GP Consistency
General Plan Designation			
1530 Pomeroy	Very Low Density Residential	No change	
1540 Pomeroy	Community Mixed Use		
Zoning Designation			
1530 Pomeroy	Low Density Multiple Dwelling (R3-18D)	No change	Consistent with R3-18D
1540 Pomeroy	Agricultural (A)	No change	Consistent with R3-36D
Land Use	Residential	No change	Residential
Lot Size (Sq. ft.) Total 0.48 acres (21,000 sq. ft.)			
1530 Pomeroy	10,080 sq. ft. (.23 acres)	8 Condominium lot sand one shared lot	Meets minimum lot size requirement of 8,000 sq. ft.
1540 Pomeroy	10,920 sq. ft. (.25 acres)		Meets minimum lot size requirement of 8,5000 sq. ft.
Building Square Footage	Total 3,412 sq. ft.	Total 19,390 sq. ft.	
1530 Pomeroy	1,706 sq. ft.	5,170 sq. ft.	Lot coverage 28.5% (max 35%)
1540 Pomeroy	1,706 sq. ft.	14,220 sq. ft.	Lot coverage 45% (max 45%)
Residential Units			
1530 Pomeroy	1	2 Detached Single Family Homes	Density: 8.7 du/acre (allowed up to 10 du/acre)
1540 Pomeroy	1	6 Attached Townhomes	Density: 24 du/acre (allowed 20-36 du/acre)

Aerial Map



Project Data -1530 and 1540 Pomeroy Avenue- Tentative Map (PLN21-15320), March 9, 2022

General Plan Map



**Community
Mixed Use**

**Very Low
Density
Residential**

Zoning Map



Agricultural (A)

**Low Density
Multiple Dwelling
Residential (R3-
18D)**

Project Data -1530 and 1540 Pomeroy Avenue- Tentative Map (PLN21-15320), March 9, 2022

RESOLUTION NO. _____

A RESOLUTION OF THE PLANNING COMMISSION OF THE CITY OF SANTA CLARA, CALIFORNIA RECOMMENDING THAT THE CITY COUNCIL APPROVE A VESTING TENTATIVE SUBDIVISION MAP (PLN21-15320) AT 1530 and 1540 POMEROY AVENUE, SANTA CLARA

BE IT RESOLVED BY THE PLANNING COMMISSION OF THE CITY OF SANTA CLARA AS FOLLOWS:

WHEREAS, on November 15, 2021, Ridgecrest Group Inc. (“Applicant”) on behalf of Masud Maesumi and Valm Pascual (“Property Owners”) filed an application (PLN21-15320) to subdivide two existing parcels with a total lot area of 0.48 acres, located at 1530 and 1540 Pomeroy Avenue (“Project Site”) into condominium lots in accordance with the development plan (PLN2019-13924) approved for the Project Site;

WHEREAS, on August 18, 2021, the Architectural Review (PLN2019-13924) to construct six townhomes and two single-family homes utilizing Assembly Bill (AB) 3194 (2019) was approved at the Development Review Hearing (DRH);

WHEREAS, the application is for a Vesting Tentative Subdivision Map (PLN21-15320) to subdivide the two residential parcels into eight condominium lots and a common parcel to serve the development;

WHEREAS, the proposed subdivision is consistent with the approved site plan and improvements on the Project Site for the development of 8 residential units in six townhomes and two single-family homes arrangement with surface parking, landscaped open space areas, and associated utilities as entitled on August 18, 2021;

WHEREAS, pursuant to Section 17.05.210 of the Code of the City of Santa Clara (“SCCC”), a Tentative Subdivision Map shall be required for all divisions of land into five or more parcels;

WHEREAS, on January 11, 2022, the Subdivision Clearance Committee determined that the application was complete and that the proposed Vesting Tentative Subdivision Map should proceed to the Planning Commission in conformance with Section 17.05.300 of the SCCC;

WHEREAS, SCCC Section 17.05.300(g) requires that the Planning Commission make recommendations of denial, approval, or conditional approval to the City Council on the Tentative Subdivision Map;

WHEREAS, the proposal is to create eight condominium lots for eight residential units and one common lot to serve the development (“Project”) as shown on the proposed Vesting Tentative Subdivision Map, attached hereto and incorporated herein by this reference;

WHEREAS, a Mitigated Negative Declaration (“MND”) and Mitigation Monitoring and Reporting Program were prepared in accordance with the California Environmental Quality Act and adopted at the Development Review Hearing on August 18, 2021, for the Project;

WHEREAS, the proposed Vesting Tentative Subdivision Map is consistent with the previously approved Project and would not result in a new impact on the environment or significantly increase the severity of any previously identified impact;

WHEREAS, on February 24, 2022, notices of the public hearing on the Vesting Tentative Subdivision Map were posted within 300 feet of the Project Site and mailed to all property owners within 500 feet of the property, according to the most recent Assessor’s roll;

WHEREAS, a notice of the public hearing was published in *The Weekly*, a newspaper of general circulation, on February 23, 2022, for a meeting to be conducted on March 9, 2022; and

WHEREAS, the Planning Commission held a duly noticed public hearing on March 9, 2022, at which time all interested persons were given an opportunity to provide testimony and the Commission considered the information presented in the Staff Report, and all verbal and written evidence.

NOW THEREFORE, BE IT FURTHER RESOLVED BY THE PLANNING COMMISSION OF THE CITY OF SANTA CLARA AS FOLLOWS:

1. That the Planning Commission hereby finds that the above Recitals are true and correct and by this reference makes them a part hereof.

2. Vesting Tentative Subdivision Map Findings. Pursuant to California Government Code Sections 66426 and 66428 and SCCC Section 17.05.300(h), the Planning Commission finds and determines that:

A. The Vesting Tentative Subdivision Map is consistent with the objectives, policies, general land uses and programs specified in the City's General Plan in that the Vesting Tentative Subdivision Map subdivides the existing 0.48 acre Project Site into eight condominium lots and one common lot to effectuate the previously approved residential development of six townhome units and two single-family homes, on-site resident and visitor parking, and common landscaped areas with shared maintenance of the building and public and private site improvements compatible with neighboring low intensity residential development and the commercial uses to the north of the project site, subject to conditions set forth in the Conditions of Vesting Tentative Subdivision Map Approval, attached hereto and incorporated by this reference.

B. The design and improvements of the proposed subdivision are consistent with the City's General Plan in that the Vesting Tentative Subdivision Map facilitates development of a diversified housing stock and construction of ownership housing opportunities for the community; and furthermore complies with General Plan Land Use and Transitional Goals and Polices of the General Plan in that it transforms the underutilized properties from a vacant, low-intensity commercial use to a medium density residential development that would transition in scale and intensity of use with existing and planned land uses; and is in proximity to transit with multi-modal connections to local and regional transit service to support transit ridership and reduce vehicle miles traveled.

C. The site is physically suitable for the proposed type of development, in that the Project provides a transition between low intensity residential development and the existing commercial uses along the El Camino Real.

D. The site is physically suitable for the proposed density of development, in that the Project Site is in an urbanized area and is served by existing utilities and infrastructure.

E. The design of the subdivision and type of improvements are not likely to cause serious health problems, in that the proposed residential subdivision will implement Covenants Conditions and Restrictions for operation and maintenance of the building and site improvements and does not propose the use of hazardous materials.

F. The design of the subdivision and type of improvements are not likely to cause substantial environmental damage and will not substantially or unavoidably injure fish or wildlife or their habitat in that the Project Site is located in an urbanized setting, is a developed site, and the proposed subdivision includes mitigation measures, as identified in the MND and MMRP, that reduces impacts to biological resources to less-than-significant levels.

G. The design of the subdivision and type of improvements will not conflict with easements acquired by the public at large or use of property within the proposed subdivision in that it is designed to avoid encroachment and conflicts with public easements in the site design.

H. The Tentative Subdivision Map provides, to the extent feasible, for future passive or natural heating or cooling opportunities, in that it would allow flexibility in the development standards to maximize the benefits of green building standards for site and building design.

3. Based on the findings set forth in this Resolution and the evidence in the Staff Report and such other evidence as received at the public hearings on this matter before the Planning Commission, the Planning Commission hereby recommends approval of the Vesting Tentative Subdivision Map to the City Council, substantially in the form on file as shown on the attached Vesting Tentative Subdivision Map and Conditions of Vesting Tentative Subdivision Map Approval, hereby incorporated by this reference.

3. Effective date. This resolution shall become effective immediately.

I HEREBY CERTIFY THE FOREGOING TO BE A TRUE COPY OF A RESOLUTION PASSED AND ADOPTED BY THE PLANNING COMMISSION OF THE CITY OF SANTA CLARA, CALIFORNIA, AT A REGULAR MEETING THEREOF HELD ON THE 9TH DAY OF MARCH 2022, BY THE FOLLOWING VOTE:

AYES: COMMISSIONERS:

NOES: COMMISSIONERS:

ABSENT: COMMISSIONERS:

ABSTAINED: COMMISSIONERS:

ATTEST: _____
ANDREW CRABTREE
DIRECTOR OF COMMUNITY DEVELOPMENT
CITY OF SANTA CLARA

Attachments Incorporated by Reference:

1. Vesting Tentative Subdivision Map
2. Conditions of Vesting Tentative Subdivision Map Approval

CONDITIONS OF APPROVAL

In addition to complying with all applicable codes, regulations, ordinances and resolutions, the following **conditions of approval** are recommended:

GENERAL

- A. If relocation of an existing public facility becomes necessary due to a conflict with the developer's new improvements, then the cost of said relocation shall be borne by the developer.
- B. Comply with all applicable codes, regulations, ordinances and resolutions.

ATTORNEY'S OFFICE

- A. The Developer agrees to defend and indemnify and hold City, its officers, agents, employees, officials and representatives free and harmless from and against any and all claims, losses, damages, attorneys' fees, injuries, costs, and liabilities arising from any suit for damages or for equitable or injunctive relief which is filed by a third party against the City by reason of its approval of developer's project.

COMMUNITY DEVELOPMENT

BUILDING DIVISION

- BD1. Informational: Prior to overall construction permit application, submit to the Santa Clara Building Division, 2 copies of an addressing diagram request, to be prepared by a licensed architect or engineer. The addressing diagram(s) shall include all proposed streets and all building floor plans. The addressing diagram(s) shall conform to Santa Clara City Manager Directive #5; Street Name and Building Number Changes, and Santa Clara Building Division Address Policy For Residential and Commercial Developments. The addressing diagram(s) shall indicate all unit numbers to be based off established streets, not alleys nor access-ways to garages. Allow a minimum of 10 working days for initial staff review. Please note city staff policy that existing site addresses typically are retired. Provide digital pdf printed from design software, not scanned from printed paper sheet.
- BD2. Informational: The construction permit application drawings submitted to the Santa Clara Building Division shall include a copy of the latest Federal Emergency Management Agency (FEMA) Flood Zone Map: <https://msc.fema.gov/portal/home>. The project drawings shall indicate how the project complies with the Santa Clara Flood Damage Prevention Code.
- BD3. Informational: The construction permit application drawings submitted to the Santa Clara Building Division shall include Santa Clara Valley Urban Runoff Pollution Prevention Program Low Impact Development (LID) practices http://www.scvurppp-w2k.com/nd_wp.shtml. All projects that disturb more than one acre, or projects that are part of a larger development that in total disturbs more than one acre, shall comply with the Santa Clara Valley Urban Runoff Pollution Prevention Program Best Management Practices (BMP): http://www.scvurppp-w2k.com/construction_bmp.shtml, and shall provide a Storm Water Pollution Prevention Plan (SWPPP) by a certified Qualified SWPPP Developer (QSD). All site drainage and grading permit applications submitted to the Santa Clara Building Division shall include a city of Santa Clara "C3" data form, available on this web page: <https://www.santaclaraca.gov/our-city/departments-g-z/public-works/environmental-programs/stormwater-pollution-prevention> and will be routed to a contract consultant for review.
- BD4. Informational: no California construction code review is being done at this time. The construction permit application drawings submitted to the Santa Clara Building Division shall include an overall California Building Code analysis, including; proposed use and occupancy of all spaces (19' CBC Ch. 3), all building heights and areas (19' CBC Ch. 5), all proposed types of construction (19' CBC Ch. 6), all proposed fire and smoke protection features, including all types of all fire rated penetrations proposed (19' CBC Ch. 7), all proposed interior finishes fire resistance (19' CBC Ch. 8), all fire protection systems proposed (19' CBC Ch. 9), and all means of egress proposed (19' CBC Ch. 10). -Noncombustible exterior wall, floor, and roof finishes are strongly encouraged.

- During construction retaining a single company to install all fire rated penetrations is highly recommended.
- The grade level lobbies shall be min. 1 hour rated all sides and above.
- All stair shafts shall be min. 1 hour rated.
- All elevator shafts shall be min. 1 hour rated.
- All trash chute shafts shall be min. 1 hour rated.
- Recommendation: provide a minimum of two trash chutes; one for recyclables, one for trash, each trash chute to be routed down to a grade level trash collection room.
- Any trash rooms shall be min. 1 hour rated all sides and above.

BD5. Informational: The overall project construction permit application shall include the geotechnical, architectural, structural, energy, electrical, mechanical, and plumbing drawings and calculations. Prior to the issuance of the overall project construction permit, a conditions of approval review meeting must be held in city hall, which meeting must be attended by the on-site field superintendent(s). The meeting will not be held without the attendance of the on-site field superintendent(s). The on-site grading permit shall be a separate permit application to the Building Division.

BD6. Informational: Temporary Certificates of Occupancy will not be routinely issued, and will be considered on a very limited basis only when there is a clear and compelling reason for city staff to consider a TCO. A TCO will be approved only after all applicable City staff have approved in writing; Planning, P.W./ Engineering, Fire Prev., Santa Clara Water, Silicon Valley Power, and any other applicable agencies such as the Santa Clara County Health Dept., with the Building Division being the final approval of all TCO.'s.

HOUSING & COMMUNITY SERVICES DIVISION

H1. In accordance with the Santa Clara City Code chapter 17.40, this project is subject to the following affordable housing requirements and impact fee:

The requirement for the for-sale residential development which applies to the proposed 8 units is as follows:

(a) Unless the City Council approves an alternate method of compliance pursuant to section (b) below, the Applicant shall provide one dwelling unit affordable to households at 100% AMI or pay an in-lieu fee. The in-lieu fee shall be equal to the difference between the unrestricted appraised market value ("Initial Market Value") and the Affordable Sales Price of the last unit at completion. The Initial Market Value of the last unit sold shall be the basis for calculating the in-lieu fee to be paid by Applicant. Any in-lieu fee payment due to the City by the Applicant shall be paid prior to receipt of the occupancy certificate of the last unit at completion.

The Affordable Unit shall have prices set in accordance with the City's Below Market Purchase (BMP) Program Policies and Procedures Manual (subject to updates and changes). Prior to issuance of Building Permits, the Developer shall enter into an Affordable Housing Agreement with the City that will determine the Affordable Sales Price, identify the actual unit to be sold as the Affordable Unit, and apply all terms and covenants guaranteeing the prescribed affordability, to the satisfaction of the Director of Community Development. There is a fee for the AHA preparation in the amount of \$3,708 which will be due prior to execution of the AHA.

(b) In the alternative, the City Council may, in its sole discretion, authorize the Applicant to utilize an alternate means of compliance pursuant to SCCC § 17.40.080(g) through the execution of a development agreement. In order to utilize such an alternative, such Development Agreement must be fully executed prior to issuance of Building Permits. If no Development Agreement has been executed at the time Building Permits are issued, then section (a) above shall apply.

PLANNING DIVISION

P1. The project shall comply with the Conditions of Architectural Review approvals (PLN2019-13924) and the mitigation measures identified in the Mitigated Negative Declaration and adopted Mitigation Monitoring and Reporting Program for the project.

- P2. Developer shall prepare and submit Covenant, Conditions and Restrictions (CC&R\$s) to the City for review and approval. The approved CC&R\$s are to be recorded prior to the issuance of building permits and a copy is provided to the Planning Department.

PUBLIC WORKS

ENGINEERING

- E1. Obtain site clearance through Public Works Department prior to issuance of Building Permit. Site clearance will require payment of applicable development fees. Other requirements may be identified for compliance during the site clearance process. Contact Public Works Department at (408) 615-3000 for further information.
- E2. All work within the public right-of-way and/or public easement, which is to be performed by the Developer/Owner, the general contractor, and all subcontractors shall be included within a Single Encroachment Permit issued by the City Public Works Department. Issuance of the Encroachment Permit and payment of all appropriate fees shall be completed prior to commencement of work, and all work under the permit shall be completed prior to issuance of occupancy permit.
- E3. Submit public improvement plans prepared in accordance with City Public Works Department procedures which provide for the installation of public improvements. Plans shall be prepared by a Registered Civil Engineer and approved by the City Engineer prior to approval and recordation of parcel map and/or issuance of building permits.
- E4. Existing non-standard or non-ADA compliant frontage improvements shall be replaced with current City standard frontage improvements as directed by the City Engineer or his designee.
- E5. Dedicate, as required, on-site easements for new and existing utilities and sidewalk by Subdivision Map or approved instrument at time of development. Developer shall pay applicable easement preparation/processing fee.
- E6. Dedicate a sidewalk easement for the sidewalk and driveway portions within private property and pay the easement preparation fee. Sidewalk easement shall be 1' behind proposed back-of-walk if there is landscaping behind sidewalk and/or at the proposed back-of-walk with a cold joint if there is hardscape concrete behind sidewalk.
- E7. After City Council approval of the Tentative Map, submit 10 copies of the Subdivision Map, prepared by a Licensed Land Surveyor or a Registered Civil Engineer with Land Surveyor privileges to the Engineering Department. The submittal shall include a title report, closure calculations, and all appropriate fees.

STREETS DIVISION

Landscape

- L1. Include City of Santa Clara Tree Preservation/City Arborist specifications on all improvement plans.
- L2. No cutting of any part of private trees, including roots, shall be done without securing prior approval of the City Arborist. Tree trimming/removal shall be done in accordance to the City of Santa Clara Tree Preservation/City Arborist specifications and with direct supervision of a certified arborist (Certification of International Society of Arboriculture).
- L3. Identified existing mature trees to be maintained. Prepare a tree protection plans for review and approval by the City of Santa Clara prior to any demolition, grading or other earthwork in the vicinity of existing trees on the site.

Stormwater

- ST1. Prior to City's issuance of Building or Grading Permits, the applicant shall develop a Final Stormwater Management Plan, update the [C.3 Data Form](#), and an Erosion and Sediment Control Plan.
- ST2. The Final Stormwater Management Plan and all associated calculations shall be reviewed and certified by a qualified 3rd party consultant from the [SCVURPPP List of Qualified Consultants](#), and a 3rd party review letter shall be submitted with the Plan.
- ST3. The applicant shall incorporate Best Management Practices (BMPs) into construction plans and incorporate post-construction water runoff measures into project plans in accordance with the City's Urban Runoff Pollution Prevention Program standards prior to the issuance of Building or Grading Permits. Include the [SCVURPPP Countywide Construction BMPs Plan Sheet](#) with the plans.

- ST4. Include [the C.3 Treatment Facilities Construction Notes](#) on the Improvement Plans and/or Stormwater Control Plans.
- ST5. During the construction phase, all stormwater control measures shall be inspected for conformance to approved plans by a qualified 3rd party consultant from the [SCVURPPP List of Qualified Consultants](#), and a 3rd party concurrence letter on the C.3 facilities construction shall be submitted to the Public Works Department. As-Built drawing shall be submitted to the Public Works Department. Include [C.3 Stormwater Treatment Facilities Construction general notes](#) on the improvement plans.
- ST6. **Permeable Pavement** shall be inspected by a third-party reviewer and/or manufacturer representative for conformance with the details and specifications. If necessary, percolation test shall be performed to ensure proper installation. The number, location and species of the interceptor trees shall be confirmed during the construction.
- ST7. Stormwater treatment facilities must be designed, installed, and maintained to achieve the site design measures throughout their life in accordance to the SCVURPPP C.3 Stormwater Handbook (Chapter 6 and Appendix C). They shall be installed using biotreatment soil media that meet the minimum specifications as set forth in this Handbook. If percolation rate test of the biotreatment soil mix is not performed on-site, a certification letter from the supplier verifying that the soil meets the specified mix.
- ST8. The property owner shall enter into an Operation and Maintenance (O&M) Agreement with the City for all installed stormwater treatment measures in perpetuity. Applicants should contact Karin Hickey at (408) 615-3097 or KaHickey@santaclaraca.gov for assistance completing the Agreement. For more information and to download the most recent version of the O&M Agreement, visit the City's stormwater resources website at <http://santaclaraca.gov/stormwater>. **For porous pavement, inspection of these facilities is to be done annually.**
- ST9. Any site design measures used to reduce the size of stormwater treatment measures shall not be installed for the project without the written approval from the City, installing the corresponding resizing of other stormwater treatment measures and an amendment of the property's O&M Agreement.
- ST10. Developer shall install an appropriate stormwater pollution prevention message such as "No Dumping – Flows to Bay" on any storm drains located on private property.
- ST11. Interior floor drains shall be plumbed to the sanitary sewer system and not connected to the City's storm drain system.
- ST12. Final C.3 inspection by Code Enforcement staff shall be completed after receipt of the 3rd party concurrence letter and installation of stormwater treatment facilities, planting materials and storm drain medallions.

WATER & SEWER

- W1. **Previous Approvals:** Permittee shall abide by and continue to comply with all previous City approvals, permits, or requirements relating to the subject property, unless explicitly superseded or revised by the Director of Water and Sewer Utilities.
- W2. **Utility Design:** The utility design plans shall reflect the following revisions to the utility design:
 - All potable water services including fire hydrant services shall connect to the existing 10" CIP on Pomeroy Avenue and not the existing 8" CIP main.
 - Locate all water meters and appurtenances in the landscaping strip adjacent to the curb or immediately adjacent to the back of the sidewalk per City Standard Details.
- W3. **Encroachment Permit:** Prior to issuance of Building Permits, the applicant shall submit an encroachment permit application and design plans for construction of water utilities that comply with the latest edition of the Water & Sewer Utilities Water Service and Use Rules and Regulations, Water System Notes, and Water Standard Details and Specifications. In addition, prior to the City's issuance of Occupancy, the applicant shall construct all public water utilities per the approved plans. The Water & Sewer Utilities will inspect all public water utility installations and all other improvements encroaching public water utilities.
- W4. **Utility Design Plans:** Utility Design Plans shall indicate the pipe material and the size of existing water, recycled water and sewer main(s). The plans shall show the nearest existing fire hydrant and the two nearest existing water main line gate valves near the project area. The plans shall show meter and backflow configurations to scale and per City of Santa Clara Water & Sewer Utilities Standard Details.

Note that all new water meters and backflow prevention devices shall be located behind the sidewalk in a landscape area. Fire hydrants should be located two feet behind monolithic sidewalk if sidewalk is present; two feet behind face of curb if no sidewalk is present, per City Std Detail 18. The plans shall provide the profile section details for utilities crossing water, sewer, or recycled water mains to ensure a 12" minimum vertical clearance is maintained.

- W5. Utility Separations: Applicant shall adhere to and provide a note indicating that all horizontal and vertical clearances comply with State and local regulations. The applicant shall maintain a minimum 12" of vertical clearance at water service crossing with other utilities, and all required minimum horizontal clearances from water services: 10' from sanitary sewer utilities, 10' from recycled water utilities, 8' from storm drain utilities, 5' from fire and other water utilities, 3' from abandoned water services, 5' from gas and electric utilities, and 5' from the edge of the propose or existing driveway. For sanitary sewer, water, and recycled water utilities, the applicant shall maintain a minimum horizontal clearance of 10' from existing and proposed trees. If applicant installs tree root barriers, clearance from tree reduces to 5' (clearance must be from the edge of tree root barrier to edge of water facilities). No structures (fencing, foundation, biofiltration swales, etc.) allowed over sanitary sewer, potable water and/or recycled water utilities and easements.
- W6. Separate Services: Applicant shall submit plans showing proposed water, recycled water, sanitary sewer, and fire services connected to a public main in the public right-of-way to the satisfaction of the Director of Water & Sewer Utilities. Different types of water and recycled water use (domestic, irrigation, fire) shall be served by separate water services, each separately tapped at the water main. Tapping on existing fire service line(s) is prohibited. Approved backflow prevention device(s) are required on all potable water services.
- W7. City Standard Meters and Backflows: All proposed meters and backflows for all water services shall meet the current City of Santa Clara Water & Sewer Utilities Standard Details. Plans shall show meter and backflow configurations to scale.
- W8. Existing Services: The applicant must indicate the disposition of all existing water and sewer services and mains on the plans. If the existing services will not be used, then the applicant shall properly abandon these services to the main per Water & Sewer Utilities standards and install a new service to accommodate the water needs of the project. The applicant shall bear the cost of any relocation or abandonment of existing Water Department facilities required for project construction to the satisfaction of the Director of Water and Sewer Utilities.
- W9. On-Site Storm Drain Treatment: Prior to issuance of Building Permit, the applicant shall submit plans showing any onsite storm water treatment system. The plan shall include a section detail of the treatment system. No water, sewer, or recycled water facilities shall be located within 5-feet of any storm water treatment system.
- W10. Water Usage: Prior to the issuance of Building Permits, the applicant shall provide documentation of water usage so the Water Division can verify the appropriate size of all proposed water meters. Please note that if the existing water services are incapable of supplying the water needs to the site, the existing services shall be abandoned, and new separate dedicated water services shall be provided for each use (domestic and irrigation).
- W11. Prior to issuance of Building Permits, the applicant shall submit plan details for all water features (including but not limited to fountains and ponds) designed to include provisions for operating the system without City potable water supply and capable of being physically disconnected from source of potable water supply during City declared water conservation periods, to the satisfaction of the Director of the Water & Sewer Utilities. Decorative water features may be permanently connected to the City's recycled water supply.
- W12. Easements: Prior to City's issuance of Building or Grading Permits, the applicant shall provide a dedicated water utility easement around the backflow prevention device onsite. The water utility easement for the water services and all other public water appurtenances shall be a minimum 15 feet wide and be adjacent to the public right-of-way without overlapping any public utility easement. Additionally, the applicant shall submit plans defining existing easements so Water Division can verify if there are any conflicts with proposed easements and water utilities.

- W13. Underground Fire Permit: Prior to issuance of Building Permits, applicant shall submit an underground fire permit unless otherwise waived by the Fire Department. If fire flow information is needed, applicant shall coordinate with Water and Sewer Utilities Department, for fire flow information at (408)615-2000. A dedicated fire service line, with an approved backflow prevention device, shall be used for on-site fire hydrants. Fire service lines required for commercial and industrial use shall be sized appropriately per fire flow demand and code requirements.
- W14. Record Drawings: Upon completion of construction and prior to the City's issuance of a Certificate of Occupancy, the applicant shall provide "as-built" drawings of the public water utility infrastructure prepared by a registered civil engineer to the satisfaction of the Director of Water & Sewer Utilities Department.
- W15. Water Shortage Response Actions: Pursuant to the City of Santa Clara's Urban Water Management Plan, during times of drought or water shortage, the City implements water shortage response actions in accordance with the level of water shortage declared. All construction activities and all new irrigation connections are subject to the Water Shortage Response Actions in effect at the time of construction and connection of the irrigation service.

Water Shortage Response Actions for Stage 2 and higher include water use restrictions that limit the use of potable water such as:

- a. prohibiting the installation of new potable water irrigation services, new irrigation connections, construction, and dust control.
- b. restrict the use of potable water used for construction and dust control if recycled water is available.

This project is subject to all the requirements and restrictions of the Water Shortage Response Actions in place or adopted during the duration of the project. For more information, visit the City of Santa Clara Water & Sewer Utilities website at www.santaclaraca.gov/waterconservation.



Agenda Report

21-997

Agenda Date: 8/18/2021

REPORT TO DEVELOPMENT REVIEW HEARING

SUBJECT

Adoption of a Mitigated Negative Declaration (MND) and Mitigation Monitoring or Reporting Program (MMRP) and action on AB 3194 proposal for the proposed six three-story townhomes and two detached, two-story single-family homes at 1530 and 1540 Pomeroy Avenue

File No.(s): PLN2019-13924

Location: 1530 and 1540 Pomeroy Ave; two parcels with a total lot area of .048 acres, located on the west side of Pomeroy Avenue between El Camino Real and Granada Ave; APN: 290-02-096, 290-02-097; Property at 1530 Pomeroy is zoned Low Density Multiple Dwelling (R3-18D) and 1540 Pomeroy Avenue zoned Agricultural (A).

Applicant: The Ridgecrest Group Inc./Omid Shakeri

Owner: Masud Maesumi and Valm Pascual

Request: Architectural Review utilizing AB 3194 to construct six three-story townhomes and two detached, two-story single-family homes on two contiguous residential lots with a total lot area of 0.48 acres with 19 parking spaces. The project involves demolition of two existing one-story single family homes at 1530 and 1540 Pomeroy Avenue.

Project Data: See Attachment 1.

Points for consideration

Summary

- The applicant proposes to redevelop the two contiguous lots with a total area of 0.48 acre with two independent projects as following:
 - 1530 Pomeroy Ave (.23 acres): Construct two detached Single Family homes.
 - 1540 Pomeroy Ave (.25 acres): Construct six attached townhomes
- The project would utilize Assembly Bill (AB) 3194 (2018), which prohibits a local agency from requiring a rezoning if a proposed project is consistent with the general plan but the zoning for the project site is inconsistent with the general plan. Further, a local agency may require the proposed housing development project to comply with the objective standards and criteria of the zoning designation which is consistent with the general plan; however, the standards and criteria are to be applied to facilitate and accommodate development at the density allowed on the site by the general plan and proposed by the proposed housing development project.
- AB 3194 is applicable to the parcel at 1540 Pomeroy Avenue, which is zoned Agricultural (A) and has a General Plan Designation of Community Mixed Use (CMU). Pursuant to AB 3194, the project proposes to employ the objective standards of the Medium Density Multiple Dwelling (R3-36D) zoning district to construct six attached townhomes, which is a permitted use in this zoning district. The prescribed density range for Community Mixed Use designation of 20-36 du/acre would allow construction of 5-9 units on this site. Based on the lot size of 10,920 square feet (.25 acres) the density for the proposal to construct the six units is 24 du/acre and within the allowable density range.
- The parcel at 1530 Pomeroy Avenue is zoned Low Density Multiple Dwelling (R3-18D) and a General Plan designation of Very Low Density Residential. The proposed two single-family homes are permitted uses per the zoning code and result in a density of 8.7 du/acre based on the lot size of 10,080 square feet (.23 acre) and is therefore, consistent with density of up to 10 units per acre allowed for the Very Low Density Residential designation.

- A condominium map would be processed to consolidate and subdivide the land into private residential lots and common space for the driveway, open space, and guest parking areas and will be filed following the DRH meeting action.
- A Community outreach meeting for the project is scheduled for Thursday August 12, 2021.

Building Design

- The project would construct two detached single family homes on the southern parcel (1530 Pomeroy) that borders single family homes to the west and to the south. Each home would be 2,165 square feet with four bedrooms and three bathrooms and a 400 square foot attached garage.
- The parcel on the north (1540 Pomeroy) that borders commercial uses to the north and the west, would be occupied with six three-story attached townhomes. Each townhome would be 1,970 square feet with three bedrooms and two bathrooms and a 400 square foot attached garage.
- The two single family homes at 1530 Pomeroy adhere to the setback, lot coverage and all other zoning regulations of the Low Density Multiple Dwelling (R3-18D) Zoning District. The six Townhomes at 1540 Pomeroy adhere to the to the setback, lot coverage and all other zoning regulations of the Medium Density Multiple Dwelling (R3-36D) Zoning District, which is the zoning district that is consistent with the CMU General Plan designation.
- The townhomes have been designed with a contemporary American colonial architecture featuring horizontal siding on the first and the second levels, interspersed on the second level with wood shingle-covered bays. The third level would be sided with wood shingles and punctuated with gable bays that continue the horizontal wood siding from the lower levels. Fenestration would include a mix of divided-light vertical and square windows. The townhomes would be a single building with hipped roof and a long central ridge. The architecture of the single family homes would be the same as that of the townhomes but in a two story format.
- The private garages would be accessed through a shared 20 feet wide driveway.
- It should be noted that only objective design and zoning standards can be applied to projects utilizing AB 3194 (here, the parcel at 1540 Pomeroy).

Open Space

The project provides an open space to the rear and south of the project site. A landscape plan has not been developed yet, but the applicant has indicated the site would be developed with drought tolerant and water efficient landscaping. The property frontage would be planted with new street trees in accordance with the City requirements. All units will have a private patio.

Parking

A 400 square foot attached garage is incorporated in the ground floor of each residential unit, exceeding the requirement of one covered and one uncovered parking spaces for zoning districts R3-18D and R3-36D. In addition, three guest parking spaces have been provided on the single family project site at 1530 Pomeroy.

Findings supporting the Staff Recommendation

1. *That any off-street parking area, screening strips and other facilitates and improvements necessary to secure the purpose and intent of this title and the general plan of the City area a part of the proposed development, in that;*
 - The development provides a total of 19 on-site parking spaces (16 covered, 3 uncovered), exceeding the requirement of eight covered and eight uncovered spaces and provides adequate circulation for vehicular access in accordance with Chapter 18.74 Parking regulations.
2. *That the design and location of the proposed development and its relation to neighboring developments and traffic is such that it will not impair the desirability of investment or occupation in the neighborhood, will not unreasonably interfere with the use and enjoyment of neighboring developments, and will not create traffic congestion or hazard, in that;*

- The proposed project would not generate an increase in trips previously anticipated with the analysis undertaken as part of the land use study in the 2010-2035 General Plan and further analyzed and determined to be less than significant in the Mitigated Negative Declaration prepared for this project.
- On balance, the project is consistent with the City’s General Plan as discussed below.

TABLE 1: PROJECT CONSISTENCY WITH SANTA CLARA GENERAL PLAN LAND USE POLICIES

Land Use Policies	Project Consistency
5.3.1-P2: Encourage advance notification and neighborhood meetings to provide an opportunity for early community review of new development proposals.	The project included a community meeting and posted Notice of Development Proposal on the project site.
5.3.1-P3: Support high quality design consistent with adopted design guidelines and the City’s architectural review process.	The project includes a variety of materials and design techniques when combined makes the project aesthetically compatible with the vision of the area.
5.3.1 P4: Encourage new development that meets the minimum intensities and densities specified in the land use classifications or as defined through applicable Focus Area, Neighborhood Compatibility or Historic Preservation policies of the General Plan.	The project’s density is within the minimum and maximum range indicated in the General Plan. The parcel at 1530 Pomeroy is approximately .23 acre and would house 2 single family homes at a density of 8.7 du/acre that is consistent with the density for the Very Low Density General Plan designation (up to 10 du/acre). The parcel at 1540 Pomeroy is approximately .25 acre and would develop six units at a density of 24 du/acre consistent with the density range of the Community Mixed Use designation (20-36 du/acre).
5.3.1 P9: Require that new development provide adequate public services and facilities, infrastructure, and amenities to serve the new employment or residential growth.	The project will be required to upgrade infrastructure facilities in accordance with the City’s policies.
5.3.1 P10: Provide opportunities for increased landscaping and trees in the community, including requirements for new development to provide street trees and a minimum 2:1 on or site replacement for trees removed as part of the proposal to help increase the urban forest and minimize the heat island effect.	Project would remove the existing vegetation and develop a landscape plan consistent with the City’s tree replacement policy. Site would be developed with drought tolerant and water efficient landscaping. The site frontage would be planted with new street trees in accordance with the City Requirements.
5.3.1 P12: Encourage convenient pedestrian connections within new and existing developments.	The project would include 5’ wide sidewalk with 4.5’ wide planter strip along the property frontage connecting to existing developments.

<p>5.3.1 P29 Encourage design of new development to be compatible with, and sensitive to, nearby existing and planned development, consistent with other applicable General Plan policies.</p>	<p>The project would construct six 3 story townhomes oriented towards the adjacent commercial uses and two 2-story single family homes oriented towards the adjacent single family homes.</p>
<p>5.3.2 P8: Require new residential development to comply with applicable regulations for the provision of affordable housing.</p>	<p>The project will be required to provide one affordable dwelling unit or pay an in-lieu fee.</p>
<p>5.5.2 P1 Require that new development incorporate building articulation and architectural features, including front doors, windows, stoops, porches or bay windows along street frontages to integrate new development into existing neighborhoods.</p>	<p>The buildings' orientation and architectural features have been employed in the project to integrate and promote compatibility with the existing neighborhood.</p>

3. *That the design and location of the proposed development is such that it is in keeping with the character of the neighborhood and is such as not to be detrimental to the harmonious development contemplated by this title and the general plan of the City, in that:*
 - The development is a two to three story medium density residential structure that is compatible with the existing residential uses in the area.
 - The two single family homes at 1530 Pomeroy adhere to the setback, lot coverage and all other zoning regulations of the Low Density Multiple Dwelling (R3-18D) Zoning District.
 - The six Townhomes at 1540 Pomeroy adhere to the to the setback, lot coverage and all other zoning regulations of the Medium Density Multiple Dwelling (R3-36D) Zoning District. Therefore, the project is consistent with this finding.

4. *That the granting of such approval will not, under the circumstances of the particular case, materially affect adversely the health, comfort or general welfare of persons residing or working in the neighborhood of said development, and will not be materially detrimental to the public welfare or injuries to property or improvements in said neighborhood, in that:*
 - The project is subject to the California Building Code and City Code requirements, which serve to regulate new construction to protect public health safety and general welfare.
 - The use, scale, and design of the development, as conditioned, are consistent with the General Plan density contemplated for this area, and is therefore compatible with the existing and future land uses in the surrounding area.

5. *That the proposed development, as set forth in the plans and drawings, are consistent with the set of more detailed policies and criteria for architectural review as approved and updated from time to time by the City Council, which set shall be maintained in the planning division office, in that:*
 - The proposed development provides for an attractive, inviting, imaginative and functional site arrangement of the building, the obscured parking areas, and a high quality architectural and landscape design. The development also complies with the objective zoning standards applicable to each parcel as permitted under AB 3194.

ENVIRONMENTAL REVIEW

A Mitigated Negative Declaration (MND) has been prepared for the project in accordance with the California Environmental Quality Act (CEQA). The MND and Notice of Availability were posted on the City’s website [here](https://www.santaclaraca.gov/Home/Components/BusinessDirectory/BusinessDirectory/405/3649) <<https://www.santaclaraca.gov/Home/Components/BusinessDirectory/BusinessDirectory/405/3649>> and circulated for 20-day review on June 9, 2021 and closed on July 1, 2021, in accordance with CEQA

requirements. No public comments were received.

An errata has been prepared and posted on the City's website [here](https://www.santaclaraca.gov/Home/Components/BusinessDirectory/BusinessDirectory/405/3649) <<https://www.santaclaraca.gov/Home/Components/BusinessDirectory/BusinessDirectory/405/3649>> to accurately describe the consistency of the project with density requirements in the General Plan.

FISCAL IMPACT

There is no impact to the City for processing the requested application other than administrative staff time and expense typically covered by processing fees paid by the applicant.

PUBLIC CONTACT

On August 5, 2021, a notice of public hearing of this item was posted within 300 feet of the project site and mailed to property owners within 500 feet of the project site. Planning Staff has received one public comments for this application and has been included with this report as Attachment 3.

RECOMMENDATION

Adopt the MND and MMRP for this project and Approve the architectural review of the proposed six three-story townhomes and two detached, two-story single-family homes at 1530 and 1540 Pomeroy Avenue per AB 3194, subject to conditions.

Prepared by: Nimisha Agrawal, Associate Planner, Community Development Department

Approved by: Gloria Sciara, Development Review Officer, Community Development Department

ATTACHMENTS

1. Project Data
2. Conditions of Approval
3. Development Plans
4. Public Comments

Project Data -1530 and 1540 Pomeroy

Project Data

	Existing	Proposed	Zoning/GP Consistency
General Plan Designation			
1530 Pomeroy	Very Low Density Residential	No change	
1540 Pomeroy	Community Mixed Use		
Zoning Designation			
1530 Pomeroy	Low Density Multiple Dwelling (R3-18D)	No change	Consistent with R3-18D
1540 Pomeroy	Agricultural (A)	No change	Consistent with R3-36D
Land Use	Residential	No change	Residential
Lot Size (Sq. ft.) Total 0.48 acres (21,000 sq. ft.)			
1530 Pomeroy	10,080 sq. ft. (.23 acres)	No change	Meets minimum lot size requirement of 8,000 sq. ft.
1540 Pomeroy	10,920 sq. ft. (.25 acres)	No change	Meets minimum lot size requirement of 8,5000 sq. ft.
Building Square Footage	Total 3,412 sq. ft.	Total 19,390 sq. ft.	
1530 Pomeroy	1,706 sq. ft.	5,170 sq. ft.	Lot coverage 28.5% (max 35%)
1540 Pomeroy	1,706 sq. ft.	14,220 sq. ft.	Lot coverage 45% (max 45%)
Residential Units			
1530 Pomeroy	1	2 Detached Single Family Homes	Density: 8.7 du/acre (allowed up to 10 du/acre)
1540 Pomeroy	1	6 Attached Townhomes	Density: 24 du/acre (allowed 20-36 du/acre)
Height			
1530 Pomeroy	1-story building	24'-8"	Consistent with R318D; max allowed 25'
1540 Pomeroy	1-story building	36'-6"	Consistent with R3-36D; max allowed 45'
Open Spaces			
1530 Pomeroy	-	4,130 sq. ft. (41%)	Consistent with R318D; max required 40%
1540 Pomeroy	-	4,370 sq. ft. (40%)	Consistent with R3-36D; max required 40%
Parking			
1530 Pomeroy	Surface parking	7	Exceeds the requirement for 4 covered parking
1540 Pomeroy	Surface parking	12	Exceeds the requirement for 6 covered and 6 uncovered parking
Flood Zone	X	No Change	

CONDITIONS OF APPROVAL

In addition to complying with all applicable codes, regulations, ordinances and resolutions, the following **conditions of approval** are recommended:

GENERAL

- A. If relocation of an existing public facility becomes necessary due to a conflict with the developer's new improvements, then the cost of said relocation shall be borne by the developer.
- B. Comply with all applicable codes, regulations, ordinances and resolutions.

ATTORNEY'S OFFICE

- A. The Developer agrees to defend and indemnify and hold City, its officers, agents, employees, officials and representatives free and harmless from and against any and all claims, losses, damages, attorneys' fees, injuries, costs, and liabilities arising from any suit for damages or for equitable or injunctive relief which is filed by a third party against the City by reason of its approval of developer's project.

COMMUNITY DEVELOPMENT

BUILDING DIVISION

- BD1. Prior to overall construction permit application, submit to the Santa Clara Building Division, 2 copies of an addressing diagram request, to be prepared by a licensed architect or engineer. The addressing diagram(s) shall include all proposed streets and all building floor plans. The addressing diagram(s) shall conform to Santa Clara City Manager Directive #5; Street Name and Building Number Changes, and Santa Clara Building Division Address Policy for Residential and Commercial Developments. The addressing diagram(s) shall indicate all unit numbers to be based off established streets, not alleys nor access-ways to garages. Allow a minimum of 10 working days for initial staff review. Please note city staff policy that existing site addresses typically are retired. Provide digital pdf printed from design software, not scanned from printed paper sheet.
- BD2. The construction permit application drawings submitted to the Santa Clara Building Division shall include a copy of the latest Federal Emergency Management Agency (FEMA) Flood Zone Map: <https://msc.fema.gov/portal/home>. The project drawings shall indicate how the project complies with the Santa Clara Flood Damage Prevention Code.
- BD3. The construction permit application drawings submitted to the Santa Clara Building Division shall include Santa Clara Valley Urban Runoff Pollution Prevention Program Low Impact Development (LID) practices http://www.scvurppp-w2k.com/nd_wp.shtml. All projects that disturb more than one acre, or projects that are part of a larger development that in total disturbs more than one acre, shall comply with the Santa Clara Valley Urban Runoff Pollution Prevention Program Best Management Practices (BMP): http://www.scvurppp-w2k.com/construction_bmp.shtml, and shall provide a Storm Water Pollution Prevention Plan (SWPPP) by a certified Qualified SWPPP Developer (QSD). All site drainage and grading permit applications submitted to the Santa Clara Building Division shall include a city of Santa Clara "C3" data form, available on this web page: <https://www.santaclaraca.gov/our-city/departments-g-z/public-works/environmental-programs/stormwater-pollution-prevention> and will be routed to a contract consultant for review.
- BD4. Informational: no California construction code review is being done at this time. The construction permit application drawings submitted to the Santa Clara Building Division shall include an overall California Building Code analysis, including; proposed use and occupancy of all spaces (19' CBC Ch. 3), all building heights and areas (19' CBC Ch. 5), all proposed types of construction (19' CBC Ch. 6), all proposed fire and smoke protection features, including all types of all fire rated penetrations proposed (19' CBC Ch. 7), all proposed interior finishes fire resistance (19' CBC Ch. 8), all fire protection systems proposed (19' CBC Ch. 9), and all means of egress proposed (19' CBC Ch. 10). -Noncombustible exterior wall, floor, and roof finishes are strongly encouraged.
- BD5. During construction retaining a single company to install all fire rated penetrations is highly recommended.

- BD6. The overall project construction permit application shall include the geotechnical, architectural, structural, energy, electrical, mechanical, and plumbing drawings and calculations. Prior to the issuance of the overall project construction permit, a conditions of approval review meeting must be held in city hall, which meeting must be attended by the on-site field superintendent(s). The meeting will not be held without the attendance of the on-site field superintendent(s). The on-site grading permit shall be a separate permit application to the Building Division.
- BD7. The construction permit application drawings submitted to the Santa Clara Building Division shall include all accessibility requirements of the 19' CBC Ch. 11 as applicable.
- BD8. The construction permit application drawings submitted to the Santa Clara Building Division shall include checklist(s) indicating compliance with the applicable Mandatory Measures of the 19' Cal. Green Building Standards Code (CGBSC). Provide Construction Waste Management (CWM) Plan per the 19' CGBSC guides on pp 59-63 of the CGBSC. Provide a Phase 1 and/ or Phase 2 Hazardous Materials site assessment, as applicable. Note: The Santa Clara Public Works Department Environmental Programs Division will require compliance with the Santa Clara Construction & Demolition Debris Recycling Program: <http://santaclaraca.gov/government/departments/public-works/environmental-programs/commercial-garbage-recycling/construction-demolition-debris-recycling-program>. Note: the Environmental Programs Division may require development projects to register with the Green Halo online waste tracking system: <https://www.greenhalosystems.com>.
- BD9. Note: Temporary Certificates of Occupancy will not be routinely issued and will be considered on a very limited basis only when there is a clear and compelling reason for city staff to consider a TCO. A TCO will be approved only after all applicable City staff have approved in writing; Planning, P.W./ Engineering, Fire Prev., Santa Clara Water, Silicon Valley Power, and any other applicable agencies such as the Santa Clara County Health Dept., with the Building Division being the final approval of all TCO's.

HOUSING & COMMUNITY SERVICES DIVISION

- H1. In accordance with the Santa Clara City Code chapter 17.40, this project is subject to the following affordable housing requirements and impact fee:
The requirement for the for-sale residential development which applies to the proposed 8 units is as follows:

The Applicant shall provide one dwelling unit affordable to households at 100%AMI or pay an in-lieu fee. The in-lieu fee shall be equal to the difference between the unrestricted appraised market value ("Initial Market Value") and the Affordable Sales Price of the last unit at completion. The Initial Market Value of the last unit sold shall be the basis for calculating the in-lieu fee to be paid by Applicant. Any in-lieu fee payment due to the City by the Applicant shall be paid prior to receipt of the occupancy certificate of the last unit at completion.

Affordable Unit shall have prices set in accordance with the City's Below Market Purchase (BMP) Program Policies and Procedures Manual (subject to updates and changes). Prior to issuance of Building Permits, the Developer shall enter into an Affordable Housing Agreement with the City that will determine the Affordable Sales Price, identify the actual unit to be sold as the Affordable Unit, and apply all terms and covenants guaranteeing the prescribed affordability, to the satisfaction of the Director of Community Development. There is a fee for the AHA preparation in the amount of \$3,708 which will be due prior to execution of the AHA.

PLANNING DIVISION

- P.1 Submit plans for final architectural review to the Planning Division and obtain architectural approval prior to issuance of building permits. Said plans to include, but not be limited to site plans, floor plans, elevations, landscaping, lighting and signage. Landscaping installation shall meet City water conservation criteria in a manner acceptable to the Director of Community Development.
- P.2 Obtain required permits and inspections from the Building Official and comply with the conditions thereof. If this project involves land area of 1 acre or more, the developer shall file a Notice of Intent (NOI) with the State Water Resources Control Board prior to issuance of any building permit for

- grading, or construction; a copy of the NOI shall be sent to the City Building Inspection Division. A storm water pollution prevention plan is also required with the NOI.
- P.3 Comply with all requirements of Building and associated codes (the CBC, CEC, CMC, CPC, California Green Building Code, the California Energy Code, etc.) current at the time of application for Building Permit, that includes grading and site utility permits.
- P.4 The Developer shall comply with the Mitigations Monitoring and Reporting Program identified in the Initial Study / Mitigated Negative Declaration and shall be incorporated in the Conditions of Approval for this project.
- P.5 A change in the original number of unit and any subsequent division or increase in the number of units shall require a discretionary development permit.
- P.6 Prior to issuance of a demolition permit, Developer/Owner shall have an asbestos survey of the proposed site performed by a certified individual. Survey results and notice of the proposed demolition are to be sent to the Bay Area Air Quality Management District (BAAQMD). No demolition shall be performed without a demolition permit and BAAQMD approval and, if necessary, proper asbestos removal.
- P.7 Incorporate Best Management Practices (BMPs) into construction plans and incorporate post construction water runoff measures into project plans in accordance with the City's Urban Runoff Pollution Prevention Program standards prior to the issuance of permits. Proposed BMPs shall be submitted to and thereafter reviewed and approved by the Planning Division and the Building Inspection Division for incorporation into construction drawings and specifications.
- P.8 An erosion control plan shall be prepared, and copies provided to the Planning Division and to the Building Inspection Division for review and approval prior to the issuance of grading permits or building permits that involve substantial disturbance of substantial ground area.
- P.9 Commercial, industrial, and multi-family residential buildings must have enclosures for solid waste and recycling containers. The size and shape of the enclosure(s) must be adequate to serve the estimated solid waste and recycling needs and size of the building(s) onsite and should be designed and located on the property so as to allow ease of access by collection vehicles. As a general rule, the size of the enclosure(s) for the recycling containers should be similar to the size of the trash enclosure(s) provided onsite. Roofed enclosures with masonry walls and solid metal gates are the preferred design. Any required enclosure fencing (trash area, utility equipment, etc.) if not see-thru, shall have a six (6) inch opening along the bottom for clear visibility. Any gates or access doors to these enclosures shall be locked.
- P.10 The Final Storm Water Management Plan (SWMP) must be certified by a third-party consultant from SCVURPP's current list of qualified consultants. Five copies of the approval letter from the certified third party review (wet stamped and signed) must be submitted prior to the issuance of grading or building permit.
- P.11 Prior to the issuance final occupancy, the applicant shall enter into Operations and Maintenance (O&M) agreement with the City. The project operator is responsible for the operations and maintenance of the SWMP and stormwater BMPs consistent with the O&M agreement throughout the life of the project.
- P.12 A complete landscape plan that includes, type, size and location of all plant species shall be required as part of architectural review of the project. Review and approval of the complete landscape plan, including water conservation calculations and irrigation plan shall be required prior to issuance of building permits. Installation of landscaping is required prior to occupancy permits.
- P.13 Site landscaping shall be maintained in good condition throughout the life of the Development and no trees shall be removed without City review and approval.
- P.14 Trees permitted by the City for removal shall be replaced at a 2:1 ratio with 24-inch box specimen trees reviewed, or equal alternative as approved by the Director of Community Development.
- P.15 The project shall provide complete streets along the project frontage on Pomeroy Avenue at least 5 foot wide detached sidewalk with a 4.5 foot landscape strip.
- P.16 Construct six-foot masonry wall along property lines abutting commercial and residential properties in accordance with Zoning Ordinance requirements or to the satisfaction of the Director of Community Development.

- P.17 Developer is responsible for collection and pick-up of all trash and debris on-site and adjacent public right-of-way.
- P.18 Minor changes to the project would be subject to Planning Division review and approval prior to issuance of building permits.
- P.19 Construction activity not confined within a building shall be limited to the hours of 7:00 a.m. to 6:00 p.m. weekdays and not permitted on Saturdays and Sundays for projects within 300 feet of a residential use. Construction activity confined within a building shall be limited to the hours of 7:00 A.M. to 6:00 P.M. following on weekdays other than holidays, Monday through Friday, inclusive; and within the hours of 9:00 A.M. to 6:00 P.M. following, inclusive, on any Saturday which is not a holiday. Construction activity shall not be allowed on recognized State holidays, as noted in Section 9.10.230 of the SCCC, as amended.

FIRE

- F1. The Fire Department's review was limited to verifying compliance per the 2016 California Fire Code (CFC), Section 503 (Fire Apparatus Access Roads), Section 507 (Fire Protection Water Supplies), Appendix B (Fire-Flow Requirements for Buildings) and Appendix C (Fire Hydrant Locations and Distribution) and City of Santa Clara Requirements.
- F2. (For buildings other than one- and two-family dwellings, Group R-3 and R-4 buildings and townhomes): At time of Building Permit application provide documentation to show the minimum required fire-flow for the building based on the construction type and square footage in accordance with the California Fire Code, Appendix B, Table B105.1 can be met. A 75% reduction in fire-flow is allowed with the installation of a automatic fire sprinkler system designed in accordance with California Fire Code § B105.2. The resulting fire-flow shall not be less than 1,500 gallons per minute (or 1,000 gallons per minute for NFPA 13 fire sprinkler systems) minute for the prescribed duration.
- F3. (Required fire flow for one- and two-family dwellings, Group R-3 and R-4 buildings and townhomes): At time of Building Permit application, provide documentation to show the minimum required fire-flow for the building based on the construction type and square footage in accordance with the California Fire Code, Appendix B, Table B105.1 can be met. A 75% reduction in fire-flow is allowed with the installation of a automatic fire sprinkler systems designed in accordance with California Fire Code § B105.1(1).
- F4. At time of Building Permit application, the required number, location and distribution of fire hydrants for the building based on the California Fire Code, Appendix C, Table C102.1 shall be incorporated into the construction documents. The required number of fire hydrants shall be based on the fire-flow before the reduction.
- F5. At time of Building Permit application, construction documents for proposed fire apparatus access, location of fire lanes and construction documents and hydraulic calculations for fire hydrant systems shall be submitted to the Fire Prevention and Hazardous Materials Division.
- F6. Prior to the start of construction, fire protection water supplies shall be installed and made serviceable prior to the time of construction or prior to combustible materials being moved onsite, unless an approved alternative method of protection is approved by the Fire Prevention and Hazardous Materials Division.

PARKS & RECREATION

- PR1. The City will accept a fee in lieu of parkland dedication for this eight-unit development – the equivalent fee due is \$ 190,656 (giving credit for residents of 2 existing SFRs).
- PR2. Any in lieu fees imposed under Chapter 17.35 shall be due and payable to the City prior to issuance of a building permit for each dwelling unit.
- PR3. A dwelling unit tax (DUT) is also due based on the number of units and additional bedrooms per City Code Chapter 3.15. The Project mix includes 4 four-bedroom units and 4 three-bedroom units for a total DUT of \$220.
- PR4. Calculations may change if the number of units change, if any areas do not conform to the Ordinance and City Code Chapter 17.35, if the fee schedule for new residential development fees due in lieu of

parkland dedication changes before this Project is deemed complete by Planning, and/or if City Council makes any changes.

POLICE

- PD1. There shall be positioned near the entrance an illustrative diagram of the complex, which shows the location of the viewer and unit designations within the complex, including separate building designations. This diagram shall be illuminated and should be protected by vandal and weather resistant covers.
- PD2. Each distinct unit within the building shall have its address displayed on or directly above both front and rear doors.
- PD3. All construction of dwelling units shall conform to the requirements of the Uniform Building Security Code as adopted by the City of Santa Clara City Council.
- PD4. All entrances to the parking areas (structure, surface, subterranean, etc.) shall be posted with appropriate signage to discourage trespassing, unauthorized parking, etc. (See California Vehicle Code section 22658(a) for guidance)
- PD5. Applicant shall install signage to prevent theft from vehicles in the parking lots. In addition, the use of quality lighting, installation of high-quality video cameras/recorders, and license plate readers are highly encouraged to prevent thefts from vehicles.

PUBLIC WORKS

ENGINEERING

- E1. Obtain site clearance through Public Works Department prior to issuance of Building Permit. Site clearance will require payment of applicable development fees. Other requirements may be identified for compliance during the site clearance process. Contact Public Works Department at (408) 615-3000 for further information.
- E2. All work within the public right-of-way and/or public easement, which is to be performed by the Developer/Owner, the general contractor, and all subcontractors shall be included within a Single Encroachment Permit issued by the City Public Works Department. Issuance of the Encroachment Permit and payment of all appropriate fees shall be completed prior to commencement of work, and all work under the permit shall be completed prior to issuance of occupancy permit.
- E3. Submit public improvement plans prepared in accordance with City Public Works Department procedures which provide for the installation of public improvements. Plans shall be prepared by a Registered Civil Engineer and approved by the City Engineer prior to approval and recordation of subdivision map and/or issuance of building permits.
- E4. Developer shall provide a complete storm drain study for the 10-year and 100-year storm events. The grading plans shall include the overland release for the 100-year storm event and any localized flooding areas. System improvements, if needed, will be at developer's expense.
- E5. Construct the storm drain main along Pomeroy Avenue (from El Camino Real) to serve the proposed development with stub beyond proposed manhole for future construction extending to the southern property line. The main size shall be sufficient to convey the 10-year event for the entire tributary area as approved by the City. Storm drain main shall be 18" per approved tributary area analysis.
- E6. Obtain Caltrans permit for all work or traffic control within the State right-of-way portion of El Camino Real.
- E7. Sanitary sewer (SS) laterals shall be VCP or SDR-26 PVC (6" diameter minimum if serving 5 or more dwelling units, 4" diameter minimum if serving less than 5 dwelling units) and have a minimum slope of 2%. The minimum cover for lateral from top of curb shall not be less than 4.5 feet.
- E8. Connection of the new Sanitary Sewer lateral to the existing Sanitary Sewer main shall be per City standard details SS-2 and SS-3
- E9. Damaged curb, gutter, and sidewalk within the public right-of-way along property's frontage shall be repaired or replaced (to the nearest score mark) in a manner acceptable to the City Engineer or his

designee. The extents of said repair or replacement within the property frontage shall be at the discretion of the City Engineer or his designee.

- E10. Existing non-standard or non-ADA compliant frontage improvements shall be replaced with current City standard frontage improvements as directed by the City Engineer or his designee.
- E11. After City Council approval of the Tentative Subdivision Map, submit 10 copies of the Final Map, prepared by a Licensed Land Surveyor or a Registered Civil Engineer with Land Surveyor privileges to the Engineering Department. The submittal shall include a title report, closure calculations, and all appropriate fees. The City approved Final Map shall be recorded by developer prior to Building Permit issuance.
- E12. File and record Subdivision Map to create parcels for proposed development and pay all appropriate fee(s) prior to Building Permit issuance.
- E13. Dedicate required on-site easements for sidewalk and any new public utilities by means of subdivision map or approve instrument at time of development.
- E14. Sanitary sewer and storm drain mains and laterals shall be outside the drip line of mature trees or 10' clear of the tree trunk whichever is greater.
- E15. SVP joint trench shall have a concrete cap for portions not under the public sidewalk.
- E16. Entire width of Pomeroy Avenue along property frontage shall be treated with a cape seal with digouts. If the encroachment permit for the Project's off-site improvements is not submitted within two (2) years of Final Approval for the Project, the required pavement treatment is subject to revision per the City's pavement program recommendations at the time of encroachment permit issuance. For purposes of this condition, "Final Approval" shall mean the final vote of the City Council necessary for all entitlements to be approved, unless a legal challenge is brought to the Council decisions, in which case the Final Approval shall mean the final disposition of the legal challenge.
- E17. On-street parking shall not be counted towards on-site parking requirements.
- E18. Proposed driveway shall accommodate fire truck/engine turning template.
- E19. All proposed driveway, sidewalk, walkway, curb & gutter, and curb ramp shall be ADA compliant.
- E20. Proposed driveways shall be per City standard ST-8 driveways and comply with Driveway's Triangle of Safety.
- E21. All traffic striping, messages, and symbols shall be thermoplastic.
- E22. Provide a minimum of 5' wide sidewalk with 4.5' wide planter strip along the property frontage.
- E23. Proposed trees shall be 5' minimum clear of sidewalks. Provide root barriers when the drip line of the mature trees covers the sidewalk. Root barriers for sidewalk protection shall be 16' long or extend to drip line of the mature tree, whichever is greater, and be 1.5' deep, and centered on trees. Root barriers for curb and gutter protection shall be 16' long or extend to drip line of the mature tree, whichever is greater, and be 2' deep, and centered on trees.
- E24. For the proposed project, provide 4 Class II bicycle rack spaces.

STREETS DIVISION

STORMWATER

- ST1. Although this Project does not create over 10,000 SF of impervious surfaces and thus, not considered a C.3 Regulated project, it proposes to install over 3,000-SF permeable pavers. In accordance to the City of Santa Clara's NPDES Permit No. CAS612008, provision C.3.h.ii.(4) and (6), this Project shall be inspected annually. For this reason, the Project shall enter into an agreement with the City of Santa Clara and have an HOA or similar entity responsible for the maintenance and operation of the common-area permeable-paved driveway as well as the associated inspection fees.
- ST2. Finalize Stormwater Management Plan to clearly note location of porous pavement.
- ST3. Include [General Notes on the C.3 Stormwater Treatment Facilities Construction](#) on the improvement plans.
- ST4. During the construction phase, permeable pavement shall be inspected for conformance to approved plans by a qualified 3rd party consultant from the [SCVURPPP List of Qualified Consultants](#), and a 3rd party inspection letter shall be submitted to the Public Works Department. As-Built drawing shall be submitted to the Public Works Department. Building occupancy will not be issued until all stormwater

- treatment measures have been adequately inspected and O&M Agreement is executed. For more information contact Rinta Perkins at (408) 615-3081 or rperkins@santaclaraca.gov
- ST5. The applicant shall incorporate Best Management Practices (BMPs) into construction plans and incorporate post-construction water runoff measures into project plans in accordance with the City's Urban Runoff Pollution Prevention Program standards prior to the issuance of Building or Grading Permits. Proposed BMPs shall be submitted to and thereafter reviewed by the Planning Division and the Building Inspection Division for incorporation into construction drawings and specifications.
- ST6. The Project shall have a legal entity formed that is responsible for the maintenance, operation as well as the associated inspection fees. The property owner shall enter into an Operation and Maintenance (O&M) Agreement with the City for all installed stormwater treatment measures in perpetuity. Applicants should contact Karin Hickey at (408) 615-3097 or KaHickey@santaclaraca.gov for assistance completing the Agreement. For more information and to download the most recent version of the O&M Agreement, visit the City's stormwater resources website at <http://santaclaraca.gov/stormwater>.
- ST7. Developer shall install an appropriate stormwater pollution prevention message such as "No Dumping – Flows to Bay" on any storm drains located on private property.
- ST8. Any site design measures used to reduce the size of stormwater treatment measures shall not be removed from the project without the corresponding resizing of the stormwater treatment measures and an amendment of the property's O&M Agreement.
- ST9. Stormwater treatment facilities must be designed and installed to achieve the site design measures throughout their life in accordance to the SCVRUPPP C.3 Stormwater Handbook (Chapter 6 and Appendix C). They shall be installed using biotreatment soil media that meet the minimum specifications as set forth in this Handbook.
- ST10. Developer shall select appropriate plant materials to promote stormwater treatment measure while implementing integrated pest management and water conservation practices in accordance to the SCVRUPPP C.3 Stormwater Handbook (Appendix D).

SOLID WASTE

- SW1 Project applicant acknowledges that permeable-paved driveway shall be designed to sustain the weight of a collection truck of at least 60,000 lbs. and that the maximum straight-line distance allowed for a collection truck to back-up is 100 feet. Project applicant agrees to bear the costs to repair damages to permeable-paved driveway caused by collection trucks over time.
- SW2 Yard waste debris generated from the maintenance of common areas landscape shall be self-hauled by landscape contractor for recycling, which shall be noted on the Post-Construction Solid Waste Generation and Estimation Form.
- SW3 [During Construction] Project shall comply with City Code Section 8.25.285 and recycle or divert at least sixty five percent (65%) of materials generated for discard by the project during demolition and construction activities. No building, demolition, or site development permit shall be issued unless and until applicant has submitted a construction and demolition debris materials check-off list. Applicant shall create a Waste Management Plan and submit, for approval, a Construction and Demolition Debris Recycling Report through the City's online tracking tool at <http://santaclara.wastetracking.com>. Insert the General Notes for the Construction & Demolition (C&D) Waste Management into construction plans in accordance with the City's municipal codes prior to the issuance of a Building or Grading permit.
- SW4 [Prior to Issuance of Building permit] This project is subject to the City's Accumulation, Transportation and Disposal of Solid Waste Ordinance (Chapter 8.25 of the Municipal Codes), which requires the handling and disposal of waste by authorized service haulers. Applicant will be required to sign Exclusive Franchise Hauling Acknowledgment form provided by building staff.
- SW5 Prior to obtaining a Temporary or Final Certificate of Occupancy, weight tickets for all materials generated for discard or reuse by the project during demolition and construction activities shall be uploaded to Green Halo and submitted for review and approval by Environmental Services. At a minimum two (2) weeks review time is required.
- SW6 Each unit must have adequate space available for garbage and recycling containers to be stored within the unit.

SW7 Garbage service level required for residential developments (single-family and multi-family) shall be no less than twenty (20) gallons per unit. All project shall submit to the Public Works Department the preliminary refuse service level assessment for approval.

LANDSCAPE:

- L1 Include City of Santa Clara Tree Preservation/City Arborist specifications on all improvement plans.
- L2 No cutting of any part of private trees, including roots, shall be done without securing prior approval of the City Arborist. Tree trimming/removal shall be done in accordance to the City of Santa Clara Tree Preservation/City Arborist specifications and with direct supervision of a certified arborist (Certification of International Society of Arboriculture).
- L3 Identified existing mature trees to be maintained. Prepare a tree protection plans for review and approval by the City of Santa Clara prior to any demolition, grading or other earthwork in the vicinity of existing trees on the site.

SILICON VALLEY POWER

- SVP1. Pull boxes for SVP secondary, fiber and streetlight to be designed in detailed design. New streetlights/streetlight foundations to be included as part of detailed design.
- SVP2. The Developer shall provide and install electric facilities per Santa Clara City Code chapter 17.15.210.
- SVP3. Electric service shall be underground. See Electric Department Rules and Regulations for available services.
- SVP4. Installation of underground facilities shall be in accordance with City of Santa Clara Electric Department standard UG-1000, latest version, and Santa Clara City Code chapter 17.15.050.
- SVP5. Underground service entrance conduits and conductors shall be “privately” owned, maintained, and installed per City Building Inspection Division Codes. Electric meters and main disconnects shall be installed per Silicon Valley Power Standard MS-G7, Rev. 2.
- SVP6. The developer shall grant to the City, without cost, all easements and/or right of way necessary for serving the property of the developer and for the installation of utilities (Santa Clara City Code chapter 17.15.110).
- SVP7. If the “legal description” (not “marketing description”) of the units is condominium or apartment, then all electric meters and services disconnects shall be grouped at one location, outside of the building or in a utility room accessible directly from the outside. If they are townhomes or single-family residences, then each unit shall have it’s own meter, located on the structure. A double hasp locking arrangement shall be provided on the main switchboard door(s). Utility room door(s) shall have a double hasp locking arrangement or a lock box shall be provided. Utility room door(s) shall not be alarmed.
- SVP8. All trees, existing and proposed, shall be a minimum of five (5) feet from any existing or proposed Electric Department facilities. Existing trees in conflict will have to be removed. Trees shall not be planted in PUE’s or electric easements.
- SVP9. Any relocation of existing electric facilities shall be at Developer’s expense.
- SVP10. Electric Load Increase fees may be applicable.
- SVP11. The developer shall provide the City, in accordance with current City standards and specifications, all trenching, backfill, resurfacing, landscaping, conduit, junction boxes, vaults, street light foundations, equipment pads and subsurface housings required for power distribution, street lighting, and signal communication systems, as required by the City in the development of frontage and on-site property. Upon completion of improvements satisfactory to the City, the City shall accept the work. Developer shall further install at his cost the service facilities, consisting of service wires, cables, conductors, and associated equipment necessary to connect a customer to the electrical supply system of and by the City. After completion of the facilities installed by developer, the City shall furnish and install all cable, switches, street lighting poles, luminaries, transformers, meters, and other equipment that it deems necessary for the betterment of the system (Santa Clara City Code chapter 17.15.210 (2)).
- SVP12. Electrical improvements (including underground electrical conduits along frontage of properties) may be required if any single non-residential private improvement valued at \$200,000 or more or any

series of non-residential private improvements made within a three-year period valued at \$200,000 or more (Santa Clara City Code Title 17 Appendix A (Table III)).

- SVP13. Encroachment permits will not be signed off by Silicon Valley Power until Developers Work substructure construction drawing has been completed.
- SVP14. All SVP-owned equipment is to be covered by an Underground Electric Easement (U.G.E.E.) This is different than a PUE. Only publically-owned dry utilities can be in a UGEE. Other facilities can be in a joint trench configuration with SVP, separated by a 1' clearance, providing that they are constructed simultaneously with SVP facilities. See UG 1000 for details.
- SVP15. All existing SVP facilities, onsite or offsite, are to remain unless specifically addressed by SVP personnel by separate document. It is the Developers responsibility to maintain all clearances from equipment and easements. Developer to contact SVP outside of the PCC process for clear definitions of these clearance requirements. Developer should not assume that SVP will be removing any existing facilities without detailed design drawings from SVP indicating potential removals. *Simply indicating that SVP facilities are to be removed or relocated on conceptual plans does not imply that this action has been approved by SVP.*
- SVP16. All interior meter rooms are to have direct, outside access through only ONE door. Interior electric rooms must be enclosed in a dedicated electric room and cannot be in an open warehouse or office space.
- SVP17. Applicant is advised to contact SVP (CSC Electric Department) to obtain specific design and utility requirements that are required for building permit review/approval submittal. Please provide a site plan to Leonard Buttitta at 408-615-6620 to facilitate plan review.

WATER & SEWER

- W1. Prior to issuance of Building Permits, the applicant shall submit design plans for construction of water utilities that comply with the latest edition of the Water & Sewer Utilities Water Service and Use Rules and Regulations, Water System Notes, and Water Standard Details and Specifications. In addition, prior to the City's issuance of Occupancy, the applicant shall construct all public water utilities per the approved plans. The Water & Sewer Utilities will inspect all public water utility installations and all other improvements encroaching public water utilities.
- W2. Prior to City's issuance of Building or Grading Permits, the applicant shall provide a dedicated water utility easement around onsite public water appurtenances. The water utility easement for the water services and all other public water appurtenances shall be a minimum 15 feet wide and be adjacent to the public right-of-way without overlapping any public utility easement. Additionally, the applicant shall submit plans defining existing easements so Water Division can verify if there are any conflicts with proposed easements and water utilities.
- W3. Prior to issuance of Building Permits, the applicant shall provide the profile section details for utilities crossing water, sewer, or recycled water mains to ensure a 12" minimum vertical clearance is maintained. The applicant must indicate the disposition of all existing water and sewer services and mains on the plans. If the existing services will not be used, then the applicant shall properly abandon these services to the main per Water & Sewer Utilities standards and install a new service to accommodate the water needs of the project.
- W4. The applicant shall contact the Water & Sewer Utilities Department, for fire flow information at (408) 615-2000.
- W5. The applicant must indicate the pipe material and the size of existing water and sewer main(s) on the plans.
- W6. Applicant shall utilize one of the two existing water service for dedicated irrigation service, if possible.
- W7. Water Dept. Standard Details and Notes shall be included in design drawings.
- W8. Applicant shall adhere to and provide a note indicating all horizontal and vertical clearances. The applicant shall maintain a minimum 12" of vertical clearance at water service crossing with other utilities, and all required minimum horizontal clearances from water services: 10' from sanitary sewer utilities, 10' from recycled water utilities, 8' from storm drain utilities, 5' from fire and other water utilities, 3' from abandoned water services, 5' from gas utilities, and 5' from the edge of the propose or existing driveway. For sanitary sewer, water, and recycled water utilities, the applicant shall maintain a minimum horizontal

clearance of 10' from existing and proposed trees. If applicant installs tree root barriers, clearance from tree reduces to 5' (clearance must be from the edge of tree root barrier to edge of water facilities).

- W9. Applicant shall reconcile landscaping and civil design drawings to match, landscaping drawings point of connection is at the existing water service and civil drawings call out for new irrigation water service installation.
- W10. The applicant shall submit a composite utility plan showing all utilities (including electrical) and landscaping (trees/shrubbery) so that the Water Department can verify conflicts for proposed water services. Note that all new water meters and backflow prevention devices shall be located behind the sidewalk in a landscape area.
- W11. The applicant must indicate the services to be connected to the 10" CIP on Pomeroy Avenue instead of the 8" CIP main.
- W12. The applicant to explore installing meters and backflow preventer devices in the landscaping strip next to the curb.
- W13. The applicant shall submit plans for independent water service to each individual residential unit, and a single service with private submeters for the attached units, connected to a public main in the public right-of-way to the satisfaction of the Director of Water & Sewer Utilities. Individual water services to single family residential shall be 1" water services. The applicant must coordinate and obtain Water Department approval to install a single service connection split into multiple meters.

PROPOSED RESIDENTIAL PROJECT AT: 1530 / 1540 POMEROY AVE. SANTA CLARA, CA

REVISIONS	BY

CB
BASSAL
Architecture
916.435.0605
408.674.9077



SITE PLAN

RESIDENTIAL PROJECT AT:
1530 / 1540 POMEROY AVE.
SANTA CLARA, CA

DATE:	5-5-2010
SCALE:	
DRAWN:	CB
JOB NO.:	14-345
SHEET NO.:	A11

RIGDECREST GROUP, INC
OMID SHAKERI (408) 666-6556
1530 / 1540 POMEROY AVE.
SANTA CLARA, CA
APN: 290-02-097 & 096

OCCUPANCY TYPE: R-3/U
CONSTRUCTION TYPE: V-B

SHEET INDEX

- A1.1 PROJECT DATA
- A1.2 SITE PLAN

1530 POMEROY AVE.

- A2.1 TYPICAL FLOOR PLANS (LARGE)
- A2.2 FIRST & SECOND FLOOR PLANS
- A2.3 THIRD FLOOR PLAN / ROOF PLAN
- A3.1 EXTERIOR ELEVATIONS
- A3.2 EXTERIOR ELEVATIONS

1540 POMEROY AVE.

- B2.1 TYPICAL FLOOR PLANS
- B3.1 EXTERIOR ELEVATIONS

- C1 CIVIL COVER SHEET
- C2 UTILITY PLAN
- C3 GRADING & DRAINAGE
- C4 STORM WATER CONTROL PLAN
- C5 STORM DRAIN PLAN
- C6 DETAILS
- TM TENTATIVE PLAN

AUTOMATIC RESIDENTIAL FIRE SPRINKLERS
TO BE INSTALLED (UNDER SEPARATE PERMIT)

1530 & 1540 POMEROY AVE.

LOT SIZE: 21,000 S.F.	PROPOSED
LOT COVERAGE	7,810 S.F. (37.2%)
LANDSCAPE COVERAGE	8,500 S.F. (40.5%)

1530 POMEROY AVE.

R3-18D
LOT SIZE: 10,080 S.F.

	PROPOSED	REQUIRED
LOT COVERAGE	2,880 S.F. (28.5%)	35% MAX.
LANDSCAPE COVERAGE	4,130 S.F. (41%)	40% MIN.
BUILDING HEIGHT	24'-8"	25'-0" MAX.

PROJECT SCOPE:
2 SINGLE FAMILY DTACHE HOMES WITH 2 CARS GARAGE EACH
3 GUEST PARKING
OPEN LANDSCAPE / COMMON SPACE
EACH HOUSE: 2,165 SQ FT. LIVING SPACE:
420 SQ .FT. GARAGE

1540 POMEROY AVE.

R3-36D
LOT SIZE: 10,920 S.F.

	PROPOSED	REQUIRED
LOT COVERAGE	4,914 S.F. (45%)	45% MAX.
LANDSCAPE COVERAGE	4,370 S.F. (40%)	40% MIN.
BUILDING HEIGHT	36'-6"	45'-0" MAX.

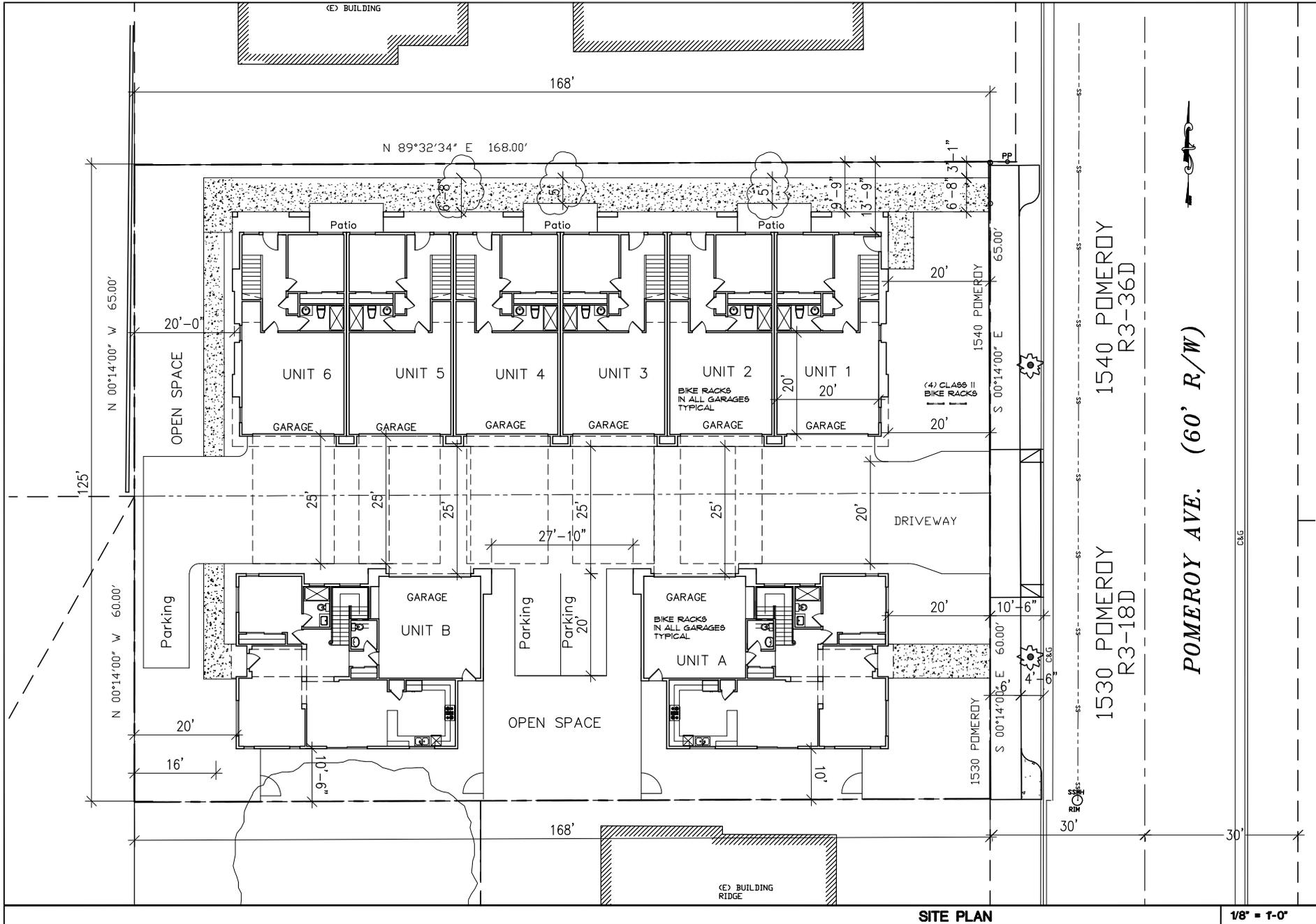
PROJECT SCOPE:
6 TWO STORY UNITS WITH ATTACHED 2 CAR GARAGE EACH
EACH UNIT: 1,970 SQ FT. LIVING SPACE:
400 SQ .FT. GARAGE



VICINITY MAP

PROJECT SUMMARY

SHEET INDEX



SITE PLAN

1/8" = 1'-0"

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408.674.9077

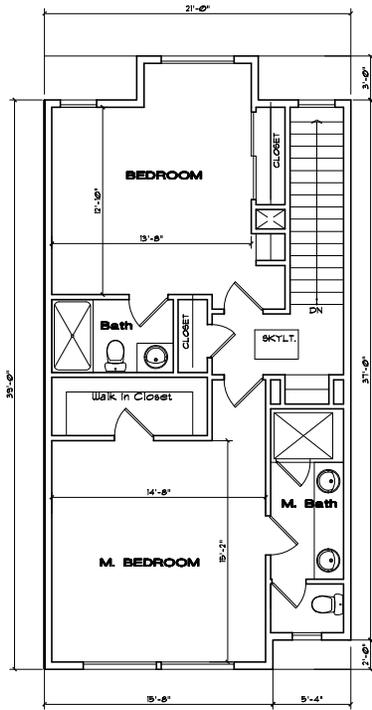
SITE PLAN

RESIDENTIAL PROJECT AT:
1530 / 1540 POMEROY AVE.
SANTA CLARA, CA

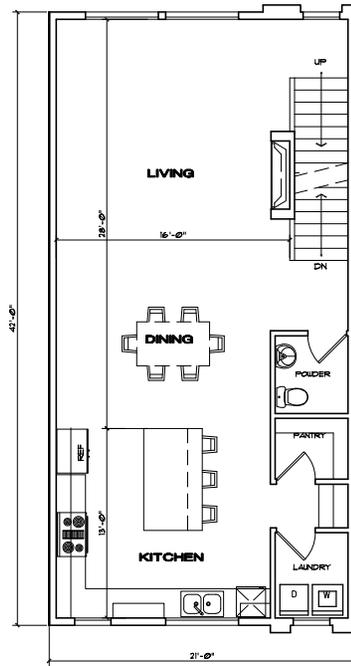
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DRAWN:	CB
JOB NO:	
SHEET NO.:	A1.2

of SHEETS

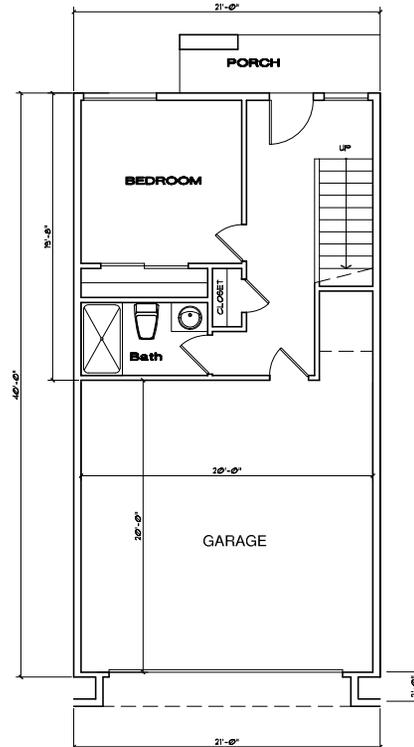
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THIRD FLOOR: 750 S.F.



SECOND FLOOR: 820 S.F.



FIRST FLOOR: 400 SQ.FT.
GARAGE: 400 S.F.

1540 POMEROY
TOTAL LIVING SPACE: 1,970 SQ.FT.
UNITS 1 TO 6

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 408.674.9077

1640 POMEROY AVE.
UNITS 1 TO 6 PLANS

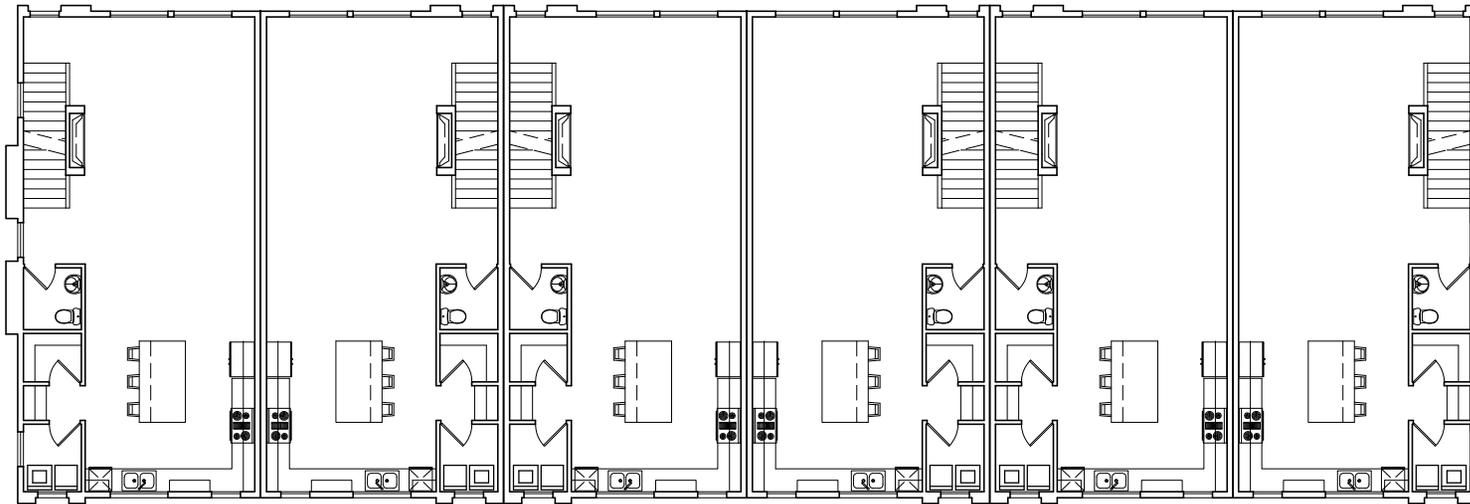
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1640 POMEROY AVE.
SANTA CLARA, CA

DATE:	1-21-2000
SCALE:	NOTED
DRAWN:	CB
JOB NO:	1
SHEET NO.:	A2.1
of SHEETS	

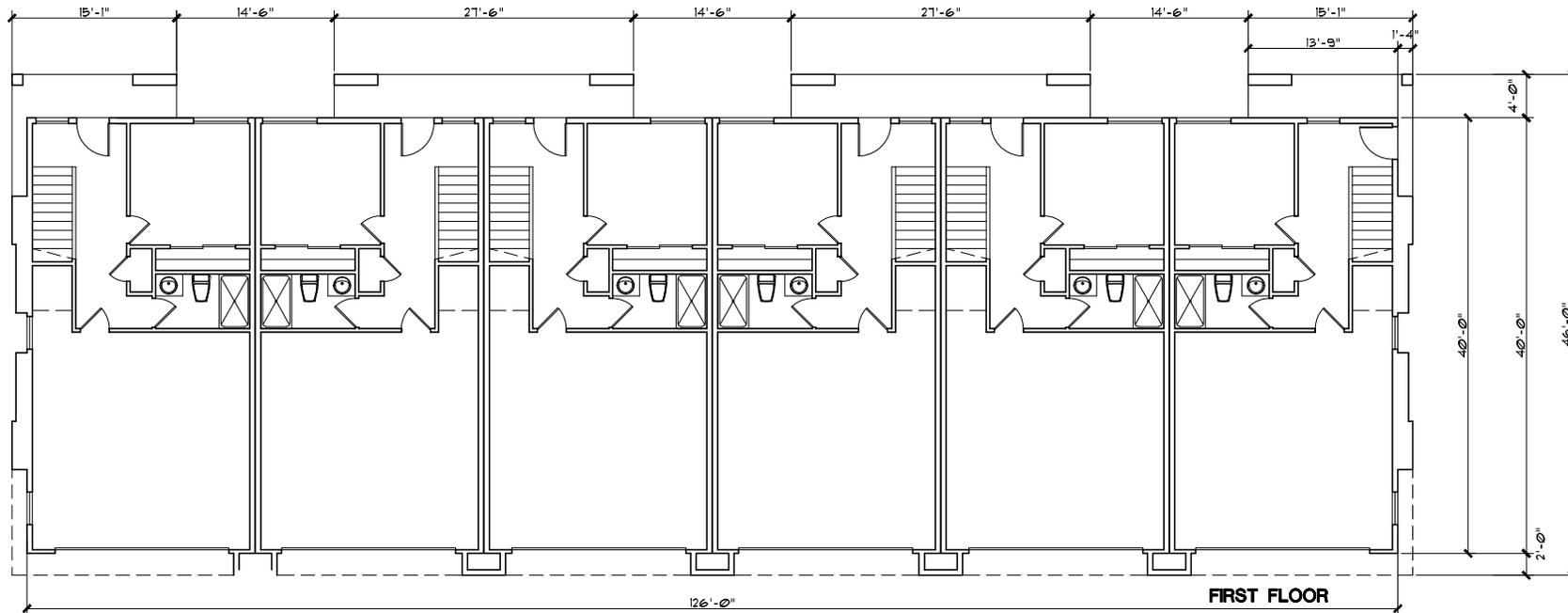
TYPICAL FLOOR PLAN

1/4" = 1'-0"

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SECOND FLOOR



FIRST FLOOR

126'-0"

FLOOR PLANS

1/4" = 1'-0"

REVISIONS

NO.	DESCRIPTION	DATE	BY



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**1540 POMEROY AVE.
 FLOOR PLANS**

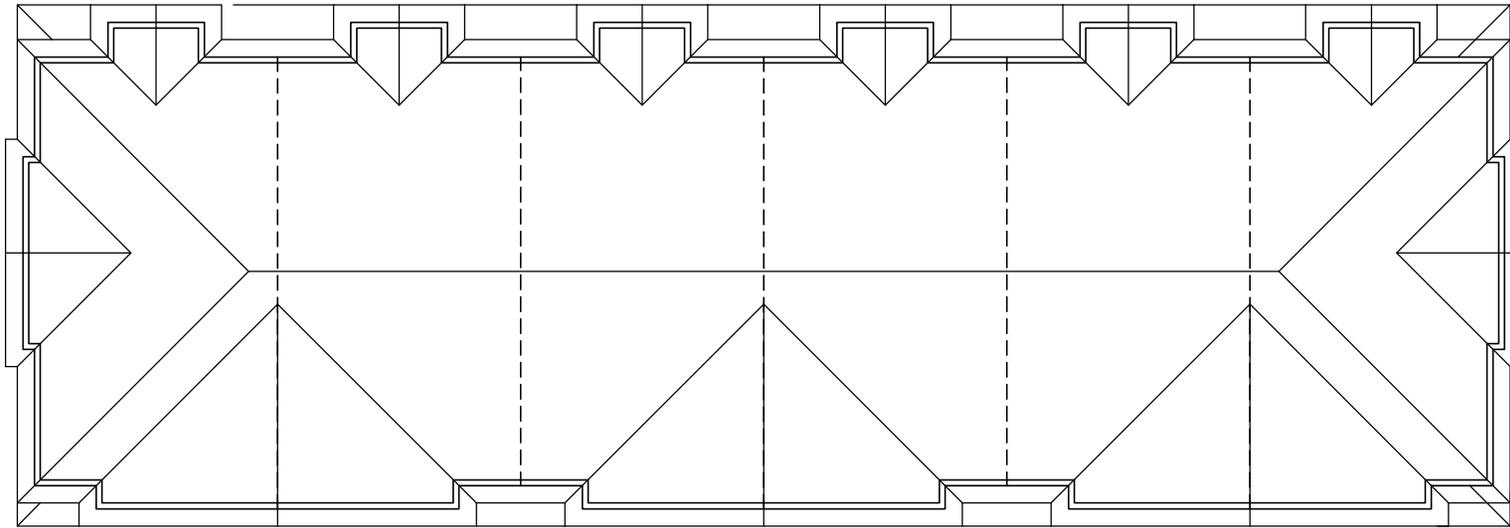
**RESIDENTIAL PROJECT AT:
 1540 POMEROY AVE.
 SANTA CLARA, CA**

DATE: 1-21-2009
 SCALE: NOTED
 DRAWN: CB
 JOB NO: 1

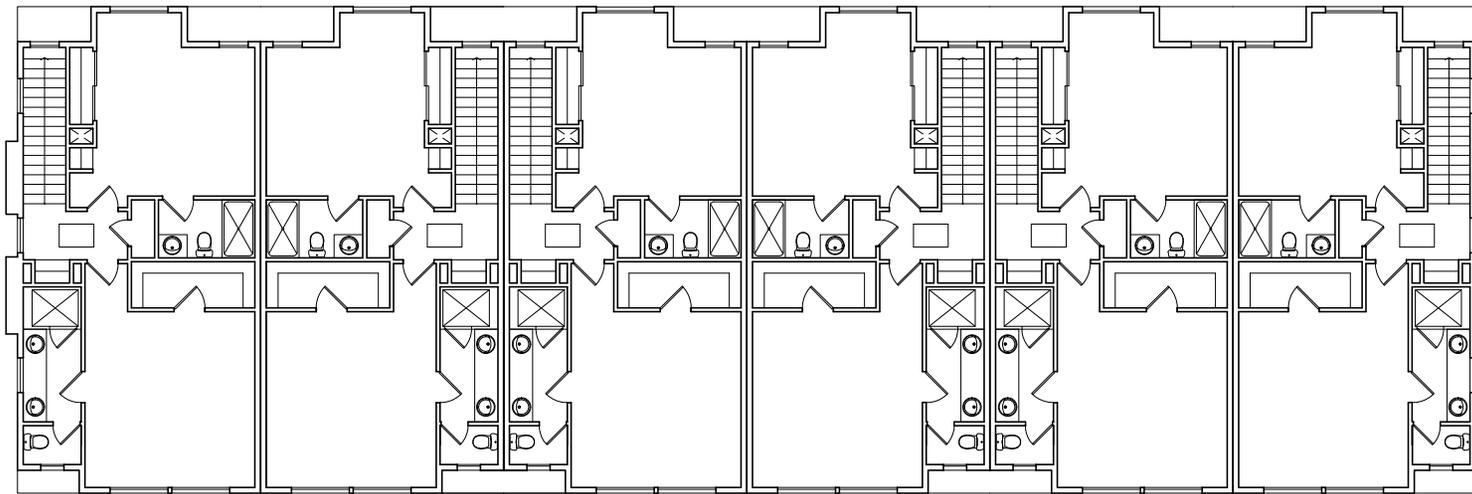
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ROOF PLAN



THIRD FLOOR

FLOOR PLANS / ROOF PLAN

1/4" = 1'-0"

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1540 POMEROY AVE
FLOOR PLANS
ROOF PLAN

RESIDENTIAL PROJECT AT:
1540 POMEROY AVE
SANTA CLARA, CA

DATE: 1-21-2009
 SCALE: NOTED
 DRAWN: CB
 JOB NO: 1

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ENTRANCE SIDE ELEVATIONS - NORTH



FRONT ELEVATIONS - EAST - POMEROY VIEW

REVISIONS	BY



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 408.674.9077

1640 POMEROY AVE.
 ELEVATIONS

RESIDENTIAL PROJECT AT:
 1640 POMEROY AVE.
 SANTA CLARA, CA

DATE:	5-5-2020
SCALE:	NOTED
DRAWN:	CB
JOB NO:	

SHEET NO.
A3.1

ELEVATIONS

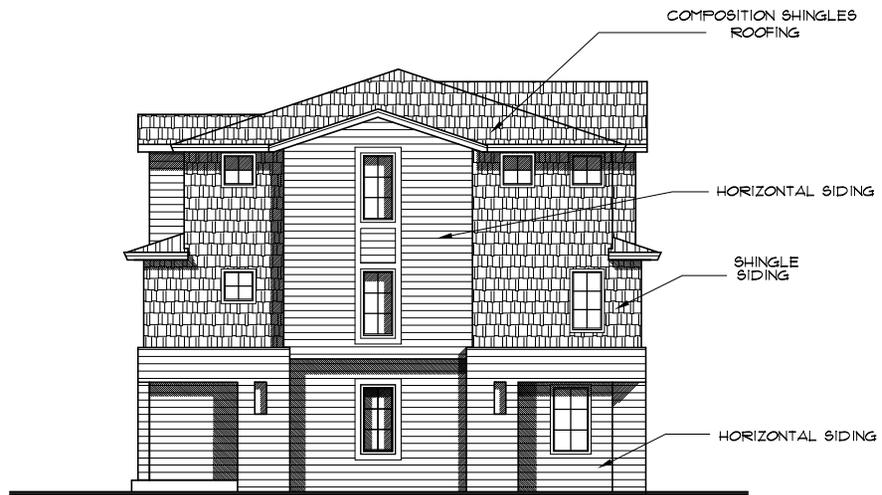
1/4" = 1'-0"

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1540 POMEROY AVE.
DRIVEWAY ELEVATIONS - SOUTH



REAR ELEVATIONS - WEST

ELEVATIONS

1/4" = 1'-0"

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1540 POMEROY AVE
 ELEVATIONS

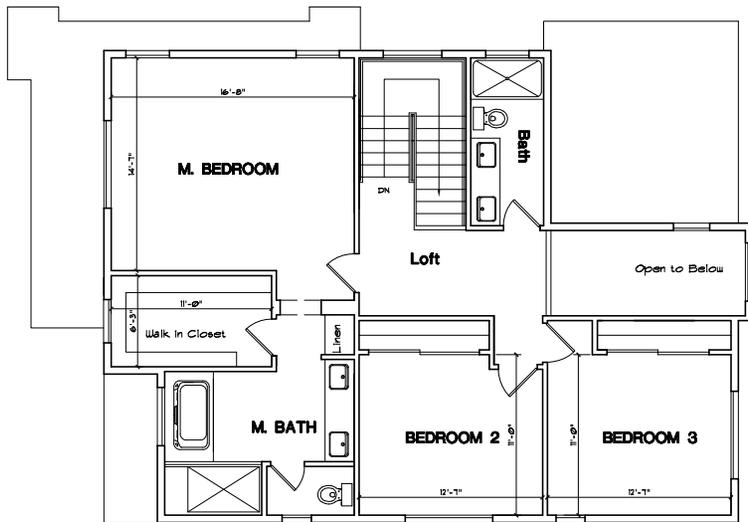
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 1540 POMEROY AVE
 SANTA CLARA, CA

DATE: 5-5-2020
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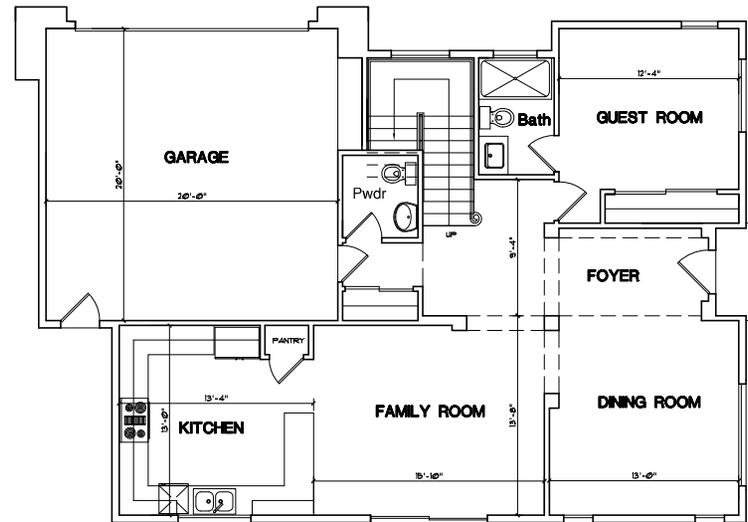
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SECOND FLOOR
LIVING AREA: 1,040 SQ.FT.



FIRST FLOOR
LIVING AREA: 1,125 SQ.FT.
GARAGE: 420 S.F.

1530 POMEROY
FLOOR PLAN
TOTAL LIVING SPACE: 2,165 SQ.FT.

REVISIONS	BY



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1530 POMEROY
FLOOR PLANS

RESIDENTIAL PROJECT AT:
1530 POMEROY AVE
SANTA CLARA, CA

DATE:	1-21-2009
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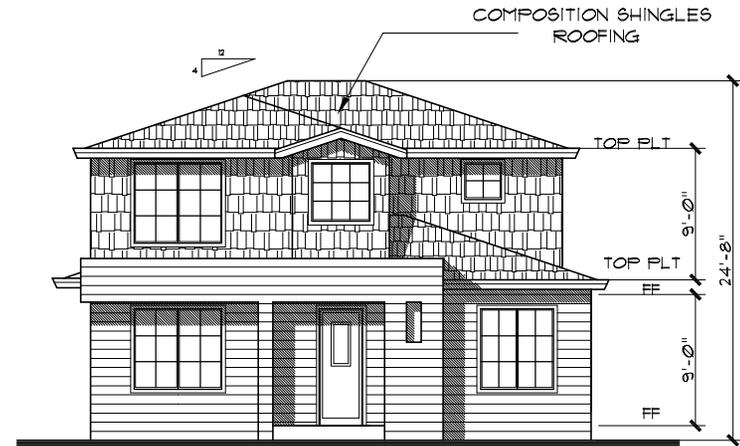
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of SHEETS



DRIVEWAY ELEVATIONS - NORTH

HORIZONTAL SIDING

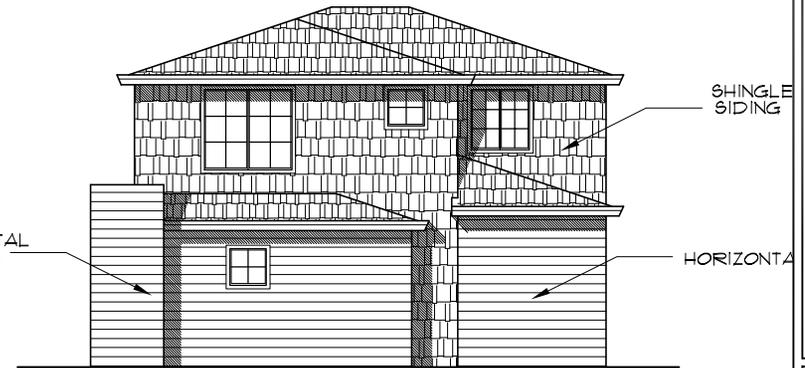


FRONT ELEVATION - EAST - POMEROY VIEW

1530 POMEROY AVE.



REAR ELEVATION - SOUTH



SIDE ELEVATION - WEST

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1530 POMEROY
 PLAN B

RESIDENTIAL PROJECT AT:
 1530 POMEROY AVE
 SANTA CLARA, CA

DATE:	9-5-2020
SCALE:	
DRAWN:	CB
JOB NO:	
SHEET NO.:	B3.1
of SHEETS	

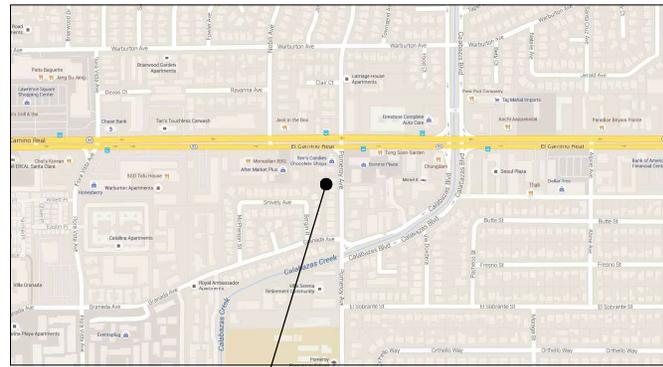
ABBREVIATIONS																																																																																																																			
DESCRIPTION	DESCRIPTION																																																																																																																		
AB	AGGREGATE BASE																																																																																																																		
AC	ASPHALT CONCRETE																																																																																																																		
AD	AREA DRAIN																																																																																																																		
BC	BACK OF CURB																																																																																																																		
BFL	BACKFLOW PREVENTOR																																																																																																																		
BW	BOTTOM OF WALL																																																																																																																		
C&G	CURB AND GUTTER <tr><td>C/L</td><td>CENTERLINE</td></tr> <tr><td>CSW</td><td>CENTRINE SWALE</td></tr> <tr><td>CO</td><td>CLEANOUT</td></tr> <tr><td>CP</td><td>CONTROL POINT</td></tr> <tr><td>DI</td><td>DRAIN INLET</td></tr> <tr><td>D-S</td><td>DOWN-SPOUT</td></tr> <tr><td>DITL</td><td>DETAIL</td></tr> <tr><td>DWY</td><td>DRIVEWAY</td></tr> <tr><td>ELECT</td><td>ELECTRIC</td></tr> <tr><td>EP</td><td>EDGE OF PAVEMENT ELEVATION</td></tr> <tr><td>EUC</td><td>EUCALYPTUS TREE</td></tr> <tr><td>ELEX</td><td>ELECTRIFYING</td></tr> <tr><td>EP</td><td>EDGE OF PAVEMENT ELEVATION</td></tr> <tr><td>FT</td><td>FINISH FLOOR</td></tr> <tr><td>FG</td><td>FINISH GRADE</td></tr> <tr><td>FH</td><td>FIRE HYDRANT</td></tr> <tr><td>FL</td><td>FLOWLINE</td></tr> <tr><td>FNC</td><td>FENCE</td></tr> <tr><td>FCC</td><td>FACE OF CURB</td></tr> <tr><td>GB</td><td>GRADE BREAK</td></tr> <tr><td>GU</td><td>GUY WIRE</td></tr> <tr><td>HP</td><td>HIGH POINT</td></tr> <tr><td>DP</td><td>DUCTILE IRON PIPE</td></tr> <tr><td>INV</td><td>INVERT</td></tr> <tr><td>JP</td><td>JOINT POLE</td></tr> <tr><td>JB</td><td>JUNCTION BOX (UTILITY)</td></tr> <tr><td>LP</td><td>LP OF GUTTER</td></tr> <tr><td>LNDG</td><td>LANDING</td></tr> <tr><td>L/P</td><td>LANDSCAPE MONUMENT</td></tr> <tr><td>M</td><td>MONUMENT</td></tr> <tr><td>N</td><td>NEW</td></tr> <tr><td>OR</td><td>OVERLAND RELEASE</td></tr> <tr><td>POV</td><td>POLE VAULT</td></tr> <tr><td>PP</td><td>PROPERTY LINE</td></tr> <tr><td>PPA</td><td>POWER POLE</td></tr> <tr><td>PSE</td><td>PLASTIC PERFORATED PIPE</td></tr> <tr><td>PVC</td><td>POLYVINYL CHLORIDE</td></tr> <tr><td>R/W</td><td>RIGHT OF WAY</td></tr> <tr><td>RCP</td><td>REINFORCED CONCRETE PIPE</td></tr> <tr><td>SB</td><td>SEWER</td></tr> <tr><td>SD</td><td>STORM DRAIN</td></tr> <tr><td>SDMH</td><td>STORM DRAIN MANHOLE</td></tr> <tr><td>STD</td><td>STANDARD</td></tr> <tr><td>SS</td><td>SANITARY SEWER</td></tr> <tr><td>SSMH</td><td>SANITARY SEWER MANHOLE</td></tr> <tr><td>SW</td><td>SIDEWALK</td></tr> <tr><td>TC</td><td>TOP OF CURB</td></tr> <tr><td>TF</td><td>TOP OF FOUNDATION</td></tr> <tr><td>TG</td><td>TOP OF GRADE</td></tr> <tr><td>TOS</td><td>TOP OF SLAB</td></tr> <tr><td>TP</td><td>TOP OF PAVEMENT</td></tr> <tr><td>TR</td><td>TYPICAL</td></tr> <tr><td>VCP</td><td>VITRIFIED CLAY PIPE</td></tr> <tr><td>W</td><td>WHITE LINE STRIPE</td></tr> <tr><td>WALK</td><td>WALKWAY</td></tr> <tr><td>WM</td><td>WATER METER</td></tr> <tr><td>WV</td><td>WATER VALVE</td></tr>	C/L	CENTERLINE	CSW	CENTRINE SWALE	CO	CLEANOUT	CP	CONTROL POINT	DI	DRAIN INLET	D-S	DOWN-SPOUT	DITL	DETAIL	DWY	DRIVEWAY	ELECT	ELECTRIC	EP	EDGE OF PAVEMENT ELEVATION	EUC	EUCALYPTUS TREE	ELEX	ELECTRIFYING	EP	EDGE OF PAVEMENT ELEVATION	FT	FINISH FLOOR	FG	FINISH GRADE	FH	FIRE HYDRANT	FL	FLOWLINE	FNC	FENCE	FCC	FACE OF CURB	GB	GRADE BREAK	GU	GUY WIRE	HP	HIGH POINT	DP	DUCTILE IRON PIPE	INV	INVERT	JP	JOINT POLE	JB	JUNCTION BOX (UTILITY)	LP	LP OF GUTTER	LNDG	LANDING	L/P	LANDSCAPE MONUMENT	M	MONUMENT	N	NEW	OR	OVERLAND RELEASE	POV	POLE VAULT	PP	PROPERTY LINE	PPA	POWER POLE	PSE	PLASTIC PERFORATED PIPE	PVC	POLYVINYL CHLORIDE	R/W	RIGHT OF WAY	RCP	REINFORCED CONCRETE PIPE	SB	SEWER	SD	STORM DRAIN	SDMH	STORM DRAIN MANHOLE	STD	STANDARD	SS	SANITARY SEWER	SSMH	SANITARY SEWER MANHOLE	SW	SIDEWALK	TC	TOP OF CURB	TF	TOP OF FOUNDATION	TG	TOP OF GRADE	TOS	TOP OF SLAB	TP	TOP OF PAVEMENT	TR	TYPICAL	VCP	VITRIFIED CLAY PIPE	W	WHITE LINE STRIPE	WALK	WALKWAY	WM	WATER METER	WV	WATER VALVE
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EASEMENT ABBREVIATIONS:

EVAE	EMERGENCY VEHICLE ACCESS EASEMENT
FLE	FIRE LADDER EASEMENT
PIE	PRIVATE INGRESS/ EGRESS EASEMENT
PSDE	PRIVATE STORM DRAINAGE EASEMENT
PSSE	PRIVATE SANITARY SEWER EASEMENT
PWE	PRIVATE WATER EASEMENT
SE	SIDEWALK EASEMENT
UGEZ	UNDERGROUND ELECTRICAL EASEMENT
WLE	WATER LINE EASEMENT

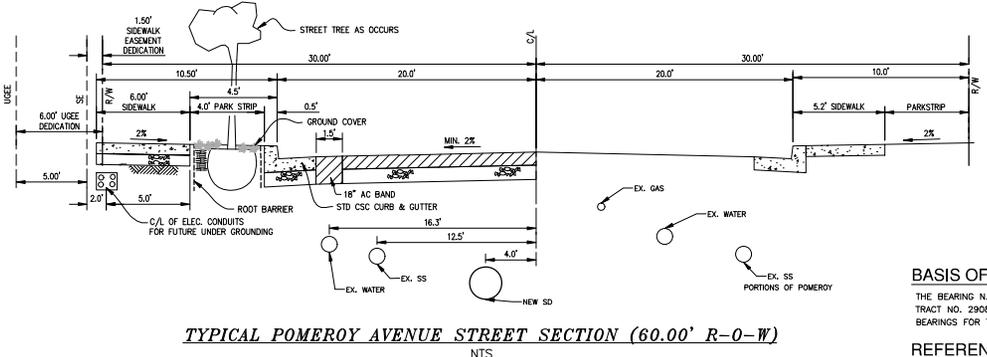
IMPROVEMENT PLANS

ONE LOT SUBDIVISION FOR CONDOMINIUM PURPOSES
EIGHT (8) RESIDENTIAL CONDOMINIUM UNITS
1530/1540 POMEROY AVE. SANTA CLARA, CA 95051

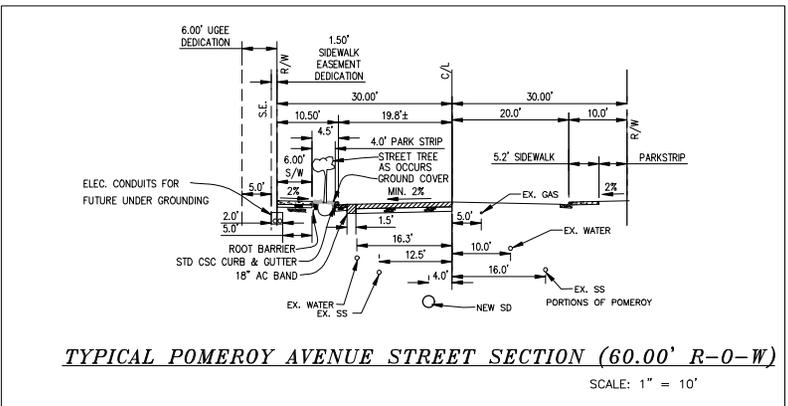


PROJECT SITE LOCATION MAP N.T.S.

EXISTING	PROPOSED	DESCRIPTION
---	---	STREET CENTER LINE
---	---	DISTINCTIVE BORDER LINE
---	---	EASEMENT LINE
---	---	LOT LINE
---	---	BUILDING FOOTPRINT
F	F	FILL AREA LIMIT
C	C	CUT AREA LIMIT
---	---	CONTOUR
---	---	WATER LINE
SD	SD	STORM DRAIN PIPE (SOLID)
SS	SS	SANITARY SEWER PIPE
SUD	SUD	SUBDRAIN PIPE (PERFORATED)
OH	OH	OVERHEAD UTILITIES WITH POLE
G	G	GAS LINE
E	E	ELECTRIC LINE (UNDERGROUND)
JT	JT	JOINT TRENCH (UNDERGROUND)
SLV	SLV	STREET LIGHT VAULT
SSCO	SSCO	SANITARY SEWER CLEANOUT
SSMH	SSMH	SANITARY SEWER MANHOLE
SDMH	SDMH	STORM DRAIN MANHOLE
SM	SM	SURVEY CITY MONUMENT
EL	EL	ELECTROLIER
WM	WM	WATER METER
T	T	TREE WITH TRUNK
ST	ST	STREET TREE
WF	WF	6' WOODEN FENCE
SE	SE	SPOT ELEVATION
TP	TP	TREE PROTECTION FENCE
TL	TL	5' TALL CHAIN LINK
ES	ES	EARTHSWALE
CS	CS	CONCRETE SWALE
IB	IB	INLET/ JUNCTION BOX
AD	AD	AREA DRAIN
OR	OR	OVERLAND RELEASE PATH
GD	GD	GRADE TO DRAIN, 2% MIN. AWAY FROM HOUSE
1%GD	1%GD	1% MIN. FROM PROPERTY LINE TO SWALE
TR	TR	(E) TREE TO BE REMOVE
D-S	D-S	DOWN-SPOUT



TYPICAL POMEROY AVENUE STREET SECTION (60.00' R-O-W) N.T.S.



TYPICAL POMEROY AVENUE STREET SECTION (60.00' R-O-W) SCALE: 1" = 10'

SHEET INDEX:

C-1	COVER SHEET
C-2	PRELIMINARY UTILITY PLAN
C-3	PRELIMINARY GRADING AND DRAINAGE PLAN
C-4	PRELIMINARY STORMWATER CONTROL PLAN
C-5	STORM DRAIN MAIN IN POMEROY
C-6	DETAILS, CROSS SECTIONS

BASIS OF BEARINGS:
THE BEARING N. 0°14'00" W. OF THE CENTERLINE OF POMEROY AVE., AS SHOWN UPON CERTAIN TRACT NO. 2908, RECORDED IN BOOK 140 OF MAPS AT PAGE 31, WAS TAKEN AS BASIS OF BEARINGS FOR THIS SURVEY MAP.

REFERENCED CITY OF SANTA CLARA B.M.
BM # E-13 CHISLED CROSS ON SE BOLT IN TRAFFIC LIGHT BASE, AT NW CORNER OF EL CAMINO REAL & POMEROY AVE. EL: 91.06' (NAVD8)

APPROVED: Rajeev Batra
Director of Public Works/ City Engineer, City of Santa Clara

Engineering Department _____ Date _____

REVIEWED: Land and Property Development Division _____ Date _____

Water and Sewer Utilities _____ Date _____

Street Department _____ Date _____

Silicon Valley Power _____ Date _____

Traffic Division _____ Date _____



Appoint: ECCO Builders, Inc.
Ornid Shakeri
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IMPROVEMENT PLANS
NINE LOT SUBDIVISION
1530 and 1540 POMEROY AVE.
SANTA CLARA, CA 95051
COVER SHEET

Revisions:

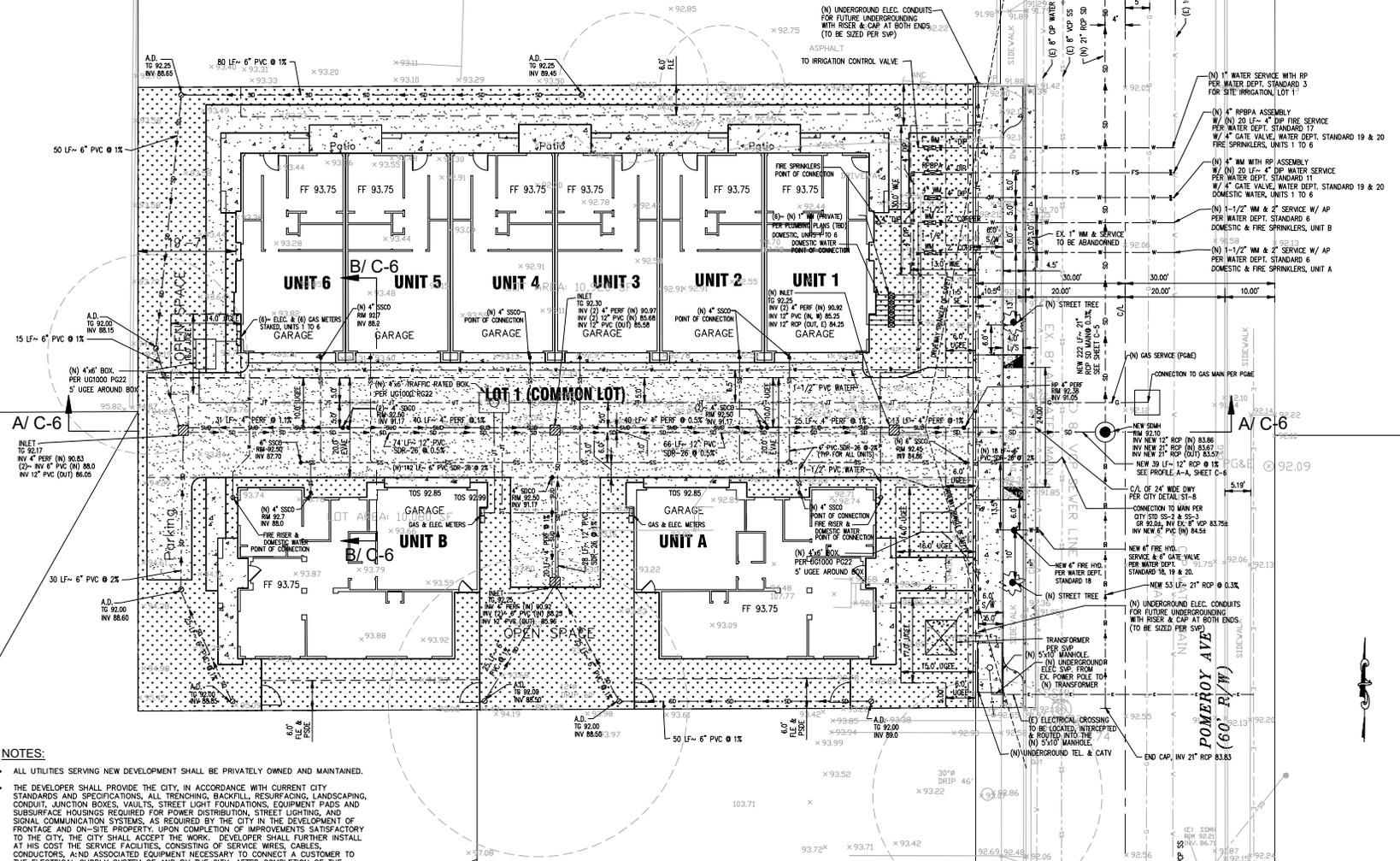
Date: 10/21/2020
Scale:
Prepared by: V.G.
Checked by: S.R.
Job #: 220050
Sheet: 1 OF 6
C-1

WATER KEY NOTES:

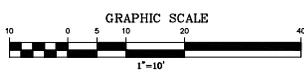
- INSTALL 4" NEW 1" WM WITH 4" NEW 1" WATER SERVICE PER CITY WATER DEPARTMENT STANDARD NO. 3, BRANCHED FROM NEW 4" DP WATER LATERAL, PER CITY WATER DEPARTMENT STANDARD NO. 11, BANKED IN A 8-40 UTILITY (20" X 60") BOX FOR LOTS DOMESTIC USE, & FIRE SPRINKLERS UNIT A AND B. LOCATE THE WMS IN THE PARK STRIP. INSTALL 4" GATE VALVE FOR 4" DP WATER SERVICE PER CITY WATER DEPARTMENT STANDARD NO. 19 & 20.
- FOR LOT 1 COMMON LANDSCAPE IRRIGATION WM, INSTALL (N) 1" WATER SERVICE WITH RP, PER CITY WATER DEPARTMENT STANDARD NO. 3. LOCATE THE WM & VALVE BOX AT BACK OF 6.0' USE, INSIDE 15.0' WIDE PWE.
- INSTALL NEW 4" DP FIRE SERVICE AND (N) 4" RBPBA ASSEMBLY PER WATER DEPT. STANDARD 17, WITH 4" GATE VALVE PER CITY WATER DEPARTMENT STANDARD NO. 19 & 20. INSTALL TRUST BLOCK & TUBS PER CITY WATER DEPARTMENT STANDARD NO. 20. LOCATE THE 4" RBPBA ASSEMBLY AT BACK OF 6.0' USE, INSIDE 15.0' WIDE PWE.

WATER CLEARANCE NOTE:

THE APPLICANT SHALL ADHERE TO AND PROVIDE A NOTE INDICATING ALL HORIZONTAL AND VERTICAL CLEARANCES. THE APPLICANT SHALL MAINTAIN A MINIMUM 12" OF VERTICAL CLEARANCE AT WATER SERVICE CROSSING WITH OTHER UTILITIES, AND ALL REQUIRED MINIMUM HORIZONTAL CLEARANCES FROM WATER SERVICES: 10' FROM SANITARY SEWER UTILITIES, 10' FROM RECYCLED WATER UTILITIES, 8' FROM STORM DRAIN UTILITIES, 5' FROM FIRE AND OTHER WATER UTILITIES, 3' FROM ABANDONED WATER SERVICES, 5' FROM GAS UTILITIES, AND 5' FROM THE EDGE OF THE PROPOSED OR EXISTING DRIVEWAY. FOR SANITARY SEWER, WATER, AND RECYCLED WATER UTILITIES, THE APPLICANT SHALL MAINTAIN A MINIMUM HORIZONTAL CLEARANCE OF 10' FROM EXISTING AND PROPOSED TREES. IF APPLICANT INSTALLS TREE ROOT BARRIERS, CLEARANCE FROM TREE REDUCES TO 5' (CLEARANCE MUST BE FROM THE EDGE OF TREE ROOT BARRIER TO EDGE OF WATER FACILITIES).



- NOTES:**
- ALL UTILITIES SERVING NEW DEVELOPMENT SHALL BE PRIVATELY OWNED AND MAINTAINED.
 - THE DEVELOPER SHALL PROVIDE THE CITY, IN ACCORDANCE WITH CURRENT CITY STANDARDS AND SPECIFICATIONS, ALL TRENCHING, BACKFILL, RESURFACING, LANDSCAPING, CONDUIT, JUNCTION BOXES, VAULTS, STREET LIGHT FOUNDATIONS, EQUIPMENT PADS AND SUBSURFACE HOUSINGS REQUIRED FOR POWER DISTRIBUTION, STREET LIGHTING, AND SIGNAL COMMUNICATION SYSTEMS, AS REQUIRED BY THE CITY IN THE DEVELOPMENT OF FRONTAGE AND ON-SITE PROPERTY. UPON COMPLETION OF IMPROVEMENTS SATISFACTORY TO THE CITY, THE CITY SHALL ACCEPT THE WORK. DEVELOPER SHALL FURTHER INSTALL AT HIS COST THE SERVICE FACILITIES, CONSISTING OF SERVICE WIRES, CABLES, CONDUCTORS, AND ASSOCIATED EQUIPMENT NECESSARY TO CONNECT A CUSTOMER TO THE ELECTRICAL SUPPLY SYSTEM OF AND BY THE CITY. AFTER COMPLETION OF THE FACILITIES INSTALLED BY DEVELOPER, THE CITY SHALL FURNISH AND INSTALL ALL CABLE, SWITCHES, STREET LIGHTING POLES, LUMINAIRES, TRANSFORMERS, METERS, AND OTHER EQUIPMENT THAT IT DEEMS NECESSARY FOR THE BETTERMENT OF THE SYSTEM (SANTA CLARA CITY CODE CHAPTER 17.1S.210 (2)).



Applicant:
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IMPROVEMENT PLANS
NINE LOT SUBDIVISION
1530 AND 1540 POMEROY AVE
SANTA CLARA, CA 95051
PRELIMINARY UTILITY PLAN

Revisions:



Date: 10/21/2020
Scale: 1" = 10'
Prepared by: V.G.
Checked by: S.R.
Job #: 220050
Sheet: 2 OF 6

C-2



ENGINEERS
CIVIL ENGINEERS

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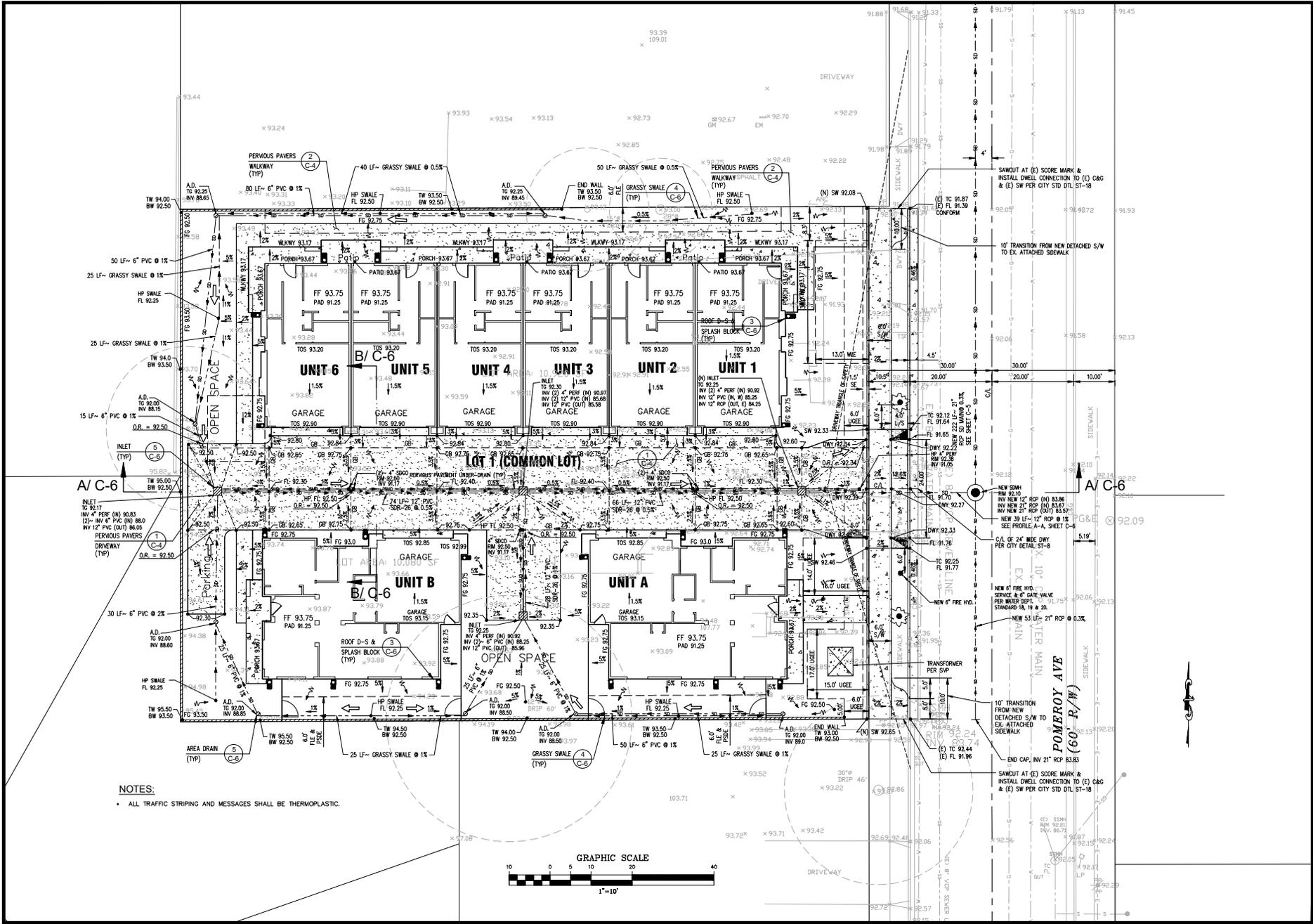
IMPROVEMENT PLANS
NINE LOT SUBDIVISION
1530 AND 1540 POMEROY AVE
SANTA CLARA, CA 95051
PRELIMINARY GRADING AND DRAINAGE PLAN

Revisions:

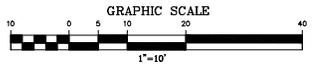


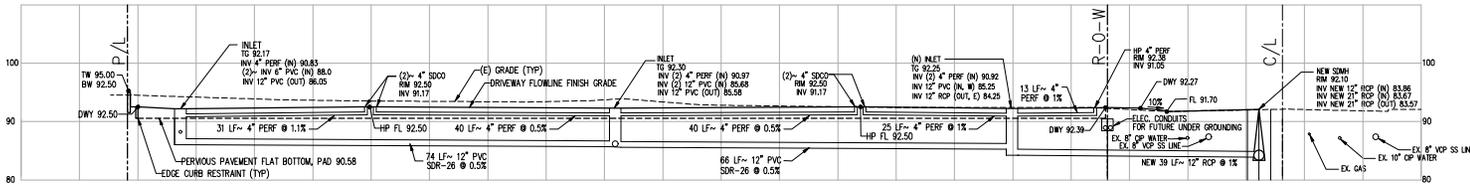
Date: 10/21/2020
Scale: 1" = 10'
Prepared by: V.G.
Checked by: S.R.
Job #: 220050

Sheet: **3 OF 6**
C-3

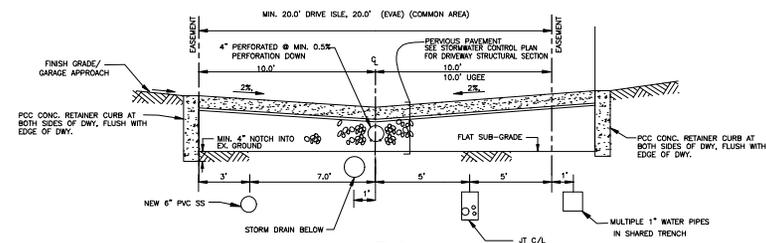


NOTES:
• ALL TRAFFIC STRIPING AND MESSAGES SHALL BE THERMOPLASTIC.

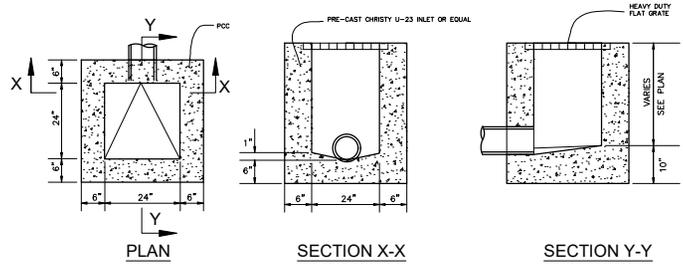




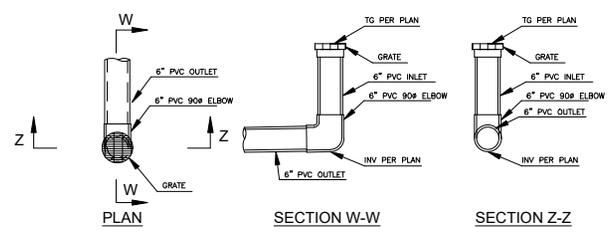
A-A
SCALE H: 1"=10'
SCALE V: 1"=10'



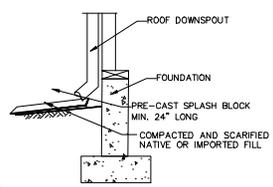
B-B
PRIVATE STREET TYPICAL SECTION
NTS



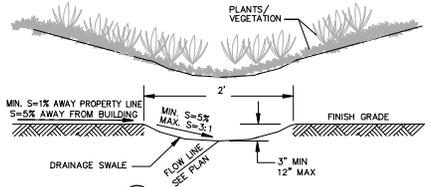
5 **STORM DRAIN INLET**
NTS



6 **STORM DRAIN AREA DRAIN**
NTS



3 **DISCONNECTED ROOF DOWN-SPOUT DETAIL**
NTS



4 **SWALE DETAIL**
NTS



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IMPROVEMENT PLANS
NINE LOT SUBDIVISION
1530 and 1540 POMEROY AVE.
SANTA CLARA, CA 95051
DETAILS, CROSS SECTIONS

Revisions:



Date: 10/21/2020
Scale: AS NOTED
Prepared by: V.G.
Checked by: S.R.
Job #: 220050

From: [HUANG LA](#)
To: [Elaheh Kerachian](#)
Subject: 1530-1540 Pomeroy Avenue Residential Project
Date: Monday, June 7, 2021 9:50:10 AM

Mayor and Members of the City Council and the Planning Commission.

I am Hung Le, 23-year home owner of 1436 Pomeroy Avenue Santa Clara, opposing the construction of multi-story townhomes at those addresses to replace current 2 single homes. The new construction would destroy our single-home neighborhood culture, impair the health, safety, and quality of life in our area and are inconsistent with the General plan and Regional Traffic objectives for the El Camino area.

Noises, parking, traffic and safety would be major concerns of our neighborhood living in this narrow strip of Pomeroy Avenue between Granada Avenue and El Camino Real.

Therefore please say NO to this new proposed construction.

Thank you

Hung Le



Sent from [Mail](#) for Windows 10

BASIS OF BEARINGS:
THE BEARING N. 01°40' W. OF THE CENTERLINE OF POMEROY AVE., AS SHOWN UPON CERTAIN TRACT NO. 2908, RECORDED IN BOOK 140 OF MAPS AT PAGE 31, WAS TAKEN AS BASIS OF BEARINGS FOR THIS SURVEY MAP.

REFERENCED CITY OF SANTA CLARA B.M.
BM # E-13 CHISEL CROSS ON SE BOLT IN TRAFFIC LIGHT B.M. AT NW CORNER OF EL CAMINO REAL & POMEROY AVE. EL: 91.00' (NAVD8)

AREA TABLE	AREA (SQFT.)	AREA (ACRES)
UNIT 1	863	0.020
UNIT 2	848	0.019
UNIT 3	848	0.019
UNIT 4	848	0.019
UNIT 5	848	0.019
UNIT 6	863	0.020
UNIT A	1,574	0.036
UNIT B	1,574	0.036
PRIVATE SPACE UNIT A	500	0.011
PRIVATE SPACE UNIT B	517	0.012
COMMON AREA	11,717	0.269
LOT 1 (COMMON LOT) (BOUNDARY)	21,000	0.482

LEGEND

- STREET CENTER LINE
- DISTINCTIVE BORDER LINE
- PROPOSED UNIT LINE
- EXISTING LOT LINE TO BE REMOVED
- EASEMENT LINE

ABBREVIATIONS

- EVAE EMERGENCY VEHICLE ACCESS EASEMENT
- FILE FIRE LADDER EASEMENT
- PIE PRIVATE INGRESS/ EGRESS EASEMENT
- PSDE PRIVATE STORM DRAINAGE EASEMENT
- PSSE PRIVATE SANITARY SEWER EASEMENT
- PWE PRIVATE WATER EASEMENT
- SE SIDEWALK EASEMENT
- UGE UNDERGROUND ELECTRICAL EASEMENT
- WLE WATER LINE EASEMENT

Tentative Map

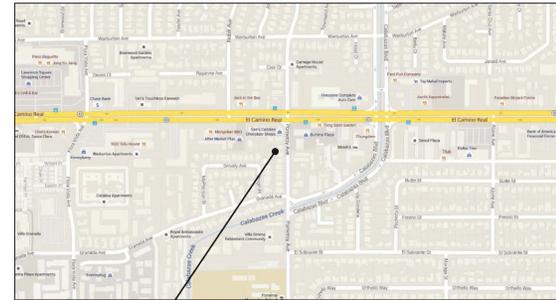
ONE LOT SUBDIVISION FOR CONDOMINIUM PURPOSES
EIGHT (8) RESIDENTIAL CONDOMINIUM UNITS

COMMENCING ON THE WESTERLY LINE OF POMEROY AVENUE, DISTANT THEREON 251.79 FEET SOUTHERLY FROM THE INTERSECTION OF SAID LINE OF POMEROY AVENUE WITH THE SOUTHERLY LINE OF SANTA CLARA-SAN FRANCISCO HIGHWAY, AND RUNNING THENCE SOUTHERLY ALONG SAID LINE OF POMEROY AVENUE 60 FEET; THENCE SOUTH 89° 45' EAST 173 FEET TO THE POINT OF BEGINNING AND PARALLEL WITH SAID LINE OF POMEROY AVENUE 60 FEET; THENCE NORTH 89° 45' EAST 173 FEET TO THE POINT OF BEGINNING AND BEGINNING AT THE POINT OF INTERSECTION OF THE SOUTHERLY LINE OF THE SANTA CLARA-SAN FRANCISCO HIGHWAY AS NOW EXISTING 100 FEET WIDE WITH THE WESTERLY LINE OF POMEROY AVENUE; THENCE RUNNING SOUTHERLY AND ALONG THE WESTERLY LINE OF POMEROY AVENUE 251.79 FEET TO THE TRUE POINT OF BEGINNING; THENCE, WESTERLY AND PARALLEL WITH SAID SOUTHERLY LINE OF SANTA CLARA-SAN FRANCISCO HIGHWAY 173 FEET TO A POINT; THENCE, NORTHERLY AND PARALLEL WITH THE WESTERLY LINE OF POMEROY AVENUE 65 FEET TO A POINT; THENCE, EASTERLY AND PARALLEL WITH THE SOUTHERLY LINE OF SANTA CLARA-SAN FRANCISCO HIGHWAY 173 FEET TO THE WESTERLY LINE OF POMEROY AVENUE; THENCE, SOUTHERLY AND ALONG SAID WESTERLY LINE OF POMEROY AVENUE, 65 FEET TO THE POINT OF BEGINNING, BEING A PORTION OF QUITO RANCHO.



LYING ENTIRELY WITHIN THE
CITY OF SANTA CLARA SANTA CLARA COUNTY STATE OF CALIFORNIA

OCTOBER 2020
SMP ENGINEERS
1534 CAROB LANE
LOS ALTOS, CA 94024



APPLICANT:
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SMP ENGINEERS
CIVIL ENGINEERS

TENTATIVE MAP
1530 and 1540 POMEROY AVE.
SANTA CLARA, CA 95051
APN# 290-02-096 and 290-02-097
TENTATIVE MAP

GENERAL NOTES AND STATEMENTS:

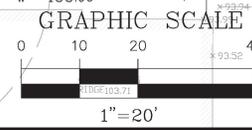
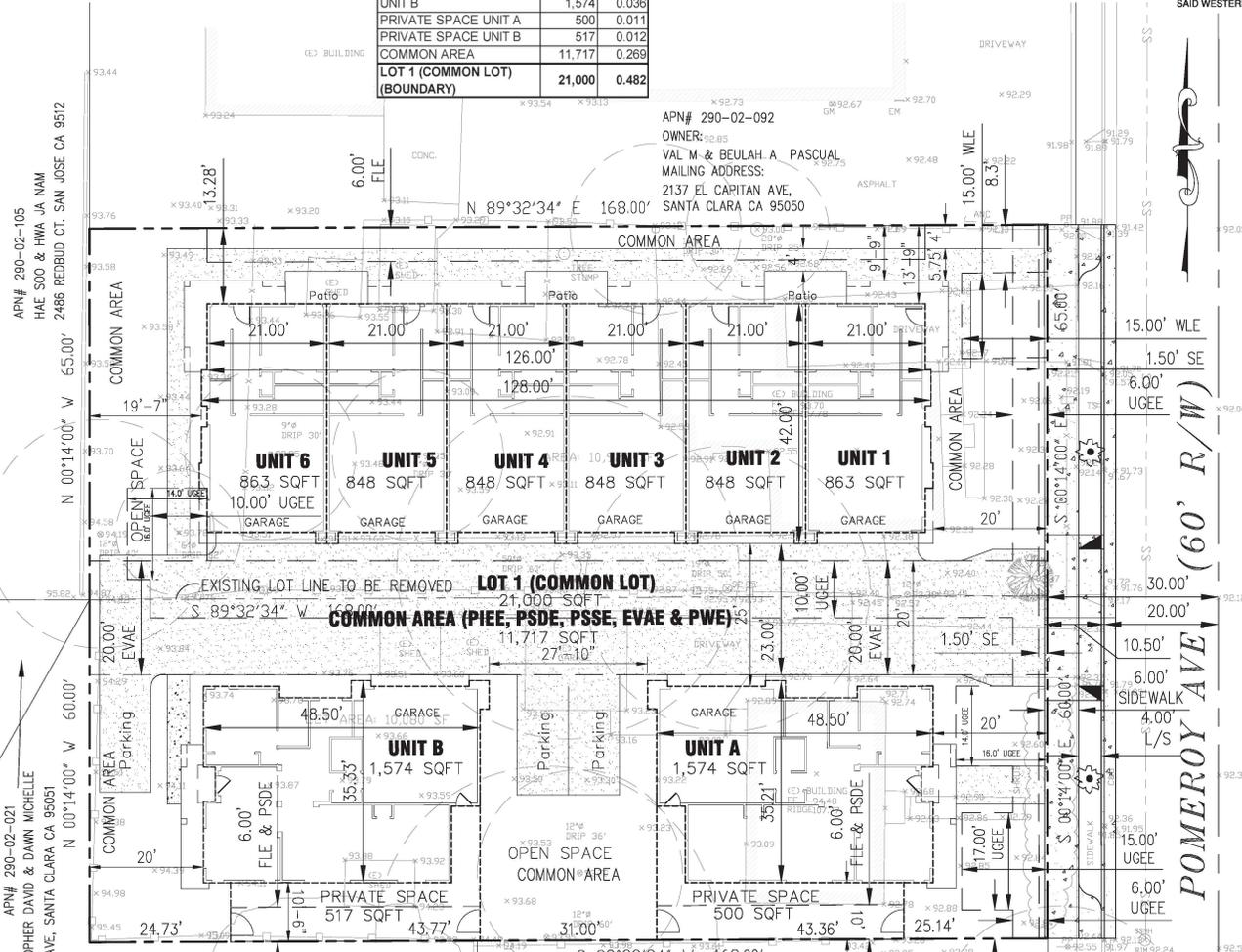
- OWNERS AND DEVELOPERS: POMEROY 1530-1540 PROPERTY Omid SHAKERI
- APPLICANT: SAME AS ABOVE
- APN# 290-02-096 and 290-02-097
- EXISTING ZONE: R3
- EXISTING USE: RESIDENTIAL
- PROPOSED USE: RESIDENTIAL, 8 CONDOMINIUM UNITS
- FLOODING, FLOOD ZONE X500
- STREETS: ALL PROPOSED STREET MODIFICATIONS WILL BE DONE TO THE SATISFACTION OF PUBLIC WORKS.
- EXISTING USE OF ADJACENT PROPERTIES: RESIDENTIAL / APARTMENTS
- WATER: CITY OF SANTA CLARA
- FIRE PROTECTION: CITY OF SANTA CLARA FIRE DEPARTMENT
- STORM/SANITARY SEWER: CITY OF SANTA CLARA
- POWER AND GAS: CITY OF SANTA CLARA/PACIFIC GAS AND ELECTRIC
- TELEPHONE: PACIFIC BELL
- STREET TREES: NEW STREET TREES WILL BE PLANTED PER CITY SATISFACTION, WITH A MINIMUM OF 10' FROM EXISTING AND NEW CITY WATERS & SANITARY SEWER FACILITIES.
- AREA OF SUBJECT PROPERTY: 0.482 ACRES (21,000 SQUARE FEET)
- ALL EXISTING STRUCTURES ARE TO BE REMOVED PRIOR TO RECORD OF FINAL MAP.

SHEET INDEX:
TM TENTATIVE MAP

Revisions:



Date: 10/21/2020
Scale: 1" = 20'
Prepared by: V.G.
Checked by: S.R.
Job #: 220050
Sheet:



TYPICAL POMEROY AVE. SECTION (60.00' R-0-W)



Agenda Report

22-374

Agenda Date: 3/9/2022

REPORT TO PLANNING COMMISSION

SUBJECT

Study Session: Climate Action Plan (CAP) Update

Following over two years of community engagement, the City has developed a Draft Climate Action Plan (CAP) 2022 to update the current CAP adopted in 2013. Adoption of the CAP Update would extend the City's Greenhouse Gas (GHG) emissions reduction goals through 2030, address all new State requirements established since the 2013 CAP was adopted, and offer a Qualified CAP that would provide California Environmental Quality Act (CEQA) streamlining benefits.

The City of Santa Clara adopted its CAP on December 3, 2013, as required by the City's 2010-2035 General Plan. The City's current CAP identified measurable actions the City could implement through the year 2020 to reduce GHG emissions to 1990 levels, as required by California's Global Warming Solutions Act of 2006 (Assembly Bill 32 or AB32). Senate Bill 32 (SB32), passed in 2016, requires further reduction in GHG emissions to 40 percent below 1990 levels by 2030 and a long-term goal of reducing emissions 80% below 1990 levels by 2050. In 2018, Executive Order B-55-18 set the target of statewide carbon neutrality by 2045.

To address these new State requirements established since the City's 2013 CAP was adopted and to extend the City's GHG emissions reduction goals through 2030, the City started the process of comprehensively updating the CAP in January 2020. Planning staff will provide an overview of the Draft CAP 2022 at the Planning Commission meeting.

CITY OF SANTA CLARA

Draft Climate Action Plan

March 2022



ACKNOWLEDGEMENTS

City Council

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Prepared by

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ACRONYMS & ABBREVIATIONS

BAWSCA	Bay Area Water Supply & Conservation Agency	SFPUC	San Francisco Public Utilities Commission
CAP	Climate Action Plan	SOV	Single-Occupancy Vehicle
C&D	Construction & Demolition	SVP	Silicon Valley Power
CARB	California Air Resources Board	TDM	Transportation Demand Management
CDD	Community Development District	TOD	Transit Oriented Development
CEQA	California Environmental Quality Act	VMT	Vehicle Miles Traveled
CFL	Compact Fluorescent Lamps	ZEV	Zero Emission Vehicles
EDR	Energy Design Rating		
EV	Electric Vehicle		
GHG	Greenhouse Gas		
GSI	Green Stormwater Infrastructure		
MWELO	Model Water Efficient Landscape Ordinance		
RPS	Renewable Portfolio Standard		



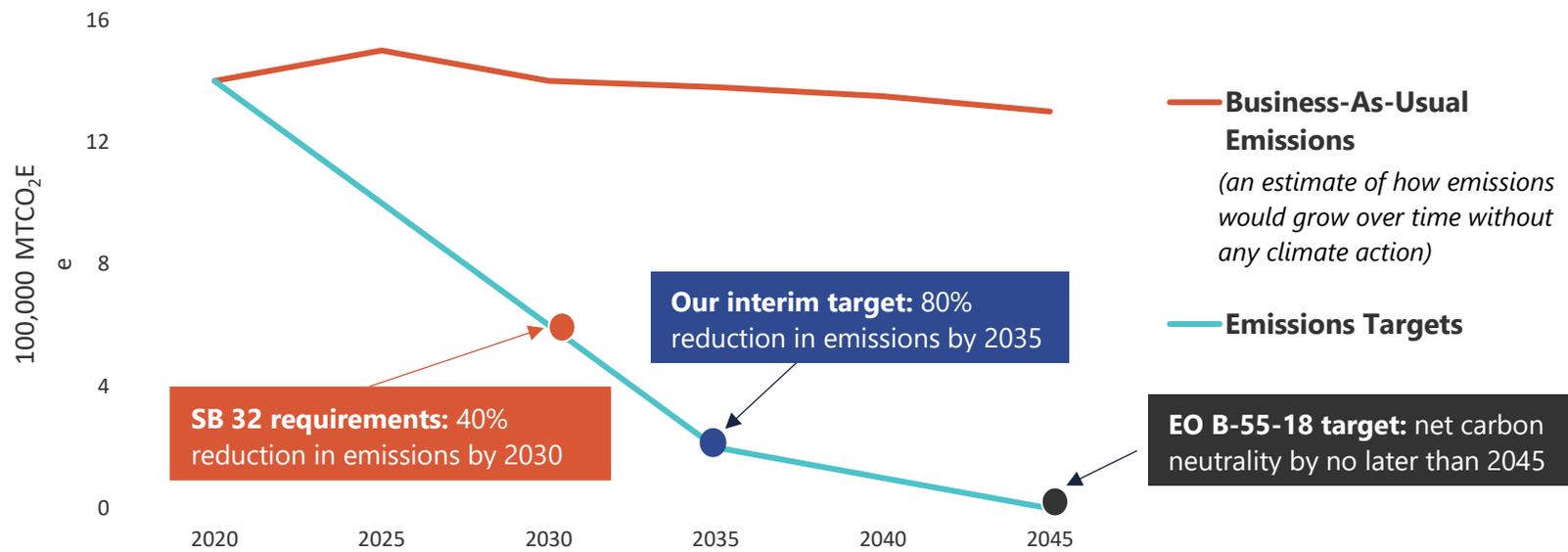
PLAN AT-A-GLANCE

OBJECTIVES

Santa Clara’s updated Climate Action Plan (CAP) aims to take an equitable, inclusive, and pragmatic approach to holistically addressing climate change in a way that not only reduces greenhouse gas emissions and builds resiliency to anticipated climate impacts, but also brings other vital social and economic co-benefits for our city.

TARGETS

This CAP update establishes a pathway toward achieving the following targets:



STRATEGIES & ACTIONS

Sector	Strategies	Key Actions
Buildings & Energy	<ul style="list-style-type: none"> ▶ Shift to electric fuels in new and existing buildings to achieve net-zero carbon buildings. ▶ Improve energy efficiency. ▶ Maximize renewable energy generation and storage capacity. 	Electrification incentives and requirements for new and existing buildings; building retrofit programs and code; expansion of carbon-neutral electrification; grid storage and resilience
Transportation & Land Use	<ul style="list-style-type: none"> ▶ Transition vehicles to electric alternatives. ▶ Expand clean mobility options and use of non-SOV transportation modes. ▶ Advance sustainable land use. 	Expansion of EV infrastructure through incentives and reach codes; improvements to public transportation; expansion of clean mobility options and incentives for walking, biking, and other active transportation modes; promotion of sustainable, dense development
Materials & Consumption	<ul style="list-style-type: none"> ▶ Increase waste diversion. ▶ Reduce landfilled food waste. ▶ Enhance sustainable production and consumption. 	Support for waste diversion and food recovery programs; new waste diversion requirements; City programs to promote sustainably produced, packaged, and shipped goods and materials
Natural Systems & Water Resources	<ul style="list-style-type: none"> ▶ Increase tree canopy cover. ▶ Enhance ecosystem resilience. ▶ Improve water supply and conservation. 	City tree planting initiatives; ecosystem resilience through sustainable landscaping; water conservation and recycled water incentives and landscaping requirements; increased recycled water use in public parks; conversion of gas to electric-powered landscaping equipment
Community Resilience & Wellbeing	<ul style="list-style-type: none"> ▶ Improve community resilience. ▶ Prepare for climate change. 	Expansion of public programs and emergency systems to prepare for climate impacts; incentives for adaptation upgrades; climate resilient infrastructure



IMPLEMENTATION PLAN

The CAP includes a plan for implementing the plan and monitoring and evaluating progress through the implementation phase, including:

- ▶ **Timeframes for implementation**, including ongoing actions and those planned for Year 1, near-term (2023-2026), mid-term (2027-2030), and long-term (after 2030) phases.
- ▶ **Implementation roles**, including lead departments and key partners.
- ▶ **Oversight and accountability**, including a creation of a City Sustainability & Climate Action Team to coordinate implementation and regular public updates and reporting on CAP progress through a Community Dashboard.
- ▶ **Monitoring and evaluation**, including annual progress reports and frequent updates of the City's GHG emissions inventory.



INTRODUCTION

Bounded to the north by the San Francisco Bay and to the south by the City of San Jose, the City of Santa Clara sits **nestled in the heart of Silicon Valley**. The nearly 130,000 members of our diverse community enjoy warm summers and mild winters—with 300 days of sunshine each year. Our vibrant **tree-lined neighborhoods and job centers** welcome new residents who enjoy access to over 40 city parks and pools and our 70-mile bicycle network.

Santa Clara is also **home to technology leaders** like Intel and NVIDIA. Our city is distinguished as the primary hub for Silicon Valley data centers—supporting cloud technology giants like Amazon.com, Inc., Microsoft Corporation, and Apple Inc. It’s no surprise that over 50 data centers are contained within Santa Clara’s modest 19 square miles. Our competitive advantage stems in large part from municipally owned Silicon Valley Power’s ability to offer **dependable, low-cost electricity**.

As our city continues to thrive, we are uniquely positioned to lead the way in **balancing economic growth and technological innovation with environmental responsibility**. We will continue to pursue **neighborhood-focused land use policies to increase infill housing developments** that both meet Santa Clara’s housing demand and support sustainability efforts through more dense development.

Collectively, we must reduce greenhouse gas (GHG) emissions while building resiliency to climate change within our community to maintain a **vibrant, healthy, and sustainable community for future generations**. The City of Santa Clara is committed to achieving these goals through resourceful, efficient, and progressive leadership.

This update of the City of Santa Clara’s CAP reflects the evolving needs of our community and the localized impacts of global climate change. It reaffirms the City of Santa Clara’s **commitment to climate leadership** and outlines a path toward a more **sustainable, healthy, and livable future** for all.



VISION FOR A SUSTAINABLE SANTA CLARA

WE ENVISION SANTA CLARA’S FUTURE AS A **HEALTHY, THRIVING, AND SAFE CITY TO LIVE, WORK, AND ENJOY LIFE**, WHERE THE BENEFITS OF LIVING HERE ARE **DISTRIBUTED EQUITABLY AMONG OUR RESIDENTS**, AND WHERE WE BUILD A HEALTHY CLIMATE FUTURE WHILE WE PRESERVE WHAT WE LOVE ABOUT THIS COMMUNITY.

We will achieve this vision by taking an **equitable, inclusive, and pragmatic approach to holistically addressing climate change** in a way that not only reduces greenhouse gas emissions and builds resiliency to anticipated climate impacts, but also brings other vital social and economic benefits to our city. This plan provides a roadmap for reaching this future, guided by the following overarching goals:

Create a more equitable and accessible community.

- ▶ Develop sustainability policies and programs in collaboration with voices and groups typically underrepresented, particularly communities of color and **those most at-risk to climate change impacts**.
- ▶ Ensure **equitable access** to the high quality of life that Santa Clara offers.

Maintain and enhance our quality of life.

- ▶ **Create a healthier, more walkable, bikeable, and affordable community** with ample neighborhood and community parks and useful green space, a connected trail network, clean air, and affordable and convenient public transportation.

Foster thoughtful and inclusive growth.

- ▶ Balance economic growth and development with thoughtful, climate-focused city planning that allows us to **live more sustainability** while also supporting our economy, local businesses, and residents.
- ▶ Bring **new, green jobs** to our community that are accessible to a range of skill levels.

Make our community more resilient.

- ▶ **Engage community members** on climate change and sustainability.
- ▶ Ensure our residents have **access to reliable clean water sources**.
- ▶ Power our community with **reliable, clean energy sources** that can withstand future strain on the energy grid.
- ▶ **Strengthen our public programs and emergency services** to protect community members from future climate impacts.



WHY UPDATE THE CAP?

The City of Santa Clara already plays an important role in electricity provision, building construction, land use and development, transportation, infrastructure maintenance, solid waste management, parks and open space management and maintenance, and water and wastewater management. Accordingly, the City is uniquely positioned to lead on climate action, facilitate collaboration and partnerships, and engage individuals, businesses, community groups, and other local governments to join these efforts.

By updating its 2013 CAP, the City of Santa Clara reaffirms its commitment to leading the way to a more sustainable community. The City has set bold targets and developed strategic pathways for reducing greenhouse gas emissions while increasing the City’s resilience to climate change impacts. The 2022 CAP aims to:

1. Prevent and prepare for the impacts of climate change.

Leading climate scientists around the world agree:

- **Human activity** is changing the earth’s climate through the release of greenhouse gas emissions—caused primarily by the combustion of fossil fuels.
- **Significant and irreversible impacts** will occur if average global temperature increase by 2°C, and that we should strive to limit the temperature increase even further to below 1.5°C.

What is a Climate Action Plan?

A Climate Action Plan is the City’s strategic planning document that outlines:

- Current and projected greenhouse gas emissions
- Greenhouse gas emissions reduction targets
- Strategies and actions for reducing emissions
- Projected vulnerability to climate change
- Strategies and actions for building climate resilience

The 2022 Climate Action Plan intends to reflect Santa Clara’s unique environment and reaffirms commitment to leading on climate action.

The Intergovernmental Panel on Climate Change projects a wide range of climate changes—including changing precipitation, ocean acidification, more extreme temperature changes, sea level rise, and an increase in extreme events such as heat waves, droughts, floods, cyclones, and wildfires.

Continued emissions of greenhouse gases will cause further warming and long-lasting changes, increasing the likelihood of severe, pervasive, and irreversible impacts for people and the natural systems we depend on.

The City of Santa Clara must take action to prevent the worst impacts of climate change and build resilience to changes that are unavoidable. In the years ahead, Santa Clara can expect to face heat waves, prolonged periods of drought, and longer and more severe regional wildfires that will impact air quality across the West.



2. Reflect the City's changing environment and community.

Addressing climate change involves all of us—residents, businessowners, and city leaders alike. As a result, effective CAPs must reflect the unique values, needs, and concerns of their community. Since the adoption of the 2013 CAP, Santa Clara's community has changed, with our total population having grown by nearly 8,000 residents. To ensure that we reflected the diverse priorities of residents and businesses, the City engaged the public throughout this CAP update process. This CAP also reflects changes in the economic landscape, city demographics, relevant technological advancements, best available climate science, greenhouse gas emissions projections, and the evolving regulatory and political environment.

Identifying and incorporating these changes into the plan helps ensure that targets, strategies, and actions reflect today's realities and tomorrow's future.

3. Align with new state requirements and guidance.

Since adoption of the 2013 CAP, California has continued to set aggressive climate action goals. New legislation requires the City of Santa Clara to meet ambitious carbon reduction targets, reflected in this updated CAP. The "State Regulatory Landscape" section below provides further detail on relevant state policy.

4. Demonstrate climate leadership and commitment.

Since the 2013 CAP was adopted, the City has developed plans and taken action to: expand and improve its pedestrian and bicycle network; decarbonize its buildings through incentives and exploration of reach codes; improve citywide transportation

demand management; expand use of electric vehicles (EV) and install EV charging; divert organic waste from the landfill; and expand Silicon Valley Power's renewable energy portfolio to reach 100% of electricity generated from renewable sources by 2045.

The 2022 CAP builds upon these long-term commitments and aligns with work already happening on the ground—led by community groups, universities, individuals, and businesses. For example, the decarbonization of Silicon Valley Power's energy portfolio aligns well with technology companies already developing innovative strategies and setting ambitious internal targets to reduce their emissions. By aligning their climate action missions, the City can continue to work collaboratively with businesses to lead the way in achieving collective climate goals.

5. Maximize co-benefits.

This CAP update provides a series of actions intended to reduce GHG emissions while building resilience to the projected impacts of climate change. These actions are designed to integrate with existing City planning efforts to gain synergies and benefit both the global climate *and* our local community.

For example, by expanding the bicycle and pedestrian network while improving accessibility and safety, the City can continue to encourage biking and walking as alternatives to driving motor vehicles. The co-benefits of this GHG emissions reduction effort allow residents to enjoy safe and accessible corridors and trails for recreation and commuting, while improving air quality and reducing traffic congestion and GHG emissions because of fewer vehicles on the road.



State Regulatory Landscape

California recognizes that GHG emissions and the impacts of climate change are a serious threat to our public health, economic wellbeing, and environment. A leader in climate action since the early 2000s, the State of California is on track to exceed its 2020 climate target of reducing GHG emissions to 1990 levels—while the economy continues to grow.

Since the adoption of Santa Clara’s 2013 CAP, the **State of California has adopted multiple climate policies relevant to this updated plan**, such as:

2015 EO B-30-15	Executive Order B-30-15 Establishes a statewide goal of reducing greenhouse gas emissions to 40 percent below 1990 levels by 2030.
2016 SB-32	Senate Bill 32 Expands on AB-32, requiring California to reduce greenhouse gas emissions to 40 percent below 1990 levels by 2030 and 80 percent below 1990 levels by 2050.
2018 EO B-55-18	Executive Order B-55-18 Expanded upon EO S-3-05, creating a statewide goal of carbon neutrality by 2045 (in addition to SB-32 targets).

This 2022 CAP reflects near-term SB-32 requirements and progress toward meeting long-term EO B-55-18 targets:

- ❖ Reduce greenhouse gas emissions to **40% below 1990 levels by 2030**.
- ❖ Achieve **net carbon neutrality no later than 2045**.



SUCCESSES & ACCOMPLISHMENTS

Prior to developing this CAP update, the City of Santa Clara made steady progress on climate action. **Santa Clara’s total emissions decreased by 4.5% and per-capita emissions decreased 13% from 2008 to 2016.** Other key accomplishments include:

Coal-free and large renewables	Energy efficiency programs	Transportation and land use	Water and natural systems	Waste reduction
<ul style="list-style-type: none"> ✓ Eliminated coal from the City’s energy mix. ✓ Provided carbon-free energy for residential customers. 		<ul style="list-style-type: none"> ✓ Reduced 9% of transportation emissions, despite population growth. ✓ Installed over 450 EV chargers in public spaces. 		<ul style="list-style-type: none"> ✓ Implemented mixed waste collection and processing to divert organic materials from landfills to satisfy SB 1383 requirements and to increase overall waste diversion rates
	<ul style="list-style-type: none"> ✓ Saved over 165 GWh of energy through energy efficiency incentives, pilot projects, and rebate programs. ✓ Implemented energy efficient practices in all new data centers. 		<ul style="list-style-type: none"> ✓ Reduced water use by over 2 billion gallons between 2008 and 2016. ✓ Planted ~4,000 new trees, facilitated by a policy to plant two trees for every one tree removed. 	



ASSOCIATED CLIMATE & SUSTAINABILITY PLANNING EFFORTS

Bicycle Master Plan | Updated in 2018, this plan outlines the City's long-term vision for improving the cycling infrastructure through policies, programs, and City-run projects. Initiatives are focused on creating an integrated cycling network, improving safety for cyclists, making cycling a more convenient transportation option, and fostering a more bike-friendly culture in Santa Clara.¹



Pedestrian Master Plan | Adopted in 2019, this is a blueprint for making Santa Clara more walkable, with particular focus on creating an integrated pedestrian network; making walkways safer, more enjoyable, and easier to navigate; and integrating walking into land use policy to make walking a more convenient mode of everyday transportation.²



EV Blueprint Plan | Developed in 2019 through a grant from the California Energy Commission, this blueprint outlines 14 program initiatives to improve Santa Clara's electric vehicle charging infrastructure to prepare to meet the state's goal to increase the number of zero-emissions vehicles to 5 million by 2030. Initiatives include updates to building codes, rebate and incentive programs, public-private partnerships, and changes to construction permitting requirements.³



Solar electric incentives | SVP offers the Low-Income Solar Grant Program, which provides free solar panel systems to qualifying residents.⁴



Reach codes | Reach codes refer to local building energy codes that "reach" beyond the minimum requirements for building energy use mandated by the state. Santa Clara's reach codes are focused on electrifying new buildings, improving energy efficiency, and increasing the electric vehicle charging infrastructure.⁵



¹ City of Santa Clara. 2019. [Bicycle Plan Update 2018](#).

² City of Santa Clara. 2019. [Pedestrian Master Plan](#).

³ City of Santa Clara. 2019. [EV Ready Communities Challenge](#).

⁴ Silicon Valley Power. [Incentives and Financing Options](#).

⁵ City of Santa Clara. 2019. [Building Electrification and Electric Vehicles Reach Codes](#).



Complete Streets Policy | This policy outlines specific principles for ensuring the city roadways are safe, accessible, and convenient for all transportation types, including pedestrians, motorists, bicyclists, persons with disabilities, and seniors.

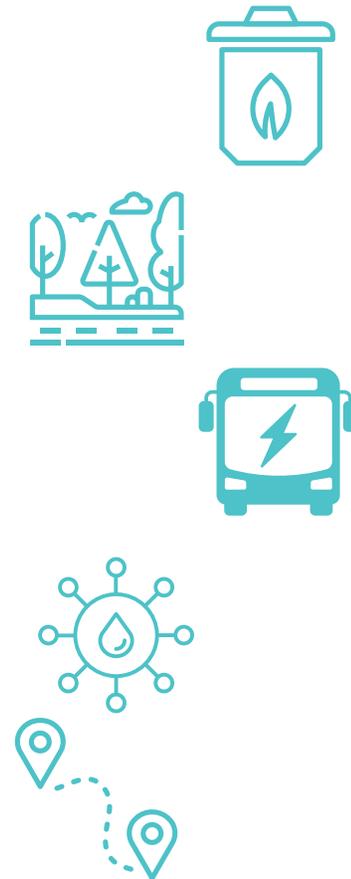
Organics Collection and Processing Program | After four years of gathering feedback and exploring alternative options, the City Council approved new contracts with Mission Trail Waste Systems (collection) and GreenWaste Recovery (processing) to provide residents and businesses with mixed-waste processing, a process that captures organic waste from the contents of garbage containers. Beginning January 2021, the organic material captured by mixed waste processing will be taken to a composting facility.⁶

Creek Trail Master Plan Expansion | Identified as a priority by the City Council in 2013, this trail expansion plan improves the existing on-street bicycle transportation system by developing bicycle and pedestrian creek trails along 5.75 miles of Calabazas Creek Corridor, 3.25 miles of Saratoga Creek Corridor, and 1.75 miles of the Hetch Hetchy corridor.⁷

City Green Fleet Policy | Directed in 2019, this policy effort ensures that the City purchases and uses the lowest emission vehicles available to reduce vehicle emissions, consumption of non-renewable resources, and maintenance and operating costs to the city.⁸

Urban Water Management Plan (UWMP) | Updated every five years, the UWMP provides an overview of the City's current and long-term water supplies, future water needs of the city, and the City's water conservation programs. The 2010 UWMP established a baseline target use goal of 186 gpcd (gallons per capita per day) per the Water Conservation Act of 2009 (SBX7-7). The City committed to achieve a 20% reduction in per capita water use by 2020. The City met this goal by achieving a calculated gpcd of 124 in 2020, saving approximately 6,328-acre feet (2,060 MG) of water from 2008 to 2016.⁹

Transportation Analysis Policy | Adopted in 2020, this policy establishes the requirements for evaluating land use and transportation projects based on the estimated vehicle miles traveled (VMT) for that project.¹⁰



⁶ City of Santa Clara. 2021. [Residential Food Scrap Recycling Pilot Program Update](#).

⁷ City of Santa Clara. 2013. [City of Santa Clara Creek Trail Network Expansion Feasibility Study](#).

⁸ City of Santa Clara. 2019. [City Manager's Directive 135-Green Fleet Policy](#).

⁹ City of Santa Clara. 2020. [Urban Water Management Plan](#).

¹⁰ City of Santa Clara. 2020. [Transportation Analysis Policy](#).



Specific Plans: Downtown, El Camino Real, Patrick Henry Drive, Freedom Circle, Tasman East | These neighborhood-specific plans outline General Plan development areas of Santa Clara where there is opportunity to intensify with limited impact on the existing neighborhood. Plans include initiatives focused on making the areas more pedestrian friendly and promoting sustainable, dense housing developments.¹¹

Green Stormwater Infrastructure (GSI) Plan | GSI uses vegetation, soils, and natural processes to manage rainwater and improve water quality. Adopted in 2019, The GSI Plan provides a roadmap for how the City will gradually transform traditional storm drainage systems from "gray" to "green" by incorporating GSI into projects in the public and City-owned properties.¹²

Sustainable Purchasing Policy | The goal of the City’s Sustainable Purchasing Policy is to strive to purchase products that minimize environmental impacts, toxins, pollution, and hazards to workers and the community. To the greatest extent practicable, the City will purchase products with the following characteristics:

- Are made with recycled content.
- Conserve energy and water.
- Reduce greenhouse gas emissions.
- Use unbleached or chlorine-free manufacturing processes.
- Are lead and mercury free.
- Use wood from sustainably harvested forests.
- Meet SB 1383 post-consumer recycled content paper purchasing requirements.

Adoption of Park and Recreational Land Dedication Ordinance | The goal of the addition of Chapter 17.35 to the City Code in July 2014 is to provide 2.6 to 3.0 acres of new developed parkland and recreational amenities in new residential development properties, pursuant to the California Quimby Act and the Mitigation Fee Act. New parkland in sufficient size, usable shape, and location near residential density provides quality of life, carbon sequestration, as well as natural habitats.



¹¹ City of Santa Clara. [Specific Plans](#).

¹² City of Santa Clara. 2019. [Green Stormwater Infrastructure Plan](#).



PLAN DEVELOPMENT PROCESS

This update of the City of Santa Clara’s CAP reflects and brings together City and community priorities developed through a robust, two-year involvement and assessment process. The primary objective of this process was to build a plan that is comprehensive, grounded in Santa Clara’s unique context, and reflects community priorities and needs. The process included the following key elements:

			
<p>BASELINE ASSESSMENT</p> <ul style="list-style-type: none"> – 2013 CAP progress review – Greenhouse gas inventory review – Emissions forecasting and modeling 	<p>TARGET SETTING</p> <ul style="list-style-type: none"> – Target, goal, key performance indicator (KPI) development – City staff meetings – Focus groups (#1) – Online survey (#1) – Planning Commission review 	<p>ACTION DEVELOPMENT</p> <ul style="list-style-type: none"> – Mitigation and adaptation strategy and action development – City staff meetings – Multi-criteria and quantitative analysis – Community workshop (#1) – Online survey (#2) – Planning Commission review – City Council review 	<p>CAP DEVELOPMENT</p> <ul style="list-style-type: none"> – Focus groups (#2) – Community workshop (#2) – Public comment period – Planning Commission review – City Council review and adoption



COMMUNITY ENGAGEMENT

The City of Santa Clara engaged the community throughout the climate action planning process. Despite pivoting to an entirely virtual community engagement strategy due to COVID-19, the City engaged with hundreds of people and received over 1,000 comments throughout the CAP development process. The primary objectives of this outreach were to:

- ▶ Reflect the community’s **values and priorities**.
- ▶ Engage a **broad representation** of stakeholders—including participants who are typically not engaged in planning.
- ▶ Gather **feedback** about how to reduce emissions and make Santa Clara a more sustainable place to live and work.
- ▶ **Educate** the community about the importance and urgency of climate action.
- ▶ **Empower** residents, stakeholders, City staff, and decision-makers to implement the CAP after its adoption.

To try and reach as many people as possible, the City engaged residents through a variety of channels over the two-year climate action planning process, including targeted stakeholder focus groups, online surveys, virtual workshops, public comments at the Planning Commission and City Council Study Sessions, and public comment period to review the draft plan. Looking ahead, the City will continue to engage the community as we move toward implementing the CAP.



Community Feedback

Target Setting		Select Strategies and Actions			Draft and Finalize Plan	
APRIL 2020	JUNE - JULY 2020	JULY 2020	OCT-NOV 2020	JULY 2021	SEPT 2021	OCT 2021
Focus groups	Survey #1	Virtual Community workshop	Survey #2	City Council Feedback and Directives	Virtual Public Workshop and Online Platform	Virtual Focus Groups
<p>Summary of Feedback:</p> <ul style="list-style-type: none"> Balance housing and job markets. Improve regional mobility. Align reduction targets with peer cities and state regulations. <p>Focus groups with: <i>Santa Clara Chamber of Commerce; Silicon Valley Leadership Group; Digital Realty; Santa Clara Youth Commission; Santa Clara University; ABAG; Silicon Valley Bicycle Coalition; Santa Clara Ped & Bike Committee</i></p>	<p>Summary of Feedback:</p> <ul style="list-style-type: none"> Greatest barrier to addressing climate change in Santa Clara: "lack of public education or awareness." Most important climate-related issue: "renewable energy." Greatest climate-related threats: "drought" and "worsened air quality." Preferred emissions target: "faster than state targets." 	<p>Summary of Feedback:</p> <ul style="list-style-type: none"> Prioritize equity. Increase amount of renewable energy provided by SVP. Develop strong transportation demand management plans. Improve transportation network for bicyclists and pedestrians. 	<p>Summary of Feedback:</p> <ul style="list-style-type: none"> Preference for voluntary and incentive-based measures. Reduce VMT by encouraging use of public transit, carpooling, biking, walking. Increase solar and battery storage. Plant/preserve trees and gardens; more public parks. Reduce waste. Increase public education and engagement. 	<p>Summary of Feedback:</p> <ul style="list-style-type: none"> Adopt an interim 80% GHG reduction target by 2035. Enact new CAP actions to reach interim target, including adopting a burnout ordinance for gas furnaces and water heaters and requiring secured bicycle parking at multi-family properties. Relax proposed TDM requirements for select businesses and residential properties. 	<p>Summary of Feedback:</p> <ul style="list-style-type: none"> Clarify how the CAP actions align with other City planning efforts, particularly reach codes. Clarify the incentives integrated into the CAP—what they will include and how the City plans to promote them. Expand private sector partnerships and seek other opportunities to engage the business community and promote sustainable business practices. 	<p>Summary of Feedback:</p> <ul style="list-style-type: none"> Prioritize incentives to mitigate cost concerns. Explore ways to use the CAP to address existing income disparities. Emphasize collaboration and resource sharing. <p>Focus groups with: Organizations representing,</p> <ul style="list-style-type: none"> The unhoused population, low-income residents, and other vulnerable groups; Asian American, Muslim American, and other underrepresented groups; Contractors, developers, and other members of the building community.
20 participants	387 respondents	38 participants	496 respondents		20 workshop participants; 33 online comments	20 participants



Centering equity

Climate change disproportionately affects vulnerable and marginalized communities, such as communities of color, low-income populations, those with disabilities, and those experiencing homelessness or housing insecurity.

The City of Santa Clara recognizes that addressing the challenges of climate change will require **uprooting systemic inequalities** to uplift and strengthen the most at risk in our community and provide opportunities and benefits for all. Solutions must begin by questioning whether strategies and actions benefit some while burdening others.

By committing to the implementation of this CAP, the City of Santa Clara has prioritized cross-cutting solutions that support **long-term equity, resilience, public health, and community wellbeing**.

As we move forward, the City of Santa Clara must prioritize **shared benefits, accessibility, capacity-building, partnerships, and accountability** throughout the community.

We will take advantage of the actions already underway to **meet our climate goals, uplift our community**, and ensure no one is excluded from the additional **benefits of a healthier, low-carbon future**.

Equity Elements in the Updated CAP

Community Engagement

- ✓ Engaged Santa Clara's diverse community throughout the CAP process through inclusive, accessible outreach strategies.

Emissions Reduction Targets

- ✓ Acknowledges the City's responsibility in reducing global emissions to slow the impacts of climate change.

Strategies & Actions

- ✓ Prioritized actions that reduce historical or current disparities among communities of color, low-income populations, and other marginalized groups.

Plan Implementation

- ✓ Developed implementation plan that considers how to make benefits broadly accessible and shared among the community without disproportionately burdening vulnerable groups.



CLIMATE CHANGE IN SANTA CLARA

Santa Clara commits to maintaining the progress made and rising to the new challenges that climate change brings.

Left unchecked, climate change will undo the progress we have made as a city. Changing conditions will significantly impact Santa Clara's economic, ecological, and social systems that make up our daily lives. Climate impacts are projected to become more severe in the future, leading to disruptions to our critical facilities and services, such as disturbances to our highways and transportation systems, destruction of assets and property, power outages, water scarcity, and increased utility rates. The City must meet the moment and continue to adapt and evolve with the changing needs.

Action today will prepare our city for future climate impacts.

By reducing GHG emissions, Santa Clara can help prevent some of the most severe climate impacts. However, some impacts are now unavoidable, and the City must prepare by making our community more resilient through initiatives aimed to protect vulnerable populations, strengthen vital infrastructure, and preserve natural ecosystems critical to a balanced climate.

Proactive local climate action is vital for achieving carbon neutrality by 2045.

As shown below, the two major sources of remaining emissions are projected to be transportation and natural gas at 53% and 35% of projected 2045 emissions, respectively. Emissions from waste and wastewater treatment are projected to continue to rise, together making up the remaining 12% of total 2045 emissions.

The majority of Santa Clara's current greenhouse gas emissions stem from non-residential electricity use. State policies and carbon-free electricity will allow Santa Clara's emissions to decrease approximately 72% by 2045, leaving emissions from transportation, natural gas, waste, and wastewater treatment remaining. This plan sets us on a pathway for addressing these remaining emission sources and reaching carbon neutrality.



CLIMATE RISK & VULNERABILITY

Santa Clara is already experiencing climate change impacts, which will continue to worsen in the years to come unless we take action. These climate impacts pose a serious threat to our economy, health, and quality of life. Potential consequences to our community include:

<p>Extreme Heat</p>  <p>Santa Clara County is expected to see an increase in annual average temperature of 2-4°F by 2050 and 4-6°F by 2100.^{Error!} Bookmark not defined.</p>	<p>Riverine Flooding</p>  <p>The probability of a 100-year flood event in Santa Clara County could be 10-20% higher by 2050 and 30-40% higher by 2100.</p>	<p>Threats to Public Health</p>  <p>Extreme heat will cause more heat-related illness and hospitalizations.</p>  <p>Increased allergens and harmful air pollutants due to higher temperatures will put people with asthma and other vulnerable populations at higher risk of health complications.</p>	
<p>Sea Level Rise & Coastal Flooding</p>  <p>The San Francisco Bay is projected to rise:</p>  <p>6 inches by 2030 11 inches by 2050 36 inches by 2100</p>	<p>Wildfire</p>  <p>The Bay Area is one of the more risk prone areas in the state. Regional wildfires threaten Santa Clara’s air quality, supply chain and distribution channels, and water quality.</p>	<p>Threats to habitat and critical species</p>  <p>Wildfires, warming temperatures, and changing precipitation patterns will disrupt forests, streams, and other critical habitats that are home to important local species.</p>	<p>Economic Impacts</p>  <p>More extreme temperatures and weather patterns threaten agriculture and food security, tourism, outdoor recreation, and other seasonal and climate-dependent industries. Resource fluctuation also threatens products and service costs.</p>

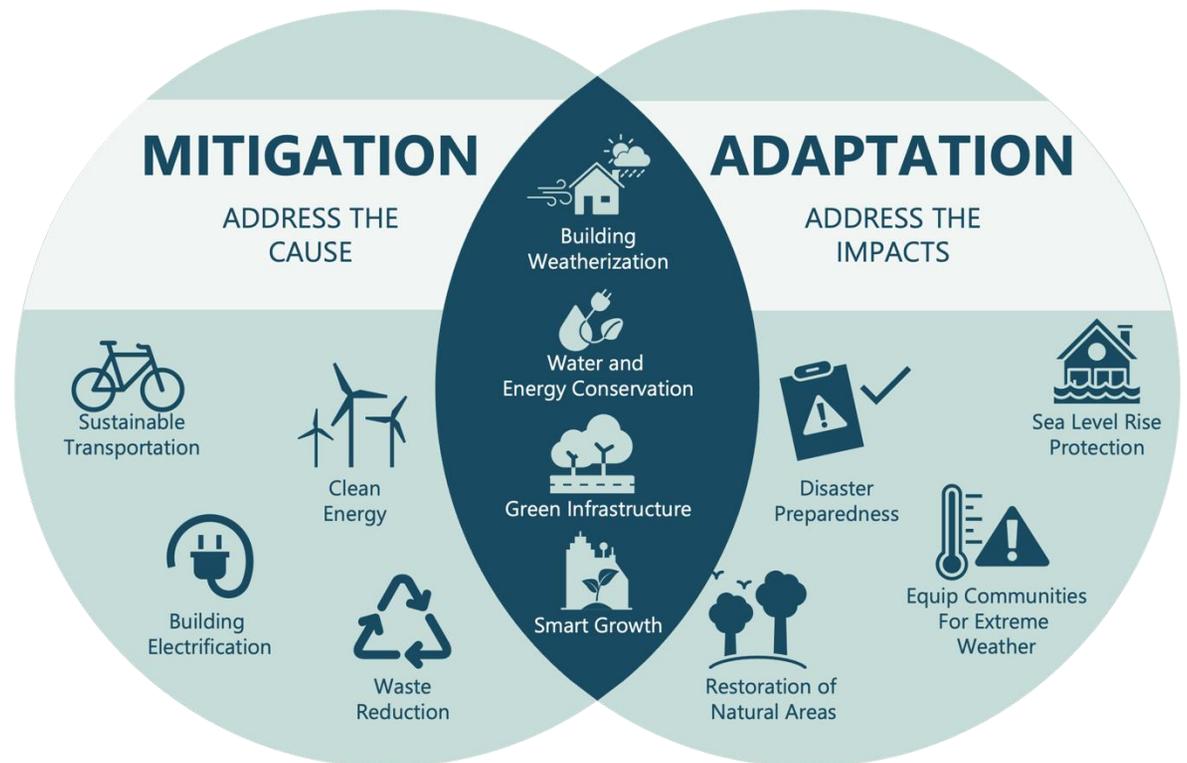


FOSTERING CLIMATE RESILIENCE

This CAP was developed using a two-pronged approach: reducing greenhouse gas emissions to slow the effects of climate change (mitigation) while building resiliency to the inevitable impacts of climate change (adaptation):

- ▶ **Mitigation actions** aim to prevent and slow down the impacts of climate change by reducing greenhouse gas emissions and increasing carbon “sinks” (oceans, forests, and soil) to store these gases.
- ▶ **Adaptation actions** manage the impacts of climate change by protecting vulnerable populations, ecosystems, natural resources, and infrastructure.

The figure at right outlines examples of climate mitigation and adaptation activities.



Successful climate change preparation includes both reducing climate-related vulnerabilities and preparing to respond to and recover from impacts as they occur.



SANTA CLARA'S GREENHOUSE GAS EMISSIONS PROFILE

Emissions Trends (2008 to 2016)

Santa Clara's **greenhouse gas emissions declined 4% from 2008 to 2016**—despite growth in Santa Clara's economy and a population increase of 10% over the same period. **Per-capita emissions also decreased**, declining 13% over the same period.

As Silicon Valley Power (SVP) provides electricity to over 50 major data centers in the city, it is no surprise that the **non-residential energy sector contributed over 60% of total community emissions**. As a municipal utility, SVP's ability to offer dependable, low-cost electricity makes Santa Clara a hub for these large data centers. Balancing the city's role as a technology hub with the need to reduce its climate footprint highlights the importance of 'cleaning the grid' to provide affordable, and renewable or carbon-neutral electricity to commercial customers.

Total emissions decreased in nearly every sector between 2008 and 2016. As detailed in the graphics on the following page, Santa Clara made the largest reductions in non-residential natural gas emissions. Transportation emissions decreased the least, indicating opportunity for further progress in this sector in the years ahead. Non-residential electricity emissions, meanwhile, increased by nearly 22% between 2008 and 2016. This increase was attributed to post-recession economic growth and the increase in data centers built during that period.

Emissions Forecast (2016 to 2045)

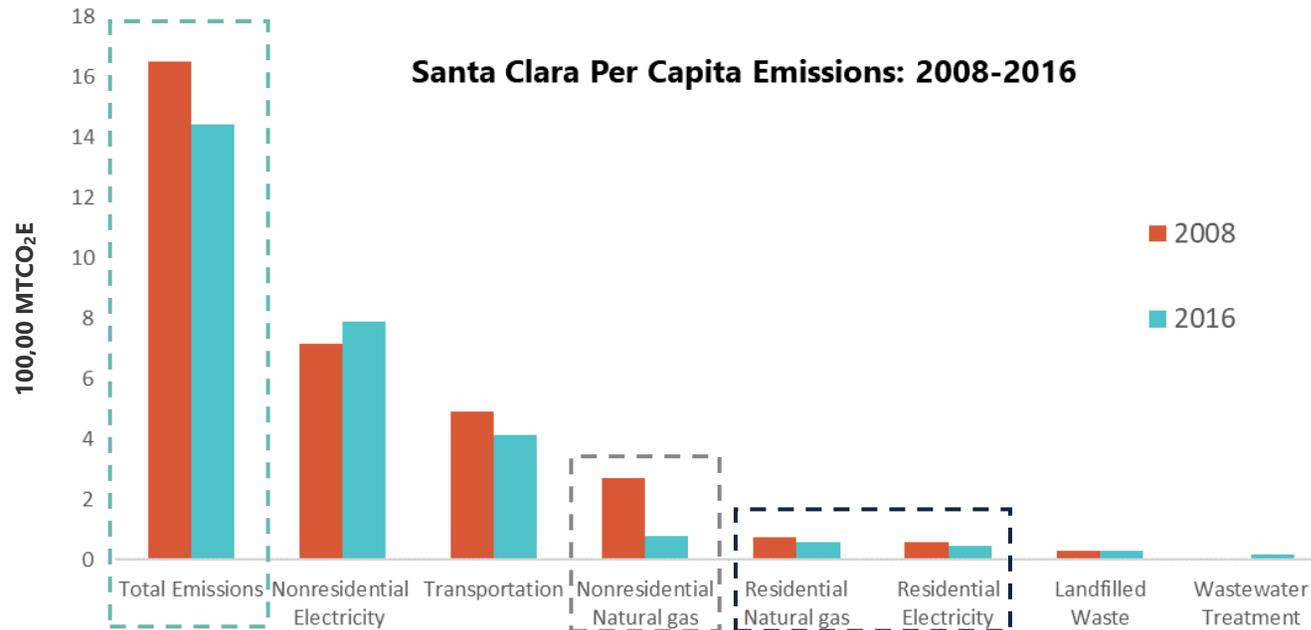
In 2018, SVP eliminated coal from its energy mix and now provides carbon-free energy to all residential customers, eliminating emissions from residential electricity. California's Renewable Portfolio Standard (RPS) requires electricity sold by utilities to be 33% renewable by 2020, 60% renewable by 2030, and 100% carbon-free by 2045. As a result, **emissions from electricity are expected to zero out by 2045**.

The 2016-to-2045 forecast, shown on page 22, reflects population growth projections, as well as reductions from state measures such as Title 24 building code standards, vehicle efficiency standards, and electric vehicle adoption. It shows that **current state policies and carbon-free electricity will allow Santa Clara's emissions to decrease approximately 70% by 2045**.

Two major sources of emissions are projected to remain: transportation and natural gas at 53% and 35% of projected 2045 emissions, respectively. Emissions from solid waste and wastewater treatment are projected to continue to rise, together making up the remaining 12% of total 2045 emissions. This forecast illuminates that California's state policies alone won't get us to our end goal; **the strategies and actions developed in this City of Santa Clara CAP are critical for a carbon-neutral future**.



Communitywide Greenhouse Gas Emissions Trends



▶ Between 2008 and 2016, **total emissions declined** by around 4%, despite the city's population growing by 10%. **Per-capita emissions** declined by 13%.

2008		2016
1,862,824 MTCO ₂ e	-4%	1,790,833 MTCO ₂ e
16.5	-13%	14.4

▶ **Non-residential natural gas decreased the most** of any sector; total emissions declined by 67% and per-capita emissions declined by 70%.

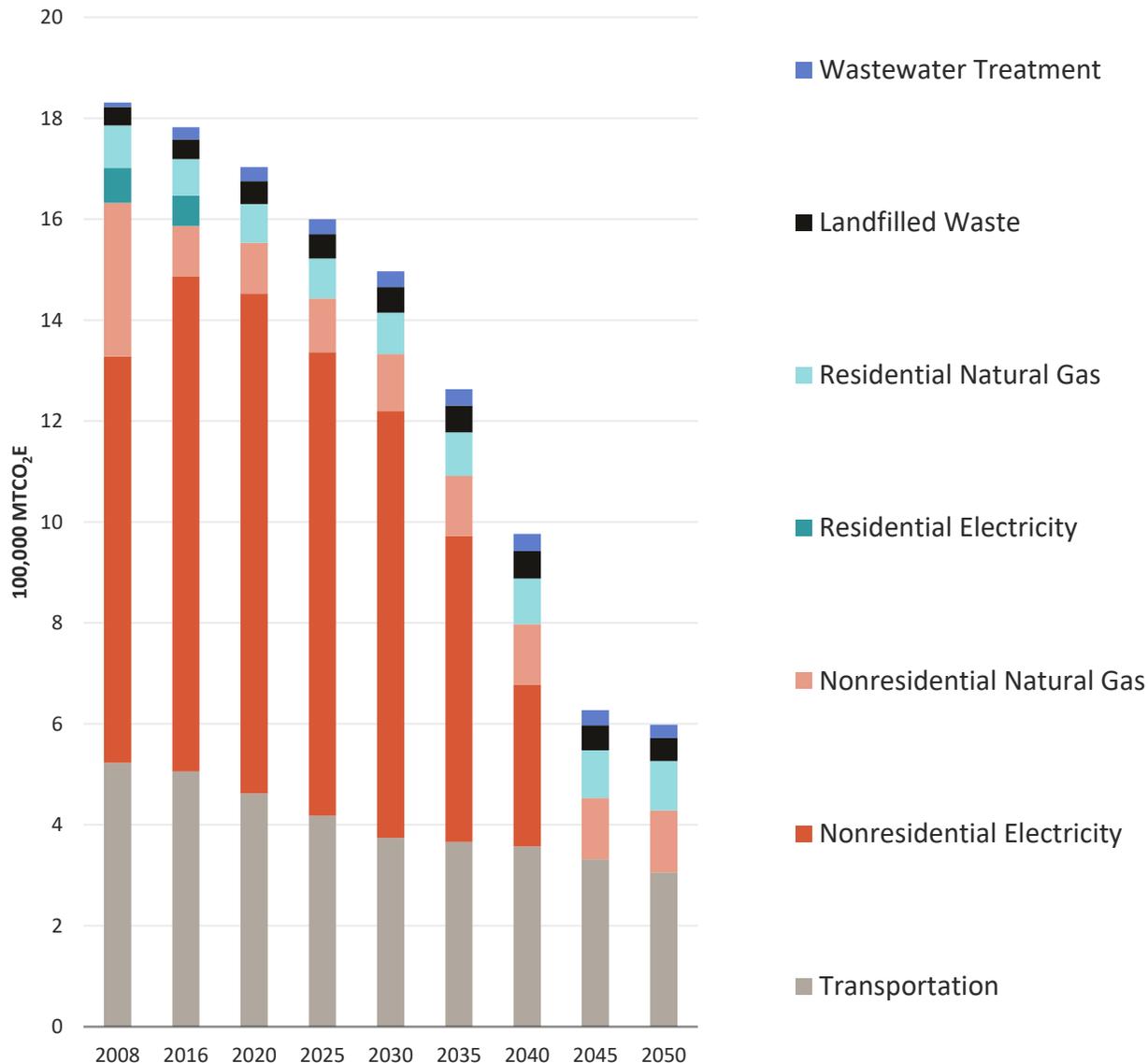
2008		2016
304,181 MTCO ₂ e	70%	99,945 MTCO ₂ e

▶ Residential electricity and natural gas per-capita emissions **both declined by around 20%**.

2008		2016
E: 0.61 NG: 0.75 MTCO ₂ e	20%	E: 0.48 NG: 0.59 MTCO ₂ e



Communitywide Greenhouse Gas Emissions Forecast



Even without this CAP, Santa Clara’s GHG emissions are projected to decline in the future—largely due to the influence of federal, state, and regional policies:

- **SB-100** requires that 100% of electricity sold by utilities be carbon-free in 2045.
- **Title 24** specifies how new buildings must be constructed, including specifying minimum energy efficiency standards; California has set a goal for zero-net energy new construction by 2030.
- **Clean Car Standard** requires that vehicles sold in California meet minimum fuel efficiency requirements, and that fuel sold in the state emits less GHGs during production and use.
- **SB 1383** requires that food scraps and other organic material is diverted from landfill disposal such that 75% of organic material is diverted from landfill by 2025.
- **EO N-79-20** sets a goal that 100% of in-state sales of new passenger vehicles and trucks are zero-emission vehicles.



CARBON NEUTRAL 2045

This CAP update establishes a pathway toward achieving:

- ❖ **SB 32 requirements:** a 40% reduction in emissions by 2030.
- ❖ **Our interim target:** an 80% reduction in emissions by 2035.
- ❖ **EO B-55-18 target:** net carbon neutrality by no later than 2045.

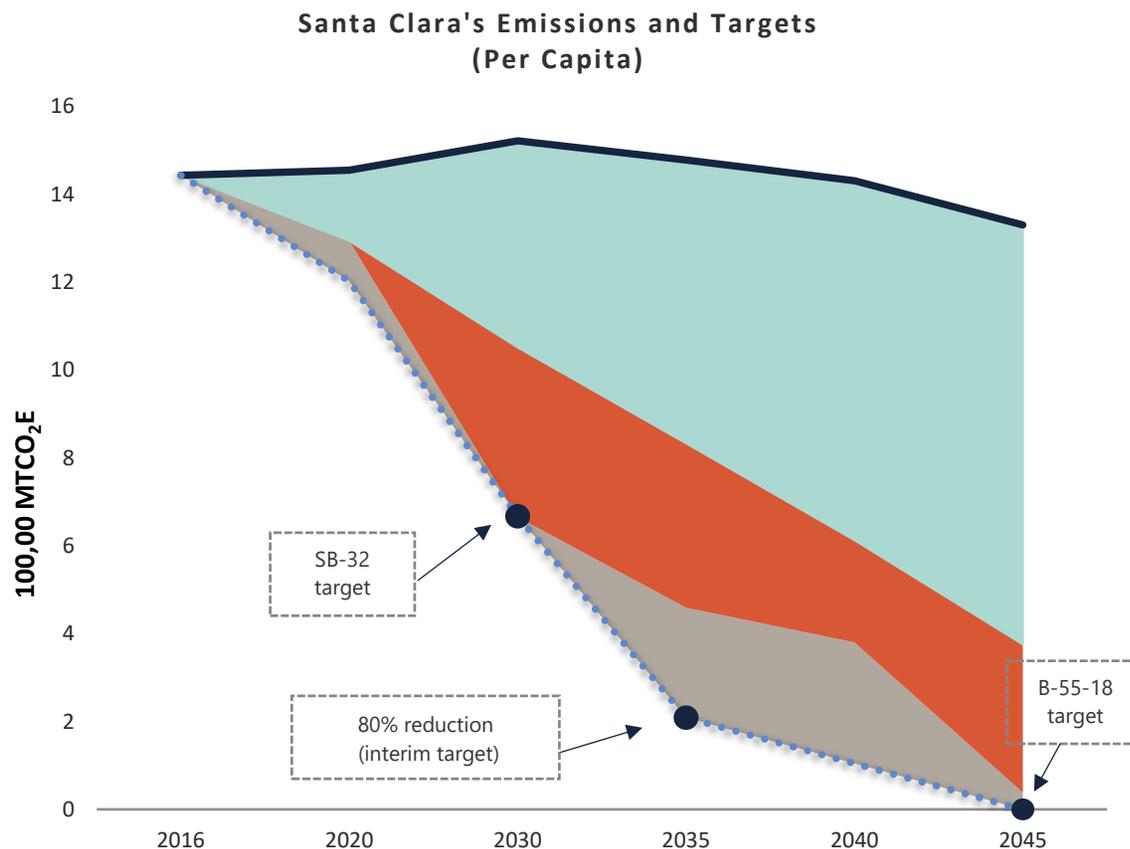
These goals reflect community and City feedback to **set bold targets to address climate change**—getting ahead of anticipated state regulations and aligning with peer communities. Carbon neutrality refers to **net zero greenhouse gas emissions caused by fossil fuel use within the city**.

This CAP includes innovative strategies and actions to significantly reduce greenhouse gas emissions into the future—but technological constraints may prevent reducing emissions to absolute zero by 2045. As a result, **to achieve carbon neutrality, we will need to offset every ton of CO₂e emitted with an equivalent amount of CO₂e removed** through a combination of nature-based solutions, carbon capture technology, and other carbon offset options.



SANTA CLARA EMISSIONS AND TARGET PATHWAYS

As illustrated in the graphic below, we will need to proactively take local climate action to reduce and offset greenhouse gas emissions to achieve our targets. State and regional policies and regulations are projected to reduce 2045 business-as-usual (BAU) emissions by 72%. The actions within this CAP are projected to further reduce emissions by 25%.



Business-As-Usual (BAU):
An estimate of how emissions would grow over time without any climate action.

External Factors:
The influence of federal, statewide, and regional policies (e.g., California’s Renewable Portfolio Standard) will have on Santa Clara’s projected emissions.

CAP Actions:
The estimated collective impact of the actions in this CAP.

Additional Actions to Reach Goal:
The additional emissions reduction needed after implementing external policies and CAP actions.

- Targets:**
- Reduce emissions by 40% by 2030 (**SB-32**).
 - Reduce emissions by 80% by 2035 (**Interim**).
 - Carbon neutral by 2045 (**B-55-18**).



80% BY 2035: HOW WILL WE GET THERE?

The chapter that follows outlines Santa Clara’s adopted strategies and actions that will collectively meet state GHG reduction requirements in the near-term (40% reduction by 2030) and set the city on a pathway to achieve carbon neutrality in the long-term (by 2045). In addition to these targets, the City aspires to reduce emissions more aggressively in the near-term: achieve an **80% reduction in emissions by 2035**.¹⁴

To achieve this interim target, one of the key actions that Santa Clara would need to take is to transition to a carbon-neutral electricity fuel mix. This would require SVP to achieve 100% carbon neutral electricity by 2035, which may require rate increases of 44% to 55%. Achieving this target would also require all-electric new construction by 2035.

An additional strategy would be to target the electrification of existing buildings.

Full implementation of actions in this CAP will also be critical for achieving long-term emission reductions, including the following:

1. Target building and transportation electrification through implementation-of the all-electric reach codes adopted in November 2021 requiring all-electric new construction (with some exceptions) and to include robust EV charging.
2. Prepare a “burnout policy” to replace natural gas furnaces and water heaters with an electric equivalent.
3. Require all new data centers to operate on 100% carbon neutral energy, with offsets as needed.
4. Require a 25% reduction in project-based VMT through active Transportation Demand Management (TDM) requirements for large employers over 500 employees, including aggressive regulations to reduce parking, in new developments.
5. Adopt a 20% reduction of VMT for multifamily residential through active TDM requirements, which may require parking maximums, in new developments.
6. Require advanced EV charging stations and secured bicycle parking at new residential developments.

¹⁴ This plan is focused on achieving a per service population reduction. Service population in this context refers to total Santa Clara residents plus total Santa Clara job force/employees.



STRATEGIES & ACTIONS

The strategies and actions in this plan reflect Santa Clara’s unique context and role in taking climate action. Considerations include:

- ▶ **Utility ownership.**
Silicon Valley Power electricity utility is municipally owned, which grants the City direct control over utility operations, business decisions, and related program activities.
- ▶ **Progressive state and regional activities.**
California has introduced ambitious climate policies and regulations, as well as tools and resources for supporting local climate action. Santa Clara’s strategies align with other California cities—setting ambitious emissions reduction targets and leading the nation in local climate action planning.
- ▶ **Data center hub in the center of Silicon Valley.**
Santa Clara’s electric utility provides unique opportunities for the provision of carbon neutral energy options. SVP will continue to work with Data Center developers who are interested in procuring additional renewable resources earlier than required by law.
- ▶ **An engaged community with a vision for a more sustainable future.**
Our community is engaged on climate change issues, particularly in relation to transportation and expanding community green spaces.

The following strategies and actions collectively work toward achieving our long-term goal of carbon neutrality no later than 2045.



STRATEGIES & ACTIONS AT-A-GLANCE



Buildings & Energy

Transition to clean renewable energy sources and reduce energy consumption.

Strategy B1	Shift to electric fuels in new and existing buildings to achieve net-zero carbon buildings.
B-1-1	Electrification incentives & financing
B-1-2	Electrification outreach for commercial & residential energy upgrades
B-1-3	Electric panel upgrades upon sale/turnover
B-1-4	Municipal Electrification Action Plan
B-1-5	Reach codes for new construction
B-1-6	Burnout ordinance
B-1-7	Carbon-neutral data centers
Strategy B2	Improve energy efficiency.
B-2-1	Municipal energy retrofits
B-2-2	Free home energy upgrades for qualifying residents
B-2-3	Energy-efficient and electric-ready building code
B-2-4	Distributed energy resource pilots
B-2-5	Energy efficiency incentives
Strategy B3	Maximize renewable energy generation and storage capacity.
B-3-1	SVP Integrated Resource Plan (IRP) for carbon-neutral electricity
B-3-2	City-owned renewable energy projects
B-3-3	Renewable installations at municipal facilities
B-3-4	Renewable energy generation & storage on private property
B-3-5	Local grid resiliency & energy storage improvements
B-3-6	Alternative fuel backup generators
B-3-7	Renewable electricity for new data centers





Transportation & Land Use

Transition to clean and efficient mobility options and transportation modes while maintaining accessibility and mobility for all.

Strategy T1	Transition vehicles to electric alternatives.
T-1-1	Community EV Blueprint implementation
T-1-2	EV charging for all new construction
T-1-3	City Fleet Electrification Plan implementation
T-1-4	Heavy duty electric trucks
T-1-5	Municipal charging infrastructure
Strategy T2	Expand clean mobility options and use of non-SOV transportation modes.
T-2-1	Pedestrian & Bicycle Master Plans implementation
T-2-2	Curb management improvements
T-2-3	Bike & shared mobility improvements
T-2-4	Transit gap & improvement study
Strategy T3	Advance sustainable land use.
T-3-1	TDM plan requirements
T-3-2	Sustainable development in underutilized non-residential areas
T-3-3	Transit-oriented development
T-3-4	Telework
T-3-5	Transportation Analysis Policy compliance





Materials & Consumption

Increase diversion of waste from landfills, reduce communitywide waste generation, and reduce the upstream GHG impacts of consumption.

Strategy M1	Increase waste diversion.
M-1-1	Compliance with state solid waste ordinances
M-1-2	Waste diversion pricing signals
Strategy M2	Reduce landfilled food waste.
M-2-1	Technical assistance to top food generators
M-2-2	Food recovery & donation
M-2-3	Food recovery organization partnerships
Strategy M3	Enhance sustainable production and consumption.
M-3-1	Reuse of salvageable building materials
M-3-2	City property consumption & waste diversion
M-3-3	Municipal Sustainable Procurement Policy
M-3-4	Carbon-smart building materials
M-3-5	Low-carbon schools





Natural Systems & Water Resources

Foster nature-based solutions, climate-resilient natural landscapes, and help store more carbon in trees and soils. Conserve community water resources by maximizing water efficiency to ensure a secure and sustainable water supply in the face of climate change.

Strategy N1	Increase tree canopy cover.
N-1-1	Right-of-way tree planting
N-1-2	Private property tree planting support
N-1-3	Urban forest partnerships
N-1-4	Tree maintenance, replacement, & plantings
Strategy N2	Enhance ecosystem resilience.
N-2-1	Carbon farming on open space lands
N-2-2	Partnerships for compost management
N-2-3	Sustainable planting guide
N-2-4	Sustainable park management
Strategy N3	Improve water supply and conservation.
N-3-1	Water conservation rebates
N-3-2	Fixture replacements
N-3-3	Water-efficient landscaping requirements
N-3-4	Community water portfolio diversification
N-3-5	Recycled water connection requirements





Community Resilience & Wellbeing

Ensure Santa Clara is prepared and can withstand climate and non-climate emergencies, focusing on those at highest risk.

Strategy C1	Improve community resilience.
C-1-1	Community resilience networks
C-1-2	Support for people experiencing homelessness
C-1-3	Community climate action grant
C-1-4	Incentives for adaptation upgrades
Strategy C2	Prepare for climate change.
C-2-1	Climate-resilient land use & development
C-2-2	On-site & natural stormwater systems
C-2-3	High-albedo parking lots
C-2-4	Climate Resilience Capital Improvement Program (CIP)
C-2-5	Planned retreat strategies



A Strategic Approach

We will work to achieve carbon neutrality no later than 2045 by building upon the progress we have already made and adopting new emissions reduction strategies and actions. Together, these strategies and actions (1) provide a **framework for reaching carbon neutrality**; (2) make Santa Clara **more resilient to future climate impacts**; and (3) have important **social and economic benefits**, such as addressing historic inequities, creating green jobs, increasing community green spaces, and improving public health. The table below describes our phased, strategic approach for reaching our climate goals.

<p>Focus immediately on expanding Santa Clara’s transition to clean electricity by phasing out all remaining fossil fuel-sourced electricity.</p> <p>This transition is key in reducing emissions from both buildings and transportation and meeting our long-term goals.</p>	<p>Meanwhile, significantly reduce emissions from energy by making buildings more energy efficient while electrifying appliances and infrastructure.</p> <p>At the same time, reduce transportation emissions by expanding electric vehicle adoption and shifting away from single occupancy vehicles.</p>	<p>As the electricity fuel mix gets cleaner with a higher percent of renewable sources, we can ensure that our community is prepared by phasing out natural gas infrastructure and fossil fuel-based transportation. We will do this by electrifying new and existing buildings and transitioning to electric vehicles.</p>	<p>As we reduce emissions from transportation and energy, we will also expand our zero waste and sustainable consumption programs. These programs will divert organic waste from landfills, where it produces potent methane emissions, and will help community members to buy less generally, which reduces upstream emissions from material production and consumption.</p>	<p>Throughout this process, we will also focus on local carbon sequestration projects, including expanding local tree planting programs and adopting nature-based solutions that protect and restore natural systems and naturally capture and store carbon. Carbon sequestration is vital in reaching carbon neutrality and will help Santa Clara close any gaps left by other initiatives.</p>
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Climate Adaptation & Resilience | In addition to reducing GHG emissions, Santa Clara will also strategically **integrate adaptation and resilience actions** throughout the CAP implementation—simultaneously both **preventing and slowing down** the worst climate impacts while also **preparing** the community for impacts that are unavoidable.



Embedding Co-Benefits & Equity

In developing the strategies and actions in this CAP, the City considered how strategies and actions contribute to social and economic co-benefits, such as creating green jobs, or enhancing public health by supporting healthier lifestyles. Co-benefits refer to the additional benefits an action brings to the city, or ways it helps meet other City goals. Co-benefits are indicated with the icons below throughout the Strategies & Actions section of this plan. **While many actions have a range of co-benefits, actions with one or more of the below icons highlight those with a particularly strong contribution.**

Specific equity considerations included:

- ▶ Who will the action benefit and/or burden?
- ▶ Are the benefits broadly accessible?
- ▶ Does the action incorporate meaningful, authentic, and culturally appropriate engagement?
- ▶ Does the action support long-term relationships and trust between communities and local government?

	<p>Climate Resilience Action is a vital, or foundational step, for improving strength against climate hazards like hotter temperatures, floods, wildfire, and drought.</p>		<p>Ecosystem Health Action helps expand protection and preservation of habitats, species, water, or air quality.</p>
	<p>Public Health Action supports enhanced health and longevity. Improving our environment to prevent lung, heart, and other diseases.</p>		<p>Cost Savings Action supports increased efficiencies, reduced health care expenses, and lower energy bills provide more money for other needs and investments.</p>
	<p>Green Job Creation Action encourages investment, jobs, and local dollars from construction, energy, transportation, and more.</p>		<p>Emissions Reduction Action leads to a relatively significant reduction in emissions or is a foundational step for reducing emissions.</p>
	<p>Equity Action has high potential to reduce or redress historical or current disparities.</p>		<p>Implementation Considerations Action has specific implementation considerations to ensure equitable outcomes.</p>



BUILDINGS & ENERGY

Goal: Transition to clean, renewable energy sources and reduce energy consumption.

Nearly 70% of Santa Clara’s GHG emissions come from our buildings and energy use—making it one of our greatest opportunities to reduce emissions. California state’s requirements under SB-100 are anticipated to reduce GHG emissions from electricity to net zero by 2045. To further reduce GHG emissions from our buildings and energy consumption, we will implement the following strategies.

Strategy	Estimated GHG reductions from baseline year (MTCO ₂ e)		
	2030	2035	2045
<ul style="list-style-type: none"> ▶ Shift to electric fuels in new and existing buildings to achieve net-zero carbon buildings. Actions include expanding incentive programs to support the electrification of residential and commercial buildings, transitioning all municipal buildings to electricity, and adopting construction requirements and building codes to electrify new buildings. 	40,000	79,000	194,000
<ul style="list-style-type: none"> ▶ Improve energy efficiency by using our energy more efficiently through retrofits to our homes and buildings. Actions include adopting building codes and other policies that enhance energy efficiency, offering residents and business owners rebates and other financing to offset the cost of energy efficiency upgrades, and conducting comprehensive energy efficient retrofits at municipal facilities. 	67,000	77,000	114,000
<ul style="list-style-type: none"> ▶ Maximize renewable energy generation and storage capacity. Actions include installing solar and other renewables at City-owned facilities, piloting new renewable energy technology in municipal buildings, and expanding financial support for residents to add renewable energy generation and storage systems to their homes. 	180,000	135,000	-



Strategy B1: Shift to electric fuels in new and existing buildings to achieve net-zero carbon buildings

Action B-1-1: Electrification incentives & financing



Work with BayREN and other local jurisdictions and agencies to expand current financial incentives to accelerate electrification in new and existing buildings, which could include (1) options for low- or zero-interest financing; and/or (2) targeted upstream and midstream incentives to distributors and contractors.¹⁵ Rebates could be structured by income level and prioritized for rental units to be used for panel upgrades, passive home design features, electric appliances, heat pumps, and renewable energy generation coupled with storage. The City will continue to promote and advertise available incentives through its website, newsletters, and other available platforms.

Co-benefits:

- ▶ Saves costs through incentives and financing for residents to make buildings more energy efficient.

Implementation Considerations:

- ▶ Structure rebates in a way that reduces barriers to adoption, such as offering direct install options for low-income residents.
- ▶ Target outreach to affordable housing organizations and other entities that could particularly benefit from financial incentives.
- ▶ Partner with BayREN to explore opportunities for expanding commercial incentives to large corporations as well as small- and medium-sized businesses.

¹⁵ More information on current incentives is available at <https://www.bayren.org/electrification>.



Action B-1-2: Electrification outreach for commercial & residential energy upgrades



Continue to promote commercial and residential energy efficiency and electrification upgrades (such as heat pump water heaters and electric panel upgrades) through education and outreach, which could include creation of a clearinghouse of trained/certified contractors and professionals.

Co-benefits:

- ▶ Supports green job creation by equipping local contractors with the tools they need to complete building electrification and efficiency projects.

Implementation Considerations:

- ▶ Recognize the challenge of promoting energy upgrades to certain demographics, such as renters and small business owners. Focus outreach efforts on reaching decision-makers affecting these groups, such as landlords and property owners and managers.

Action B-1-3: Electric panel upgrades upon sale/turnover

Require electric panel upgrades as needed upon sale and/or rental turnover for low-rise residential and small multifamily and commercial buildings to facilitate the transition to clean electricity buildings and vehicles.

Implementation Considerations:

- ▶ Explore options for completing upgrades across the whole building or unit-by-unit to determine the most effective approach.
- ▶ Clearly define which residential buildings are considered "small multifamily."

Action B-1-4: Municipal Electrification Action Plan

Work with regional energy partnerships to develop and implement a Municipal Electrification Action Plan for City facilities. This will include fuel switching in new and existing buildings, incorporating strategies to address energy storage, focusing on highlighting any hurdles or solutions that would be applicable to the broader community, and leveraging existing rebates.



Action B-1-5: Reach codes for new construction

Implement all-electric reach codes, with exceptions. These codes would require:

- ▶ All new single-family residences to be all electric OR mixed fuel buildings at least 10 Energy Design Rating (EDR) points less than the calculated total for the Standard Design Building.
- ▶ All new multi-family residences 3 stories or less to be all electric OR mixed fuel buildings at least 11 EDR points less than the calculated total for the Standard Design Building.
- ▶ All new non-residential/commercial construction (except office and retail) or new multi-family residences over 3 stories to be all electric OR mixed fuel buildings 6% more efficient than the 2019 State Energy Code.
- ▶ All new office or retail construction over 3 stories to be all electric OR mixed fuel buildings 14% more efficient than the 2019 State Energy Code.

Implementation Considerations:

- ▶ Explore educational campaigns and outreach opportunities to engage with local contractors, developers, and other members of the building community to ensure they have the technical capacity and knowledge to comply with reach codes.

Action B-1-6: Burnout ordinance

Prepare a “burn-out” ordinance requiring that when natural gas furnaces or water heaters expire, they must be replaced with available electric alternatives.

Implementation Considerations:

- ▶ Consider offering exemptions or financial support for qualifying residents to ensure that replacement costs are not unduly burdensome to low-income residents.
- ▶ Consider who will bear the cost of upgrades in rental units—building owners, landlords, and/or tenants—and plan outreach and financial support programs accordingly.



Action B-1-7: Carbon-neutral data centers

Require all new data centers to operate on 100% carbon neutral energy, with offsets as needed. This requirement does not apply to data centers with planning application approval within six months of the CAP adoption date.

Implementation Considerations:

- ▶ Use offsets as needed to help ease the transition to carbon neutral energy but ensure that reducing emissions remains the main priority.

Strategy B2: Improve energy efficiency

Action B-2-1: Municipal energy retrofits



Continue to conduct comprehensive energy retrofits of existing City equipment and implementation of previously identified energy efficiency projects with a benefit-cost ratio of one or greater.

Co-benefits:

- ▶ Saves energy costs to the City and supports green job growth by providing an opportunity for local contractors to complete retrofit projects.

Action B-2-2: Free home-energy upgrades for qualifying residents



Continue to provide free home-energy audits and upgrade incentives for low-income households and affordable housing developers and property owners.

Co-benefits:

- ▶ Reduces energy costs for low-income residents and supports equity by addressing the energy-inefficient housing that has caused low-income residents to face a disproportionately high energy burden.

Implementation Considerations:

- ▶ Structure incentives in a way that benefits property owners and residents to ensure that both are incentivized to participate.



Action B-2-3: Energy-efficient and electric-ready building code



Update local building code to increase energy efficiency standards to at least CALGreen Tier 1.

Co-benefits:

- ▶ Aligns Santa Clara with aggressive new energy efficiency standards that are vital to meet emissions reduction goals.

Implementation Considerations:

- ▶ Develop outreach and facilitate resource sharing among contractors, developers, and other members of the building community to ensure they have the necessary resources to comply with energy efficiency standards.

Action B-2-4: Distributed energy resource pilots

Pilot technologies like energy storage, vehicle-to-grid charging stations, web-enabled devices, and microgrids within City facilities and with private sector partners, and evaluate their ability to reduce utility costs and carbon emissions.

Action B-2-5: Energy efficiency incentives

Partner with BayREN, SVP, and other local jurisdictions and agencies to provide and promote energy efficiency incentives and rebate programs for residents and businesses.

Strategy B3: Maximize renewable energy generation and storage capacity

Action B-3-1: SVP Integrated Resource Plan (IRP) for carbon-neutral electricity



- ▶ Examine resource procurement and cost scenarios to provide 70% carbon neutral electricity to all customers by 2030 to meet SB-32 climate goals, and 60% renewable electricity to meet SB-100 climate goals. Explore SVP achieving 100% carbon neutral electricity by 2035.

Implementation Considerations:

- ▶ Structure program so that transition does not lead to higher utility rates and increase the energy burden for low-income community members.



Action B-3-2: City-owned renewable energy projects



Continue to investigate the use of City-owned property for additional large-scale carbon-neutral energy and storage projects.

Co-benefits:

- ▶ Large-scale energy and storage projects have the potential to significantly reduce emissions.

Implementation Considerations:

- ▶ Capitalize on unused local real estate, such as empty parking lots, by installing solar panels and other renewable infrastructure.

Action B-3-3: Renewable installations at municipal facilities

Install solar or other on-site renewable energy projects at City-owned facilities.

Action B-3-4: Renewable energy generation & storage on private property

Expand financial assistance options to SVP customers to increase the implementation of renewable energy generation systems and energy storage infrastructure, including streamlining of project permitting and expanding the City's solar grant program.

Implementation Considerations:

- ▶ Structure program to ensure equitable access to renewable energy technology.
- ▶ Due to split incentive dilemma between landlords and tenants, focus outreach on multi-family properties.

Action B-3-5: Local grid resiliency & energy storage improvements

Accelerate improvements to the energy grid or storage as needed to transition the city to renewable energy sources. These improvements may include subsidy and grant programs for electrification in existing buildings to reduce the cost of battery storage and electric vehicle charging/storage system installations.



Action B-3-6: Alternative fuel backup generators

Provide information and technical assistance to data centers and other large commercial users to transition from diesel to lower-carbon backup generators (e.g., renewable diesel).

Implementation Considerations:

- ▶ Consider promoting the use of fuel cells to serve as an alternative back-up power source for data centers.

Action B-3-7: Renewable electricity for new data centers

Support convening of a data center working group to identify and implement renewable electricity purchasing options for commercial customers.

The “Split Incentive Dilemma”

A “split incentive” occurs when neither landlord nor tenant is incentivized to pay for retrofits or upgrades to electrify buildings or reduce energy use. Split incentives typically occur for the following reasons:

- Building owners pay for upgrades, but tenants receive the benefits of lower utility bills. While this scenario is favorable to tenants, landlords are not incentivized to pay for upgrades when they do not directly realize cost benefits.
- Building owners pass the cost of the upgrades along to the tenants in the form of increased rent or other charges. However, tenants may be unable to accommodate the short-term cost increases or may not rent long enough to see the financial benefits of the upgrade.

Split incentives often result in renters—who are often low to moderate income—living in less efficient buildings with higher energy bills.¹ As a result, these residents often face a higher housing cost burden than homeowners. While this dilemma remains a complex issue to solve, Santa Clara can address it in part through targeted community outreach to both tenants and landlords. Green leases, Environmental Upgrade Assessments (EUAs), on-bill financing, and local code changes preventing landlords from passing on retrofit costs are also potential solutions.

1: The Greenlining Institute. [Equitable Building Electrification](#).



TRANSPORTATION & LAND USE

Goal: Transition to clean and efficient mobility options and transportation modes while maintaining accessibility and mobility for all.

The State of California's Clean Car Standards requiring that vehicles sold in California meet minimum fuel efficiency requirements—and the standards set under EO-79-20 that new cars and trucks be zero emission vehicles by 2035—are expected to drive a shift to electric vehicles. This shift will reduce emissions from transportation and help the City reach net zero by 2045. Santa Clara can build upon this legislation and further reduce emissions by further promoting widespread adoption of electric vehicles, supporting telecommute/telework strategies, and improving access to biking, walking, carpooling, and public transit. Planning to create convenient, pedestrian-friendly blocks with homes, businesses, parks, and other uses mixed together can also help reduce driving, particularly if these “mixed use” areas are located near transit stops.

Transportation measures overlap and can bolster each other’s success. For example, developing areas around transit stops will best lead to more public transit use if the transit system is reliable and efficient, and a City bike share program will be most effective after the City implements the Bicycle and Pedestrian Master Plans and addresses gaps in safe bicycling infrastructure.

Strategy	Estimated GHG Reductions from baseline year (MTCO ₂ e)		
	2030	2035	2045
<ul style="list-style-type: none"> ▶ Transition to electric vehicles by improving our EV charging infrastructure. Actions include expanding the number of public, commercial, and residential charging stations, transitioning a portion of the municipal fleet to electric vehicles, and incentivizing businesses and industries to electrify fleets. 	92,000	97,000	112,000
<ul style="list-style-type: none"> ▶ Expand clean mobility options and use of non-SOV transportation modes by making Santa Clara more bike and walk friendly, improving public transit, and incentivizing low-carbon transportation. Actions include improving the city’s trail network, promoting low-carbon transportation, and identifying transit gaps. 	152,000	163,000	142,000
<ul style="list-style-type: none"> ▶ Advance sustainable land use through policies and programs that encourage higher density development near transit centers, walkable and bikeable communities, and support telecommuting. Actions include promoting sustainable development through new building standards, incentivizing “transit-oriented” development that creates compact, walkable, mixed-use communities centered around public transit, and supporting telecommuting. 	N/A	N/A	N/A



Strategy T1: Transition vehicles to electric alternatives

Action T-1-1: Community EV Blueprint implementation

Implement the EV acceleration program in SVP's EV Blueprint to achieve EV Blueprint goals. Priority actions include:

- ▶ Expanding multi-unit dwelling and low-income charging availability.
- ▶ Electrifying City fleets and installing related charging stations.
- ▶ Expand public charging availability.
- ▶ Electrify transit fleets.

Implementation Considerations:

- ▶ Consider the location of charging stations, as well as the charging level (1,2, or 3), to make sure chargers are accessible to a diverse demographic and that they can charge a range of different vehicle models.
- ▶ Consider including charging stations at local schools and universities.

Action T-1-2: EV charging for all new construction

Implement EV charging requirements as specified in the adopted 2021 Reach Codes.

Implementation Considerations:

- ▶ Sequence actions to align EV charger requirements with building electrification incentives and electric panel upgrades to ensure that buildings are equipped to handle the increased electrical demand from EVs.



Action T-1-3: City Fleet Electrification Plan implementation

Require a percentage of new standard light-duty, medium-duty, and heavy-duty City fleet to be electric vehicles (zero emission) with EV infrastructure in accordance with Executive Order N-79-20 and related CARB regulations.¹⁶

Action T-1-4: Heavy duty electric trucks

Partner with businesses and industries to accelerate transition of heavy-duty trucks to electric through incentives or local tax credits. Heavy-duty trucks account for a significant portion of overall transportation GHG emissions, making it a priority to streamline the electrification of these vehicles.

Action T-1-5: Municipal charging infrastructure

Expand municipal facility charging infrastructure to serve municipal fleet, employee, and public charging needs.

¹⁶ California Air Resources Board. 2021. [Governor Newsom’s Zero-Emission by 2035 Executive Order](#).



Strategy T2: Expand clean mobility options and use of non-SOV transportation modes

Action T-2-1: Pedestrian & Bicycle Master Plans implementation



Fund and accelerate implementation of the Pedestrian Master Plan and Bicycle Master Plan—focusing on 1) closing gaps in the bicycle and pedestrian networks with a focus on high demand arterials; 2) installing painted buffers or physical vertical elements on high stress roadways documented in the Bicycle Master Plan; and 3) implementing spot improvements in high traffic areas (e.g., bicycle detection, bulb-outs, and wayfinding elements)—such that walking and biking comprise 10% of total city mode share.

Co-benefits:

- ▶ Enhances public health by encouraging active transportation.
- ▶ Promotes active transportation to and from school through initiatives focused on ensuring safe walk and bus routes to school, stationing school crossing guards nearby schools, and other programs.

Action T-2-2: Curb management improvements



Incentivize projects that optimize curbside areas for low-carbon modes and reduce VMT, such as designated rideshare parking and loading zones, scooter and bike share docks, bike parking, electric vehicle and bike charging stations, and autonomous vehicle loading zones.

Co-benefits:

- ▶ Enhances public health by encouraging active transportation.

Implementation Considerations:

- ▶ Consider the location and configuration of vehicle loading zones to mitigate potential safety risks to cyclists in nearby bike lanes.
- ▶ Consider the accessibility of curbs to people with disabilities as part of curb management improvements.



Action T-2-3: Bike & shared mobility improvements



Increase public access to bikes, including electric bikes, implementing a bikeshare program, expanded bike parking, electric bike rebates, and requiring new developments to include one secured bicycle parking spot for each multi-family residential unit. Electrical outlets shall be available in bike storage room for ebike charging. The City should look to prioritize low stress facilities to encourage increased ridership.

Co-benefits:

- ▶ Enhances public health by promoting non-motorized travel and saves costs for residents through rebates and shared mobility programs.

Implementation Considerations:

- ▶ Balance bike and shared mobility improvements with other local transportation needs to ensure that Santa Clara's future transportation systems meet the needs of a diverse demographic.
- ▶ Consider coordinating improvements with the VTA Countywide Bicycle Plan.

Action T-2-4: Transit gap & improvement study

Partner with VTA to conduct a public transit gap study to increase transit use within the city—such that transit comprises 12% of total city mode share. These studies focus on identifying gaps in transit service or transit facilities in areas where there is need or demand; the results will help VTA identify where to target public transportation infrastructure projects most effectively.

Implementation Considerations:

- ▶ Ensure that study includes a component focused on demographics and proximity to transit stops.
- ▶ Consider focusing study on both city and regional transit needs.



Strategy T3: Advance sustainable land use

Action T-3-1: TDM plan requirements



Continue to analyze projects through the City's VMT Policy of 15% reductions below Countywide baseline VMT through TDM measures. For new developments, projects that are exempt from the VMT analysis per the City's VMT Policy would also be exempt from additional VMT reductions per the CAP. Previously entitled projects that are already subject to VMT requirements and/or TDM measures would continue to follow the requirements assigned at the time of entitlement.

For projects that are not exempt from the VMT policy:

- ▶ Require a 25% reduction in project-based VMT through active TDM requirements for large employers over 500 employees, including aggressive regulations to reduce parking, in new developments. For the purpose of calculating the number of employees, separate employers sharing a building or project site would be treated as one employer.
- ▶ Adopt a 20% reduction of VMT for multifamily residential through active TDM requirements, which may require parking maximums, in new developments.
- ▶ Projects shall provide annual reports demonstrating compliance with VMT reduction targets, pursuant to procedures established by City staff

Co-benefits:

- ▶ Foundational step for reducing VMT, which is a vital step to reduce emissions from transportation.

Action T-3-2: Sustainable development in underutilized non-residential areas

Require higher density, mixed-use development in the Specific Plan Areas, especially El Camino Real Specific Plan. These developments should include increased building heights, zoning changes to higher density mixed residential, and consider opportunities for mixed land use and/or transit-oriented development. Quantify the net benefits of specific plans.

Action T-3-3: Transit-oriented development

Introduce requirements and/or incentives to encourage transit-oriented development (TOD) near transit corridors.



Action T-3-4: Telework

Expand telecommuting options through fiber infrastructure investment and expand existing TDM programs to include telecommuting. Explore longer term municipal employee telework policies building from existing practices.

Action T-3-5: Transportation Analysis Policy compliance

Require that all projects comply with the Transportation Analysis Policy that was adopted by Council in June 2020, which establishes requirements for evaluating the transportation impacts of residential, commercial, and industrial projects.



What Happens to Old EV Batteries?

EVs release much less pollution and fewer greenhouse gas emissions overall than conventional vehicles, but they do have a problem: recycling EV batteries is difficult. Lithium-ion batteries used by EVs are made of many individual cells held together using strong glues. They contain hazardous materials and may explode if handled incorrectly. This makes recycling dangerous, polluting, and costly.¹

Electric vehicle manufacturers delay the recycling process by giving used batteries second lives as large-scale storage systems. They have installed retired batteries as back-up power and renewable energy storage systems for arenas, convenience stores, and homes in Europe, Japan, and the United States. These strategies can extend the useful life of batteries by ten years or more. Batteries that are truly at the end of their lives may be disposed as hazardous waste or recycled via intensive processes. Lithium-ion battery recycling rates are still low (about 5% in the United States).² With more and more electric vehicles projected to hit the roads, battery recycling is an increasingly important issue, both to minimize waste and pollution and to guarantee a sustainable source of battery components into the future.

California has initiated the Lithium-ion Car Battery Recycling Advisory Group, which advises the state legislature on policies to address this issue.³ Other countries are considering rules holding battery producers responsible for recycling. In the meantime, in the United States, researchers at universities, start-ups, and everywhere in between are searching for efficient recycling techniques.² The answer will come in the form several solutions at once: new technological innovations in battery design and recycling, supporting policies, and enough recycling facilities so that transportation costs are not too high.¹

1: Science Magazine. 2021. [A Dead Battery Dilemma](#).

2: New York Times. 2021. [How green are electric vehicles?](#)

3: Cal EPA. n.d. [Lithium-ion Car Battery Recycling Advisory Group](#).



MATERIALS & CONSUMPTION

Goal: Increase diversion of waste from landfills, reduce communitywide waste generation, and reduce the upstream GHG impacts of consumption.

Waste from Santa Clara’s consumption and disposal of goods and materials contributes to climate change in several ways. Waste disposed in landfills—particularly organic waste—produces potent methane. Producing and distributing new products, in turn, generates upstream emissions from the energy used to manufacture goods and transport them around the world. Santa Clara can reduce these emissions by diverting waste from landfills, promoting sustainably sourced and produced products, and supporting programs that support reuse of existing goods—especially construction materials. California state has set a goal to divert 75% of organic waste from landfills by 2025, and to reduce edible food disposal by at least 20% by 2025. Santa Clara can align with this goal and reduce waste emissions by doing the following.

Strategy

- ▶ **Increase waste diversion.** Actions include aligning with California’s solid waste policies, promoting and expanding existing recycling and composting programs, such as the battery recycling stations located at City Hall and other public facilities, and requiring that more construction projects comply with regulations that aim to reduce construction and demolition (C&D) waste.
- ▶ **Reduce landfilled food waste** by preventing food waste and expanding edible food recovery and donation programs. Actions include food waste reduction education and outreach campaigns focused for top food waste producers, supporting local food recovery organizations, and joining food waste recovery and food security efforts.
- ▶ **Enhance sustainable production and consumption** by expanding programs that allow residents and businesses to buy less carbon-intensive goods and materials. Actions include supporting regional organizations that process and sell salvageable building materials, adopting new municipal purchasing policies that prioritize goods and materials with the lowest carbon footprint, and promoting sustainable building materials in local construction projects.



Strategy M1: Increase waste diversion

Action M-1-1: Compliance with state solid waste ordinances



Comply with state solid waste laws, including AB-1826, AB-341, and SB-1383. These bills require that businesses, public entities, and communities expand recycling and composting infrastructure to meet the state's ambitious landfill waste reduction targets. **AB-1826** requires commercial businesses that generate a certain level of organic waste arrange for recycling services for that waste. **AB-341** similarly requires that commercial businesses and public entities that generate a certain level of weekly waste have a recycling program in place. **SB-1383** requires that California reduce organic waste to landfills by 75% by 2025 and rescue 20% of surplus edible food in phases beginning in 2022.

Co-benefits:

- ▶ Significant emissions reduction benefit due to the methane emissions produced from landfill waste.
- ▶ Reduces upstream emissions from food production by diverting edible food from landfills.

Implementation Considerations:

- ▶ Consider pairing action with educational campaigns around proper waste disposal; consider integrating visuals in place of text to reach a wider audience.
- ▶ Consider partnering with neighboring communities to share resources and identify opportunities for costs savings (e.g., through contract negotiations with regional waste haulers).

Action M-1-2: Waste diversion pricing signals

Explore or promote existing incentives for recycling and composting and discouraging landfill waste. These programs provide tools and financial resources for individuals and institutions to divert waste, which collectively reduces Santa Clara's waste overall. Examples of these programs include:

- ▶ University of California Cooperative Extension Compost Education program, compost bin subsidies and compost giveaways for residents.
- ▶ Environmental Days provided by Recology, household hazardous waste drop-off events, and battery recycling stations at City Hall, Corp Yard, and Fire Stations.
- ▶ Collect used motor oil/oil filters/batteries/CFLs curbside.
- ▶ Recyclestuff.org.
- ▶ Countywide Bring Your Own Cup campaign, Reusable vs. Disposable, A La Carte, and South Bay Green Gardens.



Strategy M2: Reduce landfilled food waste

Action M-2-1: Technical assistance to top food generators



Provide education, outreach, and technical assistance to top food producers such as hotels, hospitals, corporate cafeterias, and campuses to prevent food waste, increase surplus of edible food donations, and comply with SB-1383 requirements. Options include food waste tracking software and food donation pickup services.

Co-benefits:

- ▶ Food waste in landfills is a major source of methane emissions. Partnering with food producers to reduce this waste at the source is a particularly effective strategy to reduce emissions.
- ▶ Takes pressure off of businesses who want to donate surplus edible food but lack the means to store it or implement the logistics needed for a successful program.
- ▶ Supports both public health and equity because it helps address food insecurity, which disproportionately impacts low income and minority community members.
- ▶ Reduces upstream emissions from food production by diverting edible food from landfills.

Action M-2-2: Food recovery & donation

Continue to partner with local agencies to implement an Edible Food Recovery Program as required under SB-1383. Establish an excess edible food baseline and then assist food recovery organizations in establishing pickup and redistribution.

Co-benefits:

- ▶ Potentially provides additional inventory to non-profit organizations that provide free meals but do not have enough food to expand their services, which helps reduce regional food insecurity.

Implementation Considerations:

- ▶ As part of support for recovery organizations, consider how the City can also improve access to these resources for community members who are most in need.
- ▶ Consider the nutritional quality of donated food to avoid inadvertently perpetuating existing health disparities among community members who rely on food assistance.



Action M-2-3: Food recovery organization partnerships



Participate in regional partnerships for promoting food waste reduction, recovery, and security, such as Loaves and Fishes, A La Carte, Silicon Valley Food Recovery, Second Harvest of Silicon Valley, and the Santa Clara County Food System Alliance.

Co-benefits:

- ▶ Supports both public health and equity because it helps address food insecurity, which disproportionately impacts low income and minority community members.

Implementation Considerations:

- ▶ Consider how regional collaboration can go beyond feeding those in need to also address the root causes of food insecurity so that fewer community members are dependent on hunger-relief organizations.

Strategy M3: Enhance sustainable production and consumption

Action M-3-1: Reuse of salvageable building materials

Promote organizations, such as The Reuse People, in Santa Clara County that salvage building materials. Building materials have a high amount of embodied carbon—the GHG emissions associated with producing a product. By supporting the reuse of these materials, Santa Clara can help reduce these emissions.

Action M-3-2: City property consumption & waste diversion

In all City contracts and event permits, require that all third-party vendors provide and utilize recyclable and/or reusable food service items to serve 50 or more people, and provide recycling and composting receptacles for attendees. Through this action, the City both diverts waste from landfills and shows the community its commitment to sustainability.

Implementation Considerations:

- ▶ As feasible, provide reusable cups, plates, and utensils at events and meetings or encourage event attendees to bring their own.



Action M-3-3: Municipal Sustainable Procurement Policy

Implement a municipal sustainable procurement policy to prioritize improvements for the highest emissions reduction impact purchasing decisions within each department, including vehicle and fuel purchases and low-carbon concrete. Adopting a formal policy both reduces the upstream emissions tied to producing products and shows the City's leadership on sustainability.

Implementation Considerations:

- ▶ Explore and consider the environmental sustainability related procurement practices and/or policies of City contractors and consultants.
- ▶ Explore feasibility of establishing an internal price on carbon to inform purchasing decisions.

Action M-3-4: Carbon-smart building materials

Educate architects, designers, and contractors to enable and promote carbon-sequestering and low-albedo building materials in new construction and renovations. This could include requirements for disclosing and/or limiting the embodied carbon emissions of buildings through whole-building or material specific policies. Sustainable building materials can significantly reduce emissions from construction projects; this action ensures that developers have the tools and information they need to build more sustainably.

Action M-3-5: Low-carbon schools

Partner with Santa Clara Unified School District to support low-carbon solutions. This may include working with the schools on energy efficiency and electrification, waste reduction and recycling, and sustainable purchasing. This action supports schools in reducing waste and emissions and engaging students in climate action.

Tracking Emissions on Purchased Products

To paint a more complete picture of their overall GHG emissions, communities around California and beyond are beginning to use "consumption-based" emissions inventories to estimate the emissions that are created from consumption of everyday goods and services. In future CAP updates, Santa Clara will explore expanding its current GHG inventory methodology to include these inventories to inform future climate change planning efforts and strengthen our overall approach to reducing emissions.



NATURAL SYSTEMS & WATER RESOURCES

Goal: Foster climate-resilient natural landscapes and help store more carbon in trees and soils. Conserve community water resources by maximizing water efficiency to ensure a secure and sustainable water supply in the face of climate change.

Santa Clara’s natural systems are a vital tool for both climate change mitigation and building resiliency to climate impacts. Natural systems like trees, grasses, and soils, naturally capture and store carbon—making them a vital tool for reducing emissions. These systems also provide valuable resiliency benefits, such as the natural cooling from tree shade and vegetated areas that reduces impacts from extreme heat. A commitment to a healthy urban forest and nature-based solutions has additional community and neighborhood benefits, such as improved air quality, traffic calming, reduced crime, building energy savings due to shading, and increased property values. Santa Clara’s water resources—vital for community wellbeing and public health—are vulnerable to climate impacts like extreme heat, less regional snowpack, and wildfires, which threaten water supply and water quality. Santa Clara’s Public Park System expansion, meanwhile, is essential for providing continued biodiversity and adequate land for trees and vegetation, as well as opportunities for outdoor recreation and green spaces. We can protect these valuable resources by doing the following.

Strategy	Estimated GHG Reductions from baseline year (MTCO ₂ e)		
	2030	2035	2045
<ul style="list-style-type: none"> ▶ Increase tree planting and cover. Actions include new City guidelines for replacing dying or hazardous trees, increased street tree planting, and regional collaboration on urban forests. 	N/A	N/A	N/A
<ul style="list-style-type: none"> ▶ Enhance ecosystem resilience by promoting more sustainable practices and other ecosystem management initiatives that optimize the carbon sequestration benefits of natural systems. Actions include partnering with regional groups on restoration projects aimed at increasing local carbon capture and developing a tree planting guide outlining best practices for tree management. 	40	45	60
<ul style="list-style-type: none"> ▶ Improve water supply and conservation. Actions include promoting water conservation incentive programs, expanding water efficiency retrofits and rebate programs, and diversifying the community water supply to prepare for future droughts. 	3,600	3,000	-



Strategy N1: Increase tree canopy cover

Action N-1-1: Right-of-way tree planting



Promote residential street tree planting in the right-of-way (in front of the property line). Under the City's current street tree program, all planting, pruning and removal of street trees is provided to residents at no charge.

Co-benefits:

- ▶ Trees have valuable climate resiliency benefits, including improved air quality, traffic calming, reduced crime, and building energy savings due to shading, including providing cooling through natural shade
- ▶ Trees provide aesthetic appeal, increase property values, facilitate a sense of community, and are also vital for overall ecosystem health.

Implementation Considerations:

- ▶ Ensure that tree planting is administered community-wide, particularly in lower income neighborhoods that historically have had less green space.
- ▶ Consider the species and size of trees to avoid future damage to streets and sidewalks.
- ▶ Use regional heat island study data to determine where to prioritize tree planting.

Action N-1-2: Private property tree planting support



Support private property planting of trees through partnerships with organizations such as Our City Forest. Advertise services on the City website.

Co-benefits:

- ▶ Trees have valuable climate resiliency benefits, including improved air quality, and building energy savings due to shading.
- ▶ Trees provide aesthetic appeal, increase property values, facilitate a sense of community, and are also vital for overall ecosystem health and have can provide traffic calming effects and reduced crime.

Implementation Considerations:

- ▶ Consider pairing action with education and outreach campaigns that build upon past successful City tree programs to promote proper tree maintenance and highlight the value that trees bring to the community.



Action N-1-3: Urban forest partnerships



Promote healthy, well-managed urban forests by participating in the County's Urban Forest Alliance partnership.

Action N-1-4: Tree maintenance, replacement, and plantings



Update Street Tree Planting plan to develop a procedure for retiring and replacing trees (when they are dying or creating hazards), with an emphasis on species that maintain tree canopy, and prevent unintended consequences, such as sidewalk uplifts from tree root growth. Collaborate with the community on appropriate tree maintenance, replacement, and plantings as per City Tree Ordinance.

Co-benefits:

- ▶ Trees have valuable climate resiliency benefits, including improved air quality, traffic calming, reduced crime, and building energy savings due to shading, including providing cooling through natural shade
- ▶ Trees provide aesthetic appeal, increase property values, facilitate a sense of community, and are also vital for overall ecosystem health.

Carbon Sequestration Opportunities in Santa Clara

Santa Clara has an estimated 10,500 city street trees and 13 miles of creeks. This urban forest and other biomass in parks and protected natural areas represent a "stock" of sequestered carbon. It is important to maintain this valuable stock of stored carbon through the maintenance and replacement of trees and plants on an ongoing basis. While this established stock of stored carbon is not able to function as a credit in the greenhouse gas analysis, any additional trees that are planted or the restoration of riparian, estuary, or other ecosystems can be counted toward GHG reductions.

Opportunities include increasing the extent of the urban forest by planting in currently empty tree wells or replacement of dead trees in parks. Additional trees can also be planted in parks and public rights of way that have not had street trees previously in parks and in public property and rights of way. An additional 1,600 trees, or a 15% increase over current stock, would result a carbon drawn down of approximately 1,060 tons of carbon by 2045.¹

Creek and riparian restoration is another option to increase carbon sequestration. Calabazas Creek, Saratoga Creek, and San Tomas Aquino Creek all flow through the City. These creek areas are generally in a moderately degraded ecological condition. Ecological restoration, planting of additional trees and woody shrubs, combined with ongoing maintenance, would enable additional carbon to be stored in the soil and additional biomass. Improving the ecological condition of the riparian areas in the City could result in the drawdown of an additional 1,200 tons of carbon by 2050.²

¹ Based on annual carbon factors from CAPCOA | ² Based on analysis using the CREEC tool developed by the California Department of Conservation.



Strategy N2: Enhance ecosystem resilience

Action N-2-1: Carbon farming on open space lands



Partner with resource conservation districts to increase carbon farming, creek restoration, wetland restoration, and local offset opportunities in open space lands within the city limits of Santa Clara.

Co-benefits:

- ▶ Carbon sequestration fills the emissions gap left by other emission mitigation strategies, making it a vital tool for reducing emissions and improving Santa Clara's overall long-term climate resiliency.
- ▶ Supporting natural ecosystems on open space lands provides cooling and aesthetic benefits, and supports species habitats.

Action N-2-2: Partnerships for compost management

Explore potential partnerships with organizations such as conservation districts to manage and utilize compost products from organics processing in compliance with SB-1383.

Action N-2-3: Sustainable planting guide

Support local organizations in developing a planting guide that prioritizes increasing available soil, carbon sequestration, resilience, and other equitably distributed co-benefits. The guide could include information on native and climate-adaptive plants, how to properly apply compost and mulch, reducing synthetic fertilizers to support soil health, how to reduce water use and store more water in the ground, and how to store carbon in soil, plants, and trees.

Action N-2-4: Sustainable park management

Utilize sustainable park management practices, including continuing to convert from gas to E-powered landscape tools and increasing recycled water use in public parks.



Strategy N3: Improve water supply and conservation

Action N-3-1: Water conservation rebates



Conduct outreach to encourage participation in Santa Clara Valley Water District's (Valley Water) water conservation rebate programs, available to single-family homes, multi-family buildings, and businesses. Expand the City's rain barrel and landscape rebate programs.

Co-benefits:

- ▶ Supports ecosystem health by conserving valuable water resources, which makes Santa Clara more resilient against future water shortages. Water conservation programs can also provide cost savings for residents by reducing water bills.

Implementation Considerations:

- ▶ Conduct tailored outreach to groups that have been historically harder to reach, such as renters and small business owners.

Action N-3-2: Fixture replacements



Expand replacement incentives, such as Santa Clara Valley Water District (Valley Water) rebate programs, of inefficient water fixtures and appliances in high-end sectors. High-end sectors include commercial, multi-family, and single-family properties.

Co-benefits:

- ▶ Supports ecosystem health by conserving valuable water resources, which makes Santa Clara more resilient against future water shortages. Replacing inefficient water fixtures can also provide cost savings for residents by reducing water bills.



Action N-3-3: Water-efficient landscaping requirements



Expand requirements for water-efficient landscaping practices, including requirements for cooling (trees, green roofs) and drought-tolerant native plants. Update the Model Water Efficient Landscape Ordinance (MWELO) to apply to landscape renovations of 1,000 square feet or larger.

Co-benefits:

- ▶ Adopting water-efficient landscaping practices today is vital for conserving future water resources.

Action N-3-4: Community water portfolio diversification



Continue collaboration with agency partners such as South Bay Water Recycling, Valley Water, BAWSCA, and SFPUC to diversify water supply portfolio and expand current sources. Diversified water portfolio towards drought resiliency could include utilizing a varying mix of surface and groundwater and requiring the increased use of recycled urban water in applicable sectors (e.g., irrigation, groundwater recharge, dual pump plumbing, cooling towers).

Co-benefits:

- ▶ Diversifying the water portfolio is a vital and foundational step for building resiliency against future water shortage from climate change.

Action N-3-5: Recycled water connection requirements



Require the use of recycled water for all non-potable uses where recycled water is available, per City Code 13.15.160. Require all new development where applicable to connect to the recycled water distribution system in order to provide recycled water for approved uses at the development site.

Co-benefits:

- ▶ Recycled water is a particularly effective way of conserving water resources and is an important foundational step for building resiliency against future water shortage from climate change.



COMMUNITY RESILIENCE & WELL-BEING

Goal: Ensure Santa Clara is prepared and can withstand climate and non-climate emergencies, focusing on those at highest risk.

Climate change poses a significant risk to the health and safety of our residents, particularly vulnerable populations, including the elderly and low-income community members. Unless we prepare today, climate impacts—including wildfires, extreme weather events, and flood events—will stress our emergency services and disrupt other vital services. In light of these threats, the Governor’s Office of Planning and Research (OPR) General Plan guidance now requires that California cities and counties include specific resilience and adaptation policies in general plans. Santa Clara can align with these guidelines and build resiliency to climate impacts by doing the following.

Strategy

- ▶ **Improve community resilience** by expanding public programs to prepare community members for climate impacts. Actions include expanding disaster relief by developing a network of community resiliency centers—including cooling centers—where residents can go in case of a climate-related emergency. Actions also include expanding public programs that support vulnerable populations, including people experiencing homelessness, and encouraging community-led climate action and adaptation initiatives through grants and other financial incentives.
- ▶ **Prepare for climate change** by strengthening vital infrastructure and adopting new climate-related regulations for development and capital improvement projects. Actions include integrating natural stormwater systems into site and building designs, requiring new parking lots be paved with more sustainable materials, and identifying and potentially relocating critical facilities threatened by severe climate impacts.



Strategy C1: Improve community resilience

Action C-1-1: Community resilience networks



Support neighborhood-based organizations and businesses in development of Neighborhood Resilience Hub Programs to prepare residents and respond to climate change. Identify suitable locations for resilience hubs, cooling centers, disaster assistance and supplies. These locations will also need to develop backup power sources in the event of a power outage.

Co-benefits:

- ▶ Foundational climate resiliency step to prepare our community for natural disasters and other climate impacts.

Implementation Considerations:

- ▶ Prioritize locating resilience hubs in communities with vulnerable populations who are most susceptible to climate impacts.
- ▶ Consider developing targeted outreach campaigns to local businesses to encourage them to offer commercial spaces for resilience hubs.

Action C-1-2: Support for people experiencing homelessness



Expand support services to people experiencing homelessness during all extreme weather and hazard events (e.g., extreme heat, flooding, wildfires).

Co-benefits:

- ▶ Enhances public health by protecting community members who are most directly threatened by climate impacts, like extreme heat. It also promotes equity by expanding support to a marginalized demographic.

Implementation Considerations:

- ▶ Expand outreach on existing support services as well as developing new programs.



Action C-1-3: Community climate action grant

Establish an annual micro-grant program to support local citizen-led projects and programs that will reduce emissions, adapt to climate change, and enhance equity.

Action C-1-4: Incentives for adaptation upgrades



Offer rebates and/or other financial incentives to encourage adaptation upgrades (e.g., cool roofs, green roofs, cool pavement) and installation of low-emissions space-cooling devices (e.g., ceiling fans, heat pumps), which increase resilience cost-effectively and with a lower environmental impact.

Co-benefits:

- ▶ Has important climate resiliency impacts by making our homes and buildings operate more efficiently, which reduces strain on the energy grid. This action can also provide cost savings by lowering energy bills.
- ▶ Adaptation upgrades reduce emissions, supporting carbon reduction goals.

Implementation Considerations:

- ▶ Consider sequencing of this action with others focused on residential and commercial energy upgrades, such as energy efficiency and electrification incentives.

Strategy C2: Prepare for climate change

Action C-2-1: Climate resilient land use & development



Evaluate city land use maps to identify whether new development is being planned in high-risk areas, such as those projected to experience increased flood risk under climate project scenarios. Consider developing guidance that would require evaluation of projected flood risk for proposed projects or limit building within identified high hazard zones. Prioritize more adaptable land uses in high hazard areas, such as parks and green space.



Action C-2-2: On-site & natural stormwater systems



Integrate natural stormwater systems within site and building design to expand on-site stormwater management capacity. Natural stormwater systems reduce pollution to waterways, conserve water resources, and reduce flood risks.

Co-benefits:

- ▶ Protects public health and ecosystem health by protecting water quality.

Action C-2-3: High-albedo parking lots

As part of conditions of approval, require new parking lots to be surfaced with more sustainable pavement materials (e.g., high-albedo, permeable pavement, e-pavement, etc.) to reduce heat gain during extreme heat events, reduce energy consumption related to cooling, and reduce stormwater runoff.

Action C-2-4: Climate Resiliency Capital Improvement Program (CIP)



Incorporate climate resiliency strategies and considerations in development of discretionary CIP projects, including new parks projects. Review design standards to incorporate climate resiliency considerations as appropriate.

Co-benefits:

- ▶ Foundational step for improving climate resiliency by reducing exposure of vital infrastructure to future climate impacts.

Action C-2-5: Planned retreat strategies



Identify and consider relocation opportunities for critical facilities (i.e., planned retreat for structures at risk of recurring damages). By identifying these locations, the City can proactively prepare for and reduce future impacts—saving costs and resources and reducing the threat to community members.

Co-benefits:

- ▶ Foundational step for improving climate resiliency by reducing exposure of vital infrastructure to future climate impacts.



IMPLEMENTING THE CAP

This CAP provides a coordinated and intentional strategy for the City of Santa Clara to meet its GHG emissions reduction targets while building community resilience. Making progress on these goals will require the City and community to work together and commit dedicated time and resources. The following section provides a framework for the next phase of this project—moving from planning to action.

IMPLEMENTATION SCHEDULE

Year 1 Implementation Summary

In 2022, Santa Clara will begin building upon its existing foundation for plan implementation by taking several key initial steps and implementing priority actions (refer to Appendix A: Implementation Matrix for action implementation timeframes beyond year 1). Key milestones include:

- ▶ Ensure **updates to other planning documents** that implement the CAP are consistent with the adopted plan. Designate potential **funding sources** for CAP actions and identify additional funding needs and opportunities for ongoing plan implementation. Current and potential funding sources could include City General Fund; utility revenues; federal and state grants; revolving loan funds; potential new local revenue streams; potential increased utility taxes; and public/private partnerships.
- ▶ Establish **oversight/accountability** function and identify **specific equity criteria and indicators** to consider during monitoring and evaluation of future CAP progress.
- ▶ Establish plans for internal City **resource sharing, outreach and education, and promoting incentives**. Immediate residential and commercial outreach campaigns are currently planned for promoting induction cooktops. Internal efforts are also planned for training departments on implementing reach codes.
- ▶ **Engage key stakeholders**; work closely with NGO's, non-profit organizations and other community partners to expand reach and impact of CAP actions. Continue to engage with SVP to support CAP actions related to energy.
- ▶ **Begin implementation** of the following actions:
 - ✓ Implement adopted **reach codes**.
 - ✓ Update local building code to **increase energy efficiency standards**.
 - ✓ Examine resource procurement and cost scenarios to provide **100% carbon neutral electricity to all SVP customers**.
 - ✓ Take action to **comply with state solid waste ordinances**, including AB-1826, AB-341, and SB-1383.



Synergy Between CAP Actions and Reach Codes

CAP actions focus on the adoption of reach codes, and prioritizes their implementation in Year 1, because of the synergy between these two planning efforts. All-electric building electrification and electric vehicle charging reach codes, for instance, call for all electric appliances in new construction and pre-wiring for EV charging, which supports the larger CAP goals of electrifying buildings and transitioning toward electric vehicles. In implementing the CAP and developing reach codes, the City will continue to align efforts to ensure that both initiatives support and build upon each other. Further development of reach codes will continue to focus on meeting the recommended CAP targets, and the City may update reach codes with the State Building Code cycle as needed to support GHG reduction goals.

OVERSIGHT & ACCOUNTABILITY

Options for developing an ongoing structure for oversight of CAP implementation and long-term plan updates include:

- ▶ Create an internal Sustainability and Climate Action Team (led by the City's Sustainability Manager) to assist in coordinating and implementing actions across departments, identifying synergies/collaboration opportunities, and identifying funding sources.
- ▶ Prepare annual updates for the Planning Commission and City Council on CAP progress.
- ▶ Develop and maintain a Climate Action Tracking Dashboard.

MONITORING & EVALUATION

In the first year of implementation, the City will identify staff responsible for:

- ▶ Monitoring progress toward CAP target achievement.
- ▶ Preparing annual progress reports for review and consideration by the Planning Commission and City Council.

Staff will update the City's emissions inventory every three to five years, with the next CAP update planned for 2030. These updates are also opportunities to amend the CAP as necessary, should the City find that specific measures are not achieving intended emissions reductions.

In preparation for the 2030 update and annual reporting to the Planning Commission and City Council, staff will use an Excel-based CAP monitoring and reporting tool to track Santa Clara's progress in reducing emissions, VMT, waste generation, and energy use over time. The tool is used to collect data, track GHG emissions, and assess the implementation of measures. It enables the City to sort measures based on timing, responsible department, and level of success, progress, or completion.



IMPLEMENTING INCENTIVES

Incentives are an important tool for motivating businesses and residents to voluntarily participate in climate actions, and are integrated in actions throughout the CAP. In implementing these actions, the City will focus both on promoting existing regional rebates and other incentive programs, as well as exploring options for non-financial incentives to motivate behavior change.

Existing regional incentives programs that the City will promote include the following:

- ▶ **SVP Commercial Rebates** | Silicon Valley Power offers a variety of rebates to Santa Clara businesses to incentivize energy efficiency investments. Rebates cover a range of projects, including energy efficient equipment upgrades, installation of energy management control systems, large-scale efficiency upgrades for data centers, and installation of emerging technologies that are not yet commercially available.
- ▶ **SVP Residential Rebates** | Silicon Valley Power offers Santa Clara residents free home energy audits, as well as rebates to fund recommended energy efficiency and electrification upgrades, and other products. Rebates can be applied to purchases of electric bicycles, clothes dryers, electric vehicle charging stations, and electric heat pump water heaters. The program also offers low-income residents grants for free solar panel installation and rebates for electric vehicle purchases.
- ▶ **BayRen Free Home Energy Savings Kit** | BayREN provides a free custom energy savings kit featuring energy and water efficiency products. Kits are personalized to each customer based on their home energy usage. The kit may include faucet aerators, power strips, LED light bulbs, and high efficiency showerheads.
- ▶ **BayRen Home Energy Rebates** | BayREN provides rebates for home energy upgrades to Bay Area residents living in homes built in 2016 or earlier. Each customer may receive up to \$5,000 of rebates on products, including: induction cooktops, air sealing services, insulation, heat pumps, energy efficient air conditioning, tankless water heaters, and more.
- ▶ **Santa Clara Valley Water Rebate Programs** | Valley Water offers a range of water conservation rebates to fund residential and commercial water efficiency upgrades, including converting from high-water use landscaping, installing graywater laundry systems, and implementing large-scale building upgrades.

Non-financial incentives that the City will explore include the following:

- ▶ **Expedited permitting** for residential and nonresidential renewable energy generation and storage systems.
- ▶ **Develop community awards** that highlight leadership in climate resiliency and emissions reductions.



APPENDIX A: IMPLEMENTATION MATRIX

Legend:	Low	Moderate	High
Impact: <ul style="list-style-type: none"> ▶ How likely is the action to address plan goals and targets? ▶ Does the action address a major sustainability need? 	🌿	🌿🌿	🌿🌿🌿
Cost: <ul style="list-style-type: none"> ▶ How much does this action cost to residents and businesses? ▶ How much does this action cost to the City of Santa Clara? 	\$	\$ \$	\$ \$ \$
Implementation Timeframe: <ul style="list-style-type: none"> ▶ Ongoing – action is already underway ▶ Year 1 – action implementation in Year 1 (2022) ▶ Near-term – action implementation in 2023-2026 ▶ Mid-term – action implementation in 2027-2030 ▶ Long-term – action implementation after 2030 			



Action ID	Action Description	Impact	Cost	Implementation Timeframe	Timeframe Considerations	Lead Department
Strategy B1: Shift to electric fuels in new and existing buildings to achieve net-zero carbon buildings.						
B-1-1	Electrification incentives & financing. Work with BayREN and other local jurisdictions and agencies to expand current financial incentives to accelerate electrification in new and existing buildings.	🌿 🌿	\$	Near-term	Incentives ease transition to mandates; electrification of building sector is high priority for addressing a major emissions source.	SVP
B-1-2	Electrification outreach for commercial & residential energy upgrades. Continue to promote commercial and residential energy efficiency and electrification through education and outreach.	🌿	\$	Ongoing	Already underway.	SVP
B-1-3	Electric panel upgrades upon sale/turnover. Require electric panel upgrades as needed upon sale and/or rental turnover for low-rise residential and small multifamily and commercial buildings.	🌿 🌿	\$ \$	Mid-term	Will require significant stakeholder engagement and buy-in as a next phase in building electrification efforts to compliment reach codes.	CDD – Building, CMO (Sustainability)
B-1-4	Municipal Electrification Action Plan. Work with regional energy partnerships to develop and implement an Electrification Action Plan for City facilities.	🌿	\$ \$	Near-term	Important to plan early to allow time for implementation; City can be a leader for the broader community.	DPW – Facilities, CMO (Sustainability)
B-1-5	Reach codes for new construction. Implement all-electric reach codes, with exceptions.	🌿 🌿	\$ \$	Year 1	All-Electric building electrification and EV Charging reach codes adopted in November 2021, Effective in 2022; start realizing benefits in new buildings right away,	CDD – Building, CMO (Sustainability)



Action ID	Action Description	Impact	Cost	Implementation Timeframe	Timeframe Considerations	Lead Department
B-1-6	Burnout ordinance. Prepare a "burn-out" ordinance requiring that when natural gas furnaces or water heaters expire, they must be replaced with electric alternatives.	🌿 🌿	\$ \$	Mid-term	May require some stakeholder engagement and buy-in.	CDD - Building
B-1-7	Carbon neutral data centers. Require all new data centers to operate on 100% carbon neutral energy, with offsets as needed.	🌿	\$ \$ \$	Year 1	Important to establish with new data centers to start realizing benefits right away.	CDD – Planning/SVP
Strategy B2: Improve energy efficiency.						
B-2-1	Municipal energy retrofits. Continue to conduct comprehensive energy retrofits of existing City equipment and implementation of previously identified energy efficiency projects with a benefit-cost ratio of one or greater.	🌿	\$ \$ \$	Ongoing	Already happening; preliminary planning and analysis already completed.	DPW - Facilities
B-2-2	Free home-energy upgrades for qualifying residents. Continue to provide free home-energy audits and upgrade incentives for low-income households and affordable housing developers and property owners.	🌿	\$ \$	Ongoing	Program already in place.	SVP
B-2-3	Energy-efficient and electric-ready building code. Update local building code to increase energy efficiency standards to at least CALGreen Tier 1.	🌿 🌿 🌿	\$ \$	Year 1	Reach codes already moving forward for adoption.	CDD - Building



Action ID	Action Description	Impact	Cost	Implementation Timeframe	Timeframe Considerations	Lead Department
B-2-4	Distributed energy resource pilots. Pilot technologies like energy storage, vehicle-to-grid charging stations, web-enabled devices and microgrids within City facilities and with private sector partners.		\$ \$	Near-term	Conduct pilots early on to learn from results and build out programs or infrastructure.	DPW/SVP and other departments as applicable
B-2-5	Energy Efficiency incentives. Partner with BayREN, SVP, and other local jurisdictions and agencies to provide and promote energy efficiency incentives and rebate programs for residents and businesses.		\$	Ongoing	Many incentive programs are already in place. Next step is focused on expanding outreach to promote existing programs.	SVP
Strategy B3: Maximize renewable energy generation and storage capacity.						
B-3-1	SVP Integrated Resource Plan (IRP) for carbon neutral electricity. Examine resource procurement and cost scenarios to provide 70% carbon neutral electricity to all customers by 2030 to meet SB-32 climate goals, and 60% renewable electricity to meet SB-100 climate goals. Explore SVP achieving 100% carbon neutral electricity by 2035.		\$ \$	Year 1	SVP's upcoming IRP plan is due to Council in 2023. Critical to move this action forward to achieve targets.	SVP



Action ID	Action Description	Impact	Cost	Implementation Timeframe	Timeframe Considerations	Lead Department
B-3-2	City-owned renewable energy projects. Continue to investigate the use of City-owned property for additional large-scale carbon-neutral energy and storage projects.	🌿 🌿 🌿	\$ \$ \$	Near-term	Study should come early so that there is time to inform infrastructure development. Could be considered as part of SVP's next IRP planning process; SVP transitioning to renewable energy is a key precursor to electrification benefits.	SVP
B-3-3	Renewable installations at municipal facilities. Install solar or other on-site renewable energy projects at City-owned facilities.	🌿 🌿	\$ \$	Near-term	Put in place while waiting for grid to get cleaner.	Cross departmental (e.g., SVP, DPW, Library)
B-3-4	Renewable energy generation and storage on private property. Expand financial assistance options to SVP customers to increase the implementation of renewable energy generation systems and energy storage infrastructure.	🌿	\$ \$	Mid-term	Residential electricity is already carbon fee. Focus in near term is electrification rather than renewable energy generation.	SVP
B-3-5	Local grid resiliency & energy storage improvements. Accelerate improvements to the energy grid or storage as needed to transition to the city to renewable energy sources.	🌿 🌿	\$ \$	Near-term	Put in place while waiting for grid to get cleaner.	CDD - Building
B-3-6	Alternative fuel backup generators. Provide information and technical assistance to data centers to transition from diesel to lower-carbon backup generators.	🌿	\$ \$	Mid-term	Look into availability of renewable diesel? Is supply a constraint?	CDD-Planning, SVP



Action ID	Action Description	Impact	Cost	Implementation Timeframe	Timeframe Considerations	Lead Department
B-3-7	Renewable electricity for new data centers. Support convening of a data center working group to identify and implement renewable electricity purchasing options for commercial customers.		\$	Near-term	Planning effort—will take time to ramp up and implement; need to put into place while waiting for electricity grid to become cleaner.	SVP
Strategy T1: Transition vehicles to electric alternatives.						
T-1-1	Community EV Blueprint Implementation. Implement the EV acceleration program in SVP's EV Blueprint to achieve EV Blueprint goals.	 	\$ \$	Ongoing	Already underway.	SVP
T-1-2	EV charging for all new Construction Implement EV charging requirements as specified in the adopted 2021 Reach Codes.	 	\$ \$	Year 1	Reach codes already adopted November 16, 2021.	CDD - Building
T-1-3	City Fleet Electrification Plan implementation. Require a percentage of new standard light-duty, medium-duty, and heavy-duty City fleet to be electric vehicles (zero emission) with EV infrastructure in accordance with state requirements.	 	\$ \$	Near-term	City lead by example.	DPW – Fleet Services
T-1-4	Heavy duty electric trucks. Partner with businesses and industries to accelerate transition of heavy-duty trucks to electric through incentives or local tax credits.	 	\$ \$	Mid-term	Technology is still being developed; wait until statewide policy/incentives may be available.	CMO



Action ID	Action Description	Impact	Cost	Implementation Timeframe	Timeframe Considerations	Lead Department
T-1-5	Municipal charging infrastructure. Expand municipal facility charging infrastructure to serve municipal fleet, employee, and public charging needs.	🍃	\$ \$ \$	Near-term	City lead by example.	DPW
Strategy T2: Expand clean mobility options and use of non-SOV transportation modes.						
T-2-1	Pedestrian & Bicycle Master Plans. Fund and accelerate implementation of the Pedestrian Master Plan and Bicycle Master Plan.	🍃 🍃	\$ \$ \$	Near-Term	Plans have already been developed; high priority for the community.	DPW - Traffic
T-2-2	Curb management improvements. Incentivize projects that optimize curbside areas for low-carbon modes and reduce VMT.	🍃 🍃	\$ \$	Mid-term	High priority for Council; City has more control; incentive carrots before sticks; develops infrastructure that supports future actions.	DPW - Traffic
T-2-3	Bike and shared mobility improvements. Increase public access to bikes, including electric bikes, through bikeshare programs and improved bike infrastructure.	🍃 🍃	\$ \$	Near-term	High priority from community; ties into Bike/Ped Plan implementation.	DPW-Traffic
T-2-4	Transit gap & improvement study. Partner with VTA to conduct a public transit gap study to increase transit use within the City.	🍃 🍃	\$ \$	Long-term	Action informs future strategies/ infrastructure.	DPW-Traffic
Strategy T3: Advance sustainable land use.						



Action ID	Action Description	Impact	Cost	Implementation Timeframe	Timeframe Considerations	Lead Department
T-3-1	TDM plan requirements. Require a 20-25% VMT reduction through active TDM requirements for non-exempt projects, for new developments. Previously entitled projects that are already subject to VMT requirements and/or TDM measures would continue to follow the requirements assigned at the time of entitlement. ¹⁷		\$ \$	Near-term	A Council priority.	CDD - Planning

¹⁷ For Action T-3-1, previously entitled projects that are already subject to VMT requirements and/or TDM measures would continue to follow the requirements assigned at the time of entitlement. In addition, projects in the following General Plan land use designations would be required to reduce project-based VMT through TDM requirements at the following percentages:

Land Use Designation	Minimum % VMT reduction from TDM
Santa Clara Station Very High Density Residential	Pre-BART: ¹ 10% Post-BART: 20%
Urban Center/Entertainment ²	Office: 4% Residential: 2%
Transit Neighborhood	10%

1. Per Council Resolution 19-8734, the VMT reduction per project from TDM for the Santa Clara Station Very High Density Residential designation shall be 10% prior to the Santa Clara BART station becoming operational, and 20% subsequent to the Santa Clara BART station becoming operational.
2. For the Urban Center / Entertainment District, the VMT reduction requirements apply to the office and residential uses within that district, and the reduction requirements are specific to those two categories of uses.



Action ID	Action Description	Impact	Cost	Implementation Timeframe	Timeframe Considerations	Lead Department
T-3-2	Sustainable development in underutilized non-residential areas. Require higher density, mixed-use development in the Specific Plan Areas, especially El Camino Real Specific Plan.		\$ \$ \$	Mid-term	Foundation of the City's Housing Element.	CDD - Planning
T-3-3	Transit-oriented development. Introduce requirements and/or incentives to encourage transit-oriented development (TOD) near transit corridors.		\$ \$	Near-term	Puts in place important infrastructure; development is a direct lever that the City has to influence change; requires planning to determine what the requirements and/or incentives will include.	CDD - Planning
T-3-4	Telework. Expand telecommuting options through fiber infrastructure investment and expand existing TDM programs to include telecommuting.		\$	Near-term	High impact action and telework becoming increasingly common—prioritize implementation.	CMO, SVP, DPW
T-3-5	Transportation Analysis Policy compliance. Require that all projects comply with the Transportation Analysis Policy that was adopted by Council in June 2020, which establishes requirements for evaluating the transportation impacts of residential, commercial, and industrial projects.		\$	Year 1	Ordinances have been enacted and require immediate compliance.	CDD - Planning
Strategy M1: Increase waste diversion.						



Action ID	Action Description	Impact	Cost	Implementation Timeframe	Timeframe Considerations	Lead Department
M-1-1	Compliance with state solid waste ordinances. Comply with state solid waste laws, including AB-1826, AB-341, and SB-1383.	🌿 🌿 🌿	\$	Year 1	Ordinances have been enacted and require immediate compliance.	DPW - Streets
M-1-2	Waste diversion pricing signals. Explore or promote existing incentives for recycling and composting and discouraging landfill waste.	🌿 🌿	\$	Ongoing	Incentives already underway.	DPW - Streets
Strategy M2: Reduce landfilled food waste.						
M-2-1	Technical assistance to top food generators. Provide education, outreach, and technical assistance to top food producers to prevent food waste, increase surplus of edible food donations, and comply with SB-1383 requirements.	🌿 🌿 🌿	\$ \$	Near-term	Supports compliance with existing ordinance; education and outreach support long-term behavior change.	DPW - Streets
M-2-2	Food recovery & donation. Continue to partner with local agencies to implement an Edible Food Recovery Program as required under SB-1383.	🌿 🌿	\$ \$	Near-term	Need to reduce organic disposal 75% by 2025 (based on 2014 levels).	DPW - Streets
M-2-3	Food recovery organization partnerships. Participate in regional partnerships for promoting food waste reduction, recovery, and security.	🌿	\$ \$	Mid-term	Ongoing initiative with no vital start date—other actions take priority. Action is focused on regional partnerships, so Santa Clara has less control over process/outcomes.	DPW - Streets
Strategy M3: Enhance sustainable production and consumption.						



Action ID	Action Description	Impact	Cost	Implementation Timeframe	Timeframe Considerations	Lead Department
M-3-1	Reuse of salvageable building materials. Promote organizations, such as The Reuse People, in Santa Clara County that salvage building materials.	🌿	\$	Mid-term	(By weight) large source of waste stream; however, typically does not contain organics; low or no emissions associated with building materials. Helps achieve diversion goals but low impact on emissions.	CDD-Building
M-3-2	City property consumption & waste diversion. In all City contracts and event permits, require that all third-party vendors provide and utilize recyclable and/or reusable food service items to serve 50 or more people, and provide recycling and composting receptacles for attendees.	🌿	\$	Mid-term	Shows City leadership, but lower impact on total emissions.	Finance/CMO - Sustainability
M-3-3	Municipal Sustainable Procurement Policy. Implement a municipal sustainable procurement policy to prioritize improvements for the highest emissions reduction impact purchasing decisions within each department, including vehicle and fuel purchases and low-carbon concrete.	🌿 🌿	\$	Mid-term	Demonstrates the City's commitment/leadership. Will take time to develop and approve policy.	Finance/CMO - Sustainability
M-3-4	Carbon-smart building materials. Educate architects, designers, and contractors to enable and promote carbon-sequestering and low-albedo building materials in new construction and renovations.	🌿 🌿	\$ \$	Long-term	Ongoing future initiative, does not require immediate start date; other initiatives take priority.	CMO - Sustainability



Action ID	Action Description	Impact	Cost	Implementation Timeframe	Timeframe Considerations	Lead Department
M-3-5	Low-carbon schools. Partner with Santa Clara Unified School District to support low-carbon solutions.	🌿 🌿	\$	Long-term	May take some ramp up time; likely to have relatively low impact; prioritize more impactful near-term options.	SVP
Strategy N1: Increase tree plantings and canopy cover.						
N-1-1	Right-of-way tree planting. Promote residential street tree planting in the right-of-way.	🌿 🌿	\$ \$	Near-term	Not immediate priority for reducing emissions; could start with outreach then build up to requirement.	DPW - Streets
N-1-2	Private property tree planting support. Support private property planting of trees through partnerships with organizations such as Our City Forest.	🌿 🌿	\$ \$	Mid-term	Not immediate priority for reducing emissions; current programs already happening. Could build upon/scale in the future.	DPW – Streets/CDD
N-1-3	Urban forest partnerships. Promote healthy, well-managed urban forests by participating in the County's Urban Forest Alliance partnership.	🌿	\$	Year 1	Not immediate priority for reducing emissions; relatively easily to facilitate so may warrant near-term implementation.	DPW – Streets/CMO Sustainability
N-1-4	Tree maintenance, replacement, & plantings. Update Street Tree Planting plan to develop a procedure for retiring and replacing trees.	🌿	\$ \$	Mid-term	Not immediate priority for reducing emissions; co-benefits (increased sidewalk accessibility; aesthetics) could warrant doing in mid-term vs long-term.	DPW - Streets
Strategy N2: Enhance ecosystem resilience.						



Action ID	Action Description	Impact	Cost	Implementation Timeframe	Timeframe Considerations	Lead Department
N-2-1	Carbon farming on open space lands. Partner with resource conservation districts to increase carbon farming, creek restoration, wetland restoration, and local offset opportunities in open space lands within the city limits of Santa Clara.	🌿 🌿	\$ \$	Long-term	Carbon sequestration lower priority (prioritize emissions reduction).	DPW – Streets/CMO Sustainability
N-2-2	Partnerships for compost management. Explore potential partnerships with organizations such as conservation districts to manage and utilize compost products from organics processing in compliance with SB-1383.	🌿 🌿	\$	Near-term	Supports compliance with SB1383 (requirements by 2025).	DPW - Streets
N-2-3	Sustainable planting guide. Support local organizations in developing a planting guide that prioritizes increasing available soil, carbon sequestration, resilience, and other equitably distributed co-benefits.	🌿	\$	Long-term	Carbon sequestration lower priority (prioritize emissions reduction). Outcomes uncertain.	DPW - Streets
N-2-4	Sustainable park management. Utilize sustainable park management practices, including continuing to convert from gas to E-powered landscape tools and increasing recycled water use in public parks.	🌿	\$ \$	Near-term	City lead by example; some initiatives already underway.	Parks & Recreation
Strategy N3: Improve water supply and conservation.						



Action ID	Action Description	Impact	Cost	Implementation Timeframe	Timeframe Considerations	Lead Department
N-3-1	Water conservation rebates. Conduct outreach to encourage participation in Santa Clara Valley Water District's (Valley Water) water conservation rebate programs, available to single-family homes, multi-family buildings, and businesses. Expand the City's rain barrel and landscape rebate programs.		\$ \$	Mid-term	Water conservation not immediate priority for reducing emissions; program already exists, can expand upon gradually.	Water Department
N-3-2	Fixture replacements. Expand replacement incentives, such as Santa Clara Valley Water District (Valley Water) rebate programs, of inefficient water fixtures and appliances in high-end sectors.		\$	Mid-term	Water conservation not immediate priority for reducing emissions; program already exists, can expand upon gradually.	Water Department
N-3-3	Water-efficient landscaping requirements. Expand requirements for water-efficient landscaping practices, including requirements for cooling (trees, green roofs) and drought-tolerant native plants.		\$ \$	Mid-term	Not immediate priority for reducing emissions but will help city become more resilient; could start with outreach then elevate to requirements.	Water Department / CDD - Planning
N-3-4	Community water portfolio diversification. Continue collaboration with agency partners such as South Bay Water Recycling, Valley Water, BAWSCA, and SFPUC to diversify water supply portfolio and expand current sources.		\$ \$	Ongoing	Collaborative partnerships already in place—can continue to build and expand over time.	Water Department



Action ID	Action Description	Impact	Cost	Implementation Timeframe	Timeframe Considerations	Lead Department
N-3-5	Recycled water connection requirements. Require the use of recycled water for all non-potable uses where recycled water is available, per City Code 13.15.160.	 	\$ \$	Mid-term	Not immediate priority for reducing emissions; currently limited availability to connect to system. Could wait to expand until infrastructure is ready.	Water Department
Strategy C1: Improve community resilience.						
C-1-1	Community resilience networks. Support neighborhood-based organizations and businesses in development of Neighborhood Resilience Hub Programs to prepare residents and respond to climate change.	 	\$ \$ \$	Long-term	Outcomes uncertain; led by outside organizations; action addresses longer term adaptation and resiliency needs; other actions take priority.	CMO - Sustainability
C-1-2	Support for people experiencing homelessness. Expand support services to people experiencing homelessness during all extreme weather and hazard events (e.g., extreme heat, flooding, wildfires).	 	\$ \$	Near-term	Climate impacts pose an immediate threat to these community members.	CDD - Housing
C-1-3	Community climate action grant. Establish an annual micro-grant program to support local citizen-led projects and programs that will reduce emissions, adapt to climate change, and enhance equity.	 	\$	Long-term	Ongoing initiative with no vital start date; other actions take priority.	CMO - Sustainability



Action ID	Action Description	Impact	Cost	Implementation Timeframe	Timeframe Considerations	Lead Department
C-1-4	Incentives for adaptation upgrades. Offer rebates and/or other financial incentives to encourage adaptation upgrades and installation of low-emissions space-cooling devices.	 	\$	Near-term	Incentives ease transition to future mandates; short-term emissions reduction opportunity.	CDD - Building
Strategy C2: Prepare for climate change.						
C-2-1	Climate resilient land use & development. Evaluate city land use maps to identify whether new development is being planned in high-risk areas, such as those projected to experience increased flood risk under climate project scenarios.	 	\$ \$	Near-term	Proactive risk mitigation less costly and more effective than reactive; important to enact immediately to reduce future infrastructure damage from climate impacts.	CDD - Building
C-2-2	On-site & natural stormwater systems. Integrate natural stormwater systems within site and building design to expand on-site stormwater management capacity.	 	\$ \$ \$	Mid-term	Longer term infrastructure investment—other actions take priority.	DPW
C-2-3	High-albedo parking lots. As part of conditions of approval, require new parking lots to be surfaced with more sustainable pavement materials to reduce heat gain, energy consumption, and stormwater runoff.	 	\$ \$	Mid-term	Incentives before mandates; action may face opposition and require stakeholder engagement get buy-in.	CDD - Planning



Action ID	Action Description	Impact	Cost	Implementation Timeframe	Timeframe Considerations	Lead Department
C-2-4	Climate Resilience Capital Improvement Program (CIP). Incorporate climate resiliency strategies and considerations in development of discretionary CIP projects, including new parks projects.		\$ \$	Near-term	Proactive risk mitigation less costly and more effective than reactive; important to enact immediately to reduce future infrastructure damage from climate impacts.	DPW - Engineering
C-2-6	Planned retreat strategies. Identify and consider relocation opportunities for critical facilities.		\$ \$	Long-term	Planning effort—will take time to implement; important to identify at-risk infrastructure immediately to plan for future relocation projects.	CMO- Sustainability



APPENDIX B: CEQA ADDENDUM

[See Santa Clara CAP Addendum]

