



City of Santa Clara

Meeting Agenda

Bicycle & Pedestrian Advisory Committee

Monday, March 23, 2026

4:00 PM

City Hall Council Chambers

1500 Warburton Avenue

Santa Clara, CA 95051

The City of Santa Clara is conducting the Bicycle and Pedestrian Committee meetings in a hybrid manner (in-person and continues to have methods for the public to participate remotely).

• Via Zoom:

<https://santaclaraca.zoom.us/j/83368467068>

Meeting ID: 833 6846 7068 or

Phone: 1 (669) 900-6833

1 CALL TO ORDER AND ROLL CALL

2 PUBLIC PRESENTATIONS

[This item is reserved for persons to address the body on any matter not on the agenda that is within the subject matter jurisdiction of the body. The law does not permit action on, or extended discussion of, any item not on the agenda except under special circumstances. The governing body, or staff, may briefly respond to statements made or questions posed, and appropriate body may request staff to report back at a subsequent meeting.]

3 CONSENT CALENDAR

- A. 26-179 [Bicycle and Pedestrian Advisory Committee Meeting Minutes of January 26, 2026 \(Megas\)](#)

Recommendation: Approve the Bicycle and Pedestrian Advisory Committee Meeting Minutes of January 26, 2026.

4 REPORTS FOR COMMITTEE INFORMATION

- A. 26-178 [Police Verbal Update \(Ernst\)](#)

5 ACTION ITEMS / GENERAL BUSINESS

- A. 26-184 [Creation of Data-Driven Approach to Evaluating Bicycle and Pedestrian Projects Subcommittee \(Megas\)](#)

Recommendation: Create Data-Driven Approach to Evaluating Bicycle and Pedestrian Projects subcommittee, if needed, and appoint members to serve on the subcommittee.

B. 26-185 [Fiscal Year 2026-27 BPAC Work Plan Approval \(Liw\)](#)

Recommendation: Review and approve staff's recommended Fiscal Year 2026-27 Work Plan.

C. 26-191 [Selection of Valley Transportation Authority Bicycle and Pedestrian Advisory Committee Representative \(Megas\)](#)

Recommendation: The BPAC to select a candidate for City Council approval to serve as the Santa Clara representative on the VTA BPAC from July 1, 2026 to June 30, 2028.

6 **REPORTS FOR COMMITTEE INFORMATION****A. 26-180** [Public Works Verbal Update \(Liw/Shariat\)](#)**B. 26-221** [Complete Streets Review for the Uncontrolled Crosswalk Improvements Phase II Project \(Shariat\)](#)**C. 26-192** [Closing Bike Lanes, Trails, and Sidewalks \(Garcia\)](#)**D. 26-194** [Trial Runs of New Bike and Pedestrian Infrastructure \(Shariat\)](#)**E. 26-181** [Valley Transportation Authority Bicycle and Pedestrian Advisory Committee Meetings Update \(Megas\)](#)**F. 26-182** [Santa Clara Station Area Task Force Update \(Shure\)](#)**G. 26-183** [Ad-hoc Subcommittee Reports \(Megas\)](#)**7** **ANNOUNCEMENTS AND MEMBER REPORTS****8** **ADJOURNMENT**

The next meeting will be on Monday, June 22, 2026

MEETING DISCLOSURES

The time limit within which to commence any lawsuit or legal challenge to any quasi-adjudicative decision made by the City is governed by Section 1094.6 of the Code of Civil Procedure, unless a shorter limitation period is specified by any other provision. Under Section 1094.6, any lawsuit or legal challenge to any quasi-adjudicative decision made by the City must be filed no later than the 90th day following the date on which such decision becomes final. Any lawsuit or legal challenge, which is not filed within that 90-day period, will be barred. If a person wishes to challenge the nature of the above section in court, they may be limited to raising only those issues they or someone else raised at the meeting described in this notice, or in written correspondence delivered to the City of Santa Clara, at or prior to the meeting. In addition, judicial challenge may be limited or barred where the interested party has not sought and exhausted all available administrative remedies.

If a member of the public submits a speaker card for any agenda items, their name will appear in the Minutes. If no speaker card is submitted, the Minutes will reflect "Public Speaker."

In accordance with the requirements of Title II of the Americans with Disabilities Act of 1990 ("ADA"), the City of Santa Clara will not discriminate against qualified individuals with disabilities on the basis of disability in its services, programs, or activities, and will ensure that all existing facilities will be made accessible to the maximum extent feasible. The City of Santa Clara will generally, upon request, provide appropriate aids and services leading to effective communication for qualified persons with disabilities including those with speech, hearing, or vision impairments so they can participate equally in the City's programs, services, and activities. The City of Santa Clara will make all reasonable modifications to policies and programs to ensure that people with disabilities have an equal opportunity to enjoy all of its programs, services, and activities.

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Individuals who require an auxiliary aid or service for effective communication, or any other disability-related modification of policies or procedures, or other accommodation, in order to participate in a program, service, or activity of the City of Santa Clara, should contact the City's ADA Coordinator at 408-615-3000 as soon as possible but no later than 48 hours before the scheduled event.



City of Santa Clara

1500 Warburton Avenue
Santa Clara, CA 95050
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Agenda Report

26-179

Agenda Date: 3/23/2026

REPORT TO BICYCLE AND PEDESTRIAN ADVISORY COMMITTEE

SUBJECT

Bicycle and Pedestrian Advisory Committee Meeting Minutes of January 26, 2026 (Megas)

RECOMMENDATION

Approve the Bicycle and Pedestrian Advisory Committee Meeting Minutes of January 26, 2026.

Written by: Ralph Garcia, Senior Civil Engineer, Public Works

Reviewed by: Carol Shariat, Acting Transportation Manager, Public Works

Approved by: Michael Liw, Assistant Director/City Engineer, Public Works

ATTACHMENT

1. BPAC Meeting Minutes, January 26, 2026



City of Santa Clara

Meeting Minutes

Bicycle & Pedestrian Advisory Committee

01/26/2026

4:00 PM

City Hall Council Chambers
1500 Warburton Avenue
Santa Clara, CA 95051

The City of Santa Clara is conducting the Bicycle and Pedestrian Committee meetings in a hybrid manner (in-person and continues to have methods for the public to participate remotely).

- Via Zoom:

<https://santaclaraca.zoom.us/j/83368467068>

Meeting ID: 833 6846 7068 or

Phone: 1 (669) 900-6833

1 CALL TO ORDER AND ROLL CALL

Chair Megas called the meeting to order at 4:12 PM

Present 7 - Member Hung Duong, Member Jorge Haro, Member Ken Kratz, Vice Chair Jonathan Marinaro, Chair Betsy Megas, Member Guillermo Jenaro, and Member Vernon Shure

2 PUBLIC PRESENTATIONS

Video [00:03:17]

Member of the Public Henry asked when the minutes from the previous October meeting would be available.

Public Works Assistant Director Liw stated the October minutes would be posted online after it is approved by the committee during this meeting.

Member of the Public Diane Harrison announced that Bike to Shop Day Santa Clara will be on May 30th, Bike to Wherever Day will be on May 14th, and there will be an Energizer Station at the Senior Center on May 15th. She stated these events are looking for volunteers and to contact BPAC Member Ken Kratz if anyone is interested. She asked about the grant report and how BPAC can make recommendations to Council.

3 CONSENT CALENDAR

- A. [25-1690](#) Bicycle and Pedestrian Advisory Committee Meeting Minutes of October 27, 2025 (Megas)

Recommendation: Approve the Bicycle and Pedestrian Advisory Committee Meeting Minutes of October 27, 2025.

Video [00:07:37]

Member Kratz made comments on the October minutes. A motion was made by Member Kratz, seconded by Vice Chair Marinaro, to Approve the Consent Calendar with the provision that staff will update the minutes regarding the discussion of bicycle use on sidewalks. The motion carried by the following votes.

Aye: 6 - Member Duong, Member Haro, Member Kratz, Vice Chair Marinaro, Chair Megas, and Member Shure

Abstained: 1 - Member Jenaro

4 REPORTS FOR COMMITTEE INFORMATION

- A. [25-1691](#) Police Verbal Update (Ernst)

Video [00:17:16]

Officer Ernst introduced himself as the new Police Department Representative and provided a verbal update on reported bicycle collisions.

Committee comments and questions followed.

Officer Ernst addressed Committee questions.

5 ACTION ITEMS / GENERAL BUSINESS

- A. [25-1696](#) Transportation Development Act/Transportation Fund for Clean Air
Funding Recommendations (Shariat)

Recommendation: 1. Approve Alternative 1 to apply for the FY 2026/27 TDA funding cycle to supplement funding for the Measure I Bicycle Wayfinding project; and
2. Approve staff recommendation to apply for the FY 2026/27 TFCA funding cycle for the purchase of bicycle racks and install them within the public right-of-way locations identified within the spot improvements graphic.

Video [00:31:31]

Acting Transportation Manager Shariat presented the staff recommendations for FY2026/27 funding allocation of the Transportation Development Act program for the Measure I Bicycle Wayfinding Project and funding allocation of the Transportation Fund For Clean Air program for bicycle racks.

Committee questions and comments followed regarding the bicycle wayfinding project.

Public Speaker: Diane Harrison

Assistant Public Works Director Liw and **Acting Transportation Manager Shariat** addressed **Committee** questions.

A motion was made by Member Jenaro and seconded by Vice Chair Marinaro, to approve the staff recommendation for allocating TDA funds for the bicycle wayfinding project. The motion is carried by the following votes:

Aye: 7 - Member Duong, Member Haro, Member Kratz, Vice Chair Marinaro, Chair Megas, Member Jenaro, and Member Shure

Video [01:11:07]

Committee questions and comments followed regarding the bicycle racks.

Public Speaker: Henry, Diane Harrison

Assistant Public Works Director Liw and **Acting Transportation Manager Shariat** addressed **Committee** questions.

A motion was made by Member Kratz and seconded by Vice Chair Marinaro, to approve the staff recommendation for allocating TFCA funds for bicycle rack project with the request for BPAC involvement in bicycle rack locations and staff review on how publicly funded bike racks could be implemented on private property on request of the property owner. The motion is carried by the following votes:

Aye: 7 - Member Duong, Member Haro, Member Kratz, Vice Chair Marinaro, Chair Megas, Member Jenaro, and Member Shure

- B.** [25-1697](#) Review of Work Plan Topics Proposed for Fiscal Year 2026/27 BPAC Work Plan (Garcia)

Recommendation: Approve staff recommendation regarding the development of the FY 2026/27 BPAC Work Plan.

Video [01:35:21]

Senior Civil Engineer Garcia presented the proposed work plan topics for FY 2026-2027 BPAC meetings.

Committee comments and questions followed.

Senior Civil Engineer Garcia addressed **Committee** questions.

Public Speaker: Diane Harrison

Member Jenaro was excused from the meeting at 6:15 PM.

A motion was made by Chair Megas, seconded by Member Shure, to combine the proposed work plan items per Committee discussion and advance the proposed work plan topics for ranked choice voting. The motion carried by the following votes:

Aye: 6 - Member Duong, Member Haro, Member Kratz, Vice Chair Marinaro, Chair Megas, and Member Shure

Excused: 1 - Member Jenaro

- C. [25-1692](#) Creation of Data-Driven Approach to Evaluating Bicycle and Pedestrian Projects Subcommittee (Megas)

Recommendation: Create Data-Driven Approach to Evaluating Bicycle and Pedestrian Projects subcommittee, if needed, and appoint members to serve on the subcommittee.

Video [03:09:53]

Senior Civil Engineer Garcia presented the item for BPAC to discuss the creation of a new subcommittee for data-driven approach to evaluating bicycle and pedestrian projects and appoint members to serve on the subcommittee.

Committee comments and questions followed.

A motion was made by Vice Chair Marinaro seconded by Chair Megas, to table the agenda item to the next BPAC meeting so Member Jenaro can provide his input on the proposed subcommittee. The motion carried by the following votes:

Aye: 6 - Member Duong, Member Haro, Member Kratz, Vice Chair Marinaro, Chair Megas, and Member Shure

Excused: 1 - Member Jenaro

6 REPORTS FOR COMMITTEE INFORMATION

A. [25-1694](#) Public Works Verbal Update (Liw/Shariat)

Video [03:17:07]

Assistant Public Works Director Liw and Acting Transportation Manager Shariat gave the following updates:

1. Transportation Manager position and staff vacancies
2. Super Bowl 60 traffic control and detour map information available by Bay Area Host Committee
3. Vision Zero going to Council for adoption on March 10th
4. Spreadsheet of grant program activity posted on BPAC webpage to streamline meeting agenda

Committee questions and comments followed.

Assistant Public Works Director Liw addressed **Committee** questions.

B. [25-1704](#) Improvements to Safe Routes to School Program (Shariat)

Video [03:29:39]

Acting Transportation Manager Shariat reported on the work topic about Safe Routes to School program improvements regarding education and engagement activities at participating schools and bicycle safety inspection and maintenance classes.

Committee questions and comments followed.

Public Speaker: Diane Harrison

Acting Transportation Manager Shariat addressed **Committee** questions.

C. [25-1705](#) Safety Tips on BPAC Website, Posters, and Bicycle Map (Garcia)

Video [03:48:20]

Senior Civil Engineer Garcia reported on the work topic about updating the BPAC webpage, providing bicycle and pedestrian safety tip poster, and providing safety information to the City's bicycle map.

Committee questions and comments followed.

Public Speaker: Henry, Diane Harrison

Senior Civil Engineer Garcia addressed Committee questions.

D. [25-1708](#) Valley Transportation Authority Bicycle and Pedestrian Advisory Committee Meetings Update (Megas)

Video [04:04:44]

Chair Megas reported on the Valley Transportation Authority Bicycle and Pedestrian Advisory Committee Meetings.

Committee questions and comments followed.

Chair Megas addressed **Committee** questions.

E. [25-1709](#) Santa Clara Station Area Task Force Update (Shure)

Video [04:13:53]

Member Shure reported on the update of the Santa Clara Station Area Task Force.

Committee questions and comments followed.

Member Shure addressed **Committee** questions.

F. [25-1700](#) Ad-hoc Subcommittee Reports (Megas)

Video [04:23:10]

Member Kratz and **Member Haro** reported on the Bicycle Wayfinding subcommittee.

Public Speaker: Diane Harrison

Committee questions and comments followed.

7 ANNOUNCEMENTS AND MEMBER REPORTS

Video [04:41:39]

Committee made several statements and announcements about upcoming events:

1. Requested staff to provide list of prioritized bicycle and pedestrian resources that BPAC members and the public should be familiar with.
2. Stevens Creek Steering Committee meeting on Wednesday 1/26 2:30pm at San Jose City Hall.
3. Online e-bike town hall with State Senator Josh Becker.
4. Draft Guadalupe River Trail Connection Plan comments are due January 30th.

8 ADJOURNMENT

The meeting was adjourned at 8:55 PM.

The next scheduled meeting will be on March 23, 2026 at 4:00 PM.

A motion was made by Vice Chair Marinaro, seconded by Member Kratz, to adjourn the meeting

Aye: 6 - Member Duong, Member Haro, Member Kratz, Vice Chair Marinaro, Chair Megas, and Member Shure

Excused: 1 - Member Jenaro

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Agenda Report

26-178

Agenda Date: 3/23/2026

REPORT TO BICYCLE AND PEDESTRIAN ADVISORY COMMITTEE

SUBJECT

Police Verbal Update (Ernst)

DISCUSSION

Police Department staff will provide a verbal update on bicycle and pedestrian related items.

Written by: Ralph Garcia, Senior Civil Engineer, Public Works

Reviewed by: Carol Shariat, Acting Transportation Manager, Public Works

Approved by: Michael Liw, Assistant Director/City Engineer, Public Works



Agenda Report

26-184

Agenda Date: 3/23/2026

REPORT TO BICYCLE AND PEDESTRIAN ADVISORY COMMITTEE

SUBJECT

Creation of Data-Driven Approach to Evaluating Bicycle and Pedestrian Projects Subcommittee (Megas)

BACKGROUND

In order to remain in compliance with the Brown Act and in alignment with other City commissions, BPAC subcommittees shall be considered ad-hoc committees.

An ad-hoc committee is a committee that is (1) advisory; and (2) composed solely of members of the legislative body that are less than a quorum. To ensure that the subcommittees supporting BPAC's work qualify as ad-hoc committees, the subcommittees should:

1. Be comprised solely of BPAC members;
2. Consist of less than a quorum;
3. Have a defined purpose and timeframe to accomplish that purpose; and
4. Be advisory.

The intent of subcommittees is to provide feedback on topics needing additional analysis and discussion. Once the work of the subcommittee is complete, the subcommittee is disbanded. Generally, subcommittees should be disbanded within 6-12 months.

DISCUSSION

On August 25, 2025, the BPAC discussed creating an ad-hoc subcommittee on a Data-Driven Approach to Evaluating Bicycle and Pedestrian Projects as part of a report on a work plan topic request from BPAC Member Jenaro (Attachment 1). A BPAC agenda item was scheduled for both the October 2025 and January 2026 BPAC meetings, however Member Jenaro was not in attendance during these meetings. BPAC deferred additional discussion of this item to the March 2026 BPAC meeting.

Staff recommends that the BPAC discuss the purpose of the new subcommittee, create the new subcommittee, if needed, and appoint members to serve on the subcommittee.

RECOMMENDATION

Create Data-Driven Approach to Evaluating Bicycle and Pedestrian Projects subcommittee, if needed, and appoint members to serve on the subcommittee.

Written by: Ralph Garcia, Senior Civil Engineer, Public Works

Reviewed by: Carol Shariat, Interim Transportation Manager, Public Works

Approved by: Michael Liw, Assistant Director/City Engineer, Public Works

ATTACHMENTS

1. RTC 25-802 Proposal for a Data-Driven Framework to Evaluate Bicycle & Pedestrian Projects in Santa Clara



Agenda Report

25-802

Agenda Date: 8/25/2025

REPORT TO BICYCLE AND PEDESTRIAN ADVISORY COMMITTEE

SUBJECT

Proposal for a Data-Driven Framework to Evaluate Bicycle & Pedestrian Projects in Santa Clara (Chan)

BACKGROUND

In July 2024, Bicycle and Pedestrian Advisory Committee (BPAC) Member Jenaro requested BPAC to discuss establishment of a data analysis working group that would measure the impact of BPAC on bike-friendly initiatives in Santa Clara (Attachment 1).

The City is committed to improving safety, mobility, and the use of active transportation. As the City develops its Vision Zero Plan, there is growing recognition of the need for a more expanded, data-driven approach that goes beyond traditional collision reports, which can underrepresent the full scope of safety concerns.

To address this, the BPAC member outlined a potential framework for evaluating bicycle and pedestrian projects using multiple data sources, including collision records, user counts, mobile GPS data, near-miss tracking, and other key performance indicators. This approach aligns with state and federal trends emphasizing proactive safety planning and outcome-based evaluation and could support measuring project effectiveness, guiding investment priorities, and strengthening future grant applications.

DISCUSSION

Current Approaches to Data-Driven Project Evaluation

To effectively evaluate bicycle and pedestrian projects, some jurisdictions have adopted practices that prioritize safety and data-informed planning. An expanded data strategy and evaluation framework could improve transparency, guide future investments, and help achieve active transportation and Vision Zero goals.

Key Data Collection Methods and Innovations

- **Collision Data Integration**
Combine police collision reports with hospital trauma data and community reports to better capture underreported pedestrian and cyclist injuries. Tools such as public reporting maps and Intelligent Transportation Systems (ITS) sensors can supplement these efforts by tracking near-misses and risky behaviors.
- **Bicycle & Pedestrian Counts**
Implement a mix of permanent automated counters and temporary manual counts to monitor usage trends and calculate collision rates. There is the potential to pilot emerging technologies

such as AI-enabled video counters prior to large-scale deployment.

- **Multi-Modal Data Collection**
Include all micromobility users-bicycles, e-scooters, e-bikes, skateboards-by combining automated and manual classification counts. Have shared mobility providers submit anonymized trip data to better understand travel patterns.
- **Mobile Phone and GPS Data**
Leverage anonymized smartphone data from providers like StreetLight to analyze walking and biking patterns citywide. Calibrate this data with on-the-ground counts to enhance accuracy and track post-project changes.
- **Crowdsourcing and Surveys**
Use online tools and community surveys (e.g., BikeMaps.org) to collect feedback on near-misses, perceived safety, and user behavior. These sources can help identify issues not captured in official records.
- **Automated Conflict Detection**
Apply video analytics at high-risk locations to detect and analyze near-misses using metrics such as time-to-collision. Partner with agencies or vendors to pilot this technology in a cost-effective way.
- **Close Call Reporting and Sensor-Based Tools**
Encourage the public to report near-misses through web-based maps or forms. Explore dynamic sensing pilots using smartphones or micromobility devices to detect risky conditions in real time.

Potential Framework for Project Evaluation

- **Baseline Data Collection**
Prior to implementation, collect key data such as multi-day bike/ped counts, five-year collision history, speed and volume data, and community surveys to establish a robust “before” baseline.
- **During Implementation**
Use mobile counters, cameras, or integrated GPS systems to monitor temporary or pilot installations in real time. Telematics data from micromobility devices can provide insights into user behavior and compliance.
- **Post-Implementation Monitoring**
Collect “after” data at regular intervals (potentially at 3, 6, and 12 months) to track changes in usage, speeds, collisions, and near-misses. Ensure consistent methodology for accurate comparisons. Use mobile data for broader system-wide insights.
- **All-Mode Impact Assessment**
Expand evaluations to include impacts on motor vehicles, transit, and micromobility. Analyze mode shift and traffic diversion using origin-destination data or travel demand models, in coordination with agencies such as VTA and MTC.
- **Community Feedback**
Conduct surveys, walking/biking audits, and public workshops post-implementation so that community feedback can assist with understanding project successes and areas for improvement that may not be evident in quantitative data alone.

Illustrative Case Studies and Best Practices

The following case studies and guidance illustrate the benefits of robust, data-informed evaluation

and planning:

- San Francisco used counts, GPS data, and surveys to link new bikeways to a 14 percent cycling increase, showing how mixed data sources can evaluate impact.
- Santa Barbara/Santa Cruz demonstrated that local before/after counts strengthen statewide tools like the Active Transportation Benefit-Cost Tool.
- Los Angeles collected trip data from e-scooter providers to address equity and improve infrastructure planning-highlighting the value of public-private data sharing.
- Vision Zero Cities (for example, NYC and DC) use collision data, near-miss reports, and community feedback to prioritize interventions, an approach aligned with Santa Clara's goals.
- NACTO's Guide recommends structured data programs and public dashboards to track trends and support transparent decision-making.

Institutionalizing the Framework

To support long-term success, the City can benefit from establishing an expanded data framework that promotes consistent collection, analysis, and use of transportation data across programs.

- **Standardized Data Protocols**
Develop consistent procedures for collecting before-and-after data, including collision, count, and survey metrics.
- **Invest in Counting Infrastructure**
Install permanent or mobile devices at key locations. Where feasible, integrate smart sensor technology into new infrastructure projects.
- **Leverage Partnerships and Data Tools**
Collaborate with universities, research institutions, and vendors to access advanced tools and reduce staff burden. Ensure all data collection complies with privacy standards.
- **Analyze and Report**
Publish clear, visual reports comparing pre- and post-project data. Highlight usage trends, safety outcomes, and community input. Share findings to inform future projects.
- **Use Data to Guide Future Investments**
Incorporate evaluation results into funding decisions, capital improvement planning, and grant applications. Prioritize high-impact projects where data shows the greatest need or opportunity.

Other Considerations

Establishing an expanded data-driven framework will require the City to supplement its current data collection efforts. While some projects already include pre and post construction evaluations, often tied to grant requirements, a consistent citywide system for tracking vehicular and active transportation data will require additional staffing and funding, subject to City Council approval.

To begin addressing this need, the City has secured a one-year subscription to StreetLight, funded through a Vision Zero grant. Using anonymized mobile data, StreetLight monitors vehicle, bicycle, and pedestrian activity, providing valuable insights into travel patterns and safety trends at a broad scale.

Data collection strategies can be phased in over time. Lower cost tools such as community surveys

and manual counts can be deployed incrementally, while more advanced technologies such as ITS sensors and integrated data platforms will require greater investment. Some costs may be offset through regional partnerships or external grants.

As data from multiple sources is gathered, consolidating it into a centralized, accessible platform will be essential for maintaining consistency, improving coordination, and promoting transparency. A unified system would allow City staff and decision makers to monitor performance, minimize duplication, and better align future initiatives across departments and partner agencies. Additional hardware and software may be required to create a centralized platform.

Long-term planning should anticipate future growth and latent demand, meaning residents who would walk, bike, or use micromobility options if safer, more connected infrastructure were available. Realizing this potential is important to supporting population growth, advancing climate action goals, and adapting to evolving transportation preferences. Together, these elements could form the foundation for an improved strategy, as reflected in the City's pedestrian, bicycle, and creek trail master plans, as well as various specific plans.

RECOMMENDATION

To advance a data-driven investment approach, BPAC could place on a future agenda consideration for the creation of a data analysis subcommittee to serve in an advisory role. Working with City staff, this group could identify cost-effective, scalable data collection strategies and develop a key performance indicator framework aligned with available resources and city policies. The subcommittee's recommendations could help guide a more informed, performance-based approach to future active transportation projects. It is important to note that current staffing levels and funding does not support implementation of this proposal beyond the current one-year subscription to Streetlight. Any additional staffing or funding to support this effort requires the approval of the City and City Council.

Written by: Steve Chan, Transportation Manager, Public Works

Approved by: Michael Liw, Assistant Director/City Engineer, Public Works

ATTACHMENT

1. Work Plan Topic Request Form - Member Jenaro



Bicycle and Pedestrian Advisory Committee Annual Work Plan Topic Request Form

Requestor Name:

Guillermo Jenaro Rabadan

Date:

07/26/2024

Proposed Topic Title:

BPAC projects data collection and analysis

Background and Discussion: Please provide justification regarding how this topic pertains to the Bicycle and Pedestrian Advisory Committee's roles and responsibilities:

Motion: Establishment of a Data Analysis Team for Project Impact Assessment

Objective: To set up a dedicated data analysis working group that will develop and implement an analytical framework for measuring the impact of BPAC projects on the bike-friendly initiatives in Santa Clara.

Background: The BPAC reviews several project proposals aimed at making Santa Clara a bike-friendly city. These projects include bike lanes, bike storage, and other related infrastructure improvements. However, decisions often require trade-offs regarding motor traffic impact, parking availability, investments, and prioritization. To make informed decisions and optimize resource allocation, it is essential to have a robust data-driven approach.

Cities renowned for their bike-friendly infrastructure, such as those in the Netherlands, have successfully implemented data-driven decision-making processes that highlights the efficiency and discipline in using data to drive urban transformation.

Proposal: This member proposes the establishment of a data analysis working group tasked with developing a comprehensive analytical framework. This framework will focus on measuring various impacts of BPAC projects, such as:

- 1 Increase in Bike Transit: i) Implementation of regular bike counts ii) Analysis of bike usage patterns over time.
- 2 Survey Integration: i) Conducting surveys to gather feedback from residents on bike infrastructure and its usability. ii) Assessing the satisfaction and needs of the biking community.
- 3 City Response to BPAC Initiatives: i) Monitoring city responses to BPAC recommendations and initiatives. ii) Evaluating the effectiveness of implemented projects.
- 4 Resource Utilization: i) Measuring the return on investment (ROI) for BPAC projects. ii) Analyzing the cost-effectiveness of different initiatives.



Agenda Report

26-185

Agenda Date: 3/23/2026

REPORT TO BICYCLE AND PEDESTRIAN ADVISORY COMMITTEE

SUBJECT

Fiscal Year 2026-27 BPAC Work Plan Approval (Liw)

BACKGROUND

Each fiscal year (FY), the Bicycle and Pedestrian Advisory Committee (BPAC) prepares an Annual Work Plan to summarize topics that will be discussed at BPAC meetings for the public. The work plan consists of staff topics (i.e. grant funding opportunities, project and program updates, and complete streets review) and BPAC requested topics. Staff develops the overall work plan by taking into consideration available staff resources and other priorities for the Department of Public Works (DPW).

DISCUSSION

At the March BPAC meeting, staff will present a proposed work plan for the following fiscal year. The work plan will consist of staff topics and BPAC requested topics (Attachment 1) prioritized based on the BPAC rank choice voting results (Attachment 2), available staff resources, other staff priorities, and the number of BPAC meetings in a fiscal year. Staff topics will include project updates and a Complete Streets review of projects such as the City's annual pavement maintenance project. Staff will then request BPAC to review and approve the proposed work plan (Attachment 3).

RECOMMENDATION

Review and approve staff's recommended Fiscal Year 2026-27 Work Plan.

Written by: Ralph Garcia, Senior Civil Engineer, Public Works

Reviewed by: Carol Shariat, Acting Transportation Manager, Public Works

Approved by: Michael Liw, Assistant Director/City Engineer, Public Works

ATTACHMENTS

1. FY 2026-27 BPAC Agenda Topic Requests
2. BPAC Rank Choice Voting of Work Plan Topics
3. Recommended BPAC Fiscal Year 2026-27 Work Plan



City of Santa Clara
The Center of What's Possible

DEPARTMENT OF PUBLIC WORKS - TRAFFIC ENGINEERING DIVISION
1500 Warburton Ave, Santa Clara, CA 95050
Tel: 408.615.3000 Fax: 408-983-0931 Email: engineering@santaclaraca.gov

Bicycle and Pedestrian Advisory Committee Annual Work Plan Topic Request Form

Requestor Name:

HUNG G. DUONG

Date:

12/10/25

Proposed Topic Title:

Sidewalk Improvement and Expansion

Background and Discussion: Please provide justification regarding how this topic pertains to the Bicycle and Pedestrian Advisory Committee's roles and responsibilities:

There are many areas in the City of Santa Clara that lack adequate and safe sidewalk for pedestrians to use, forcing people to rely on cars and other modes of transportations.

There are areas where sidewalk disappear suddenly.

There are areas where there are no sidewalk.

There area areas where sidewalk are inaccessible due to hazardous environments, such as overgrown trees and residential/commercial activities.

We should aim for more sidewalk improvement and expansion, to encourage pedestrians to utilize our roads more.

We need more trees and shades.

We need leveled and paved sidewalk.

We need well-lit sidewalk for pedestrians to use at night.

We need to enforce fines and citations where cars parking too close to the curb/intersection and blocking views of sidewalk, creating unnecessary risks for pedestrians while crossing/passing.



Bicycle and Pedestrian Advisory Committee Annual Work Plan Topic Request Form

Requestor Name: Date:

Proposed Topic Title:

Background and Discussion: Please provide justification regarding how this topic pertains to the Bicycle and Pedestrian Advisory Committee's roles and responsibilities:

Millikin Basics+ Elementary School operates as a program-of-choice (open-enrollment) school, drawing students from across Santa Clara. Under district policy, students attending via open enrollment are not eligible for school bus service, resulting in heavy reliance on private vehicle drop-off and pick-up, as well as walking and biking to a lesser extent. This model creates predictable and recurring traffic congestion during school peak hours, including unsafe vehicle maneuvers, curbside conflicts, and frequent interactions between cars, pedestrians, and cyclists. Parents and caregivers report that walking and biking to school feel unsafe, particularly for young students during arrival and dismissal.

Although walking and biking routes are possible, there is no evidence of a recent, site-specific traffic engineering safety evaluation addressing current traffic volumes, circulation patterns, and pedestrian/bicycle conflicts. Education and encouragement programs alone are insufficient to mitigate infrastructure-related safety risks.

Given the school's enrollment structure, lack of bus service, and sustained peak-hour traffic, these conditions are structural and ongoing, warranting a formal engineering review. I personally, being a passionate cyclist, had to stop biking to school as it felt very unsafe. I therefore request initiation of a Traffic Engineering–led bicycle and pedestrian safety study for the Millikin Basics+ Elementary School area, including:

- Evaluation of circulation, access, crossings, and conflict zones
- Data collection during school peak hours (traffic, speeds, pedestrian/bike activity)
- Identification of short-, medium-, and long-term safety improvements
- Coordination with the school and Safe Routes to School efforts



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Bicycle and Pedestrian Advisory Committee Annual Work Plan Topic Request Form

Requestor Name:

Vern Shure

Date:

12/6/2025

Proposed Topic Title:

Policy on Bike Riding on Sidewalks

Background and Discussion: Please provide justification regarding how this topic pertains to the Bicycle and Pedestrian Advisory Committee's roles and responsibilities:

We have had community comments during 2025 BPAC meetings both for and against allowing bike riding on sidewalks in Santa Clara. One parent said that we should raise the age limit for bike riding on sidewalks. A senior citizen stated that she was concerned for her safety with people riding on sidewalks. The BPAC should examine the current policy and consider whether it needs to be adjusted.



Bicycle and Pedestrian Advisory Committee Annual Work Plan Topic Request Form

Requestor Name:

Ken Kratz

Date:

12/12/2025

Proposed Topic Title:

right-of-way on city creek trails

Background and Discussion: Please provide justification regarding how this topic pertains to the Bicycle and Pedestrian Advisory Committee's roles and responsibilities:

Propose trail rules establishing right-of-way for city creek trails and post those rules at trail heads. Rules to include reducing cyclist's speed to walking speed (1-2 miles per hour) when overtaking pedestrians.
Propose similar rules for other uses including skateboards, scooters, electric bicycles, electric scooters and other vehicles overtaking pedestrians. Suggested right-of way order to be: pedestrians, skateboards, push scooters, traditional and in-line roller skates, bicycles, trikes, electric powered skateboards and scooters, electric powered bicycles.
Helps reduce impact of collisions between trail users.



Bicycle and Pedestrian Advisory Committee Annual Work Plan Topic Request Form

Requestor Name:

Ken Kratz

Date:

12/12/2025

Proposed Topic Title:

Establish right-of-way rules for city sidewalks

Background and Discussion: Please provide justification regarding how this topic pertains to the Bicycle and Pedestrian Advisory Committee's roles and responsibilities:

Change existing City sidewalk ordinance to establish right-of-way rules. Change current prohibition on riding a bicycle on city sidewalks to allow bicycles to use on city sidewalks in residential sections of the City with restrictions. Restrictions to include reducing bicyclist's speed to walking speed (1-2 miles per hour) when near a pedestrian or dismounting from bicycle and walking when passing a pedestrian.

Propose similar restrictions for skateboards, scooters, electric bicycles, electric scooters and other vehicles that are normally faster than pedestrians. Suggested right-of way order to be: pedestrians, skateboards, push scooters, traditional and in-line roller skates, bicycles, trikes, electric powered skateboards and scooters.

Bicyclist must leave sidewalk to allow wider pedestrian oriented equipment to pass (wheelchairs strollers)

Trikes and other human powered vehicles that are wider than a bicycle must to pull off sidewalk and come to a complete stop to allow 42" of clearance on the adjacent sidewalk to allow pedestrians to pass (needed for wheelchair users in particular).

Helps to promote cycling when there is not sufficient lane width to allow for safe cycling. Helps to provide for the safety of all those who use city sidewalks.



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Bicycle and Pedestrian Advisory Committee Annual Work Plan Topic Request Form

Requestor Name:

Vern Shure

Date:

12/6/2025

Proposed Topic Title:

BPAC Processes and Governance

Background and Discussion: Please provide justification regarding how this topic pertains to the Bicycle and Pedestrian Advisory Committee's roles and responsibilities:

The BPAC needs to consider establishing rules that will allow it to operate more efficiently. The meetings are excessively long. We would be more productive with more frequent, shorter meetings. Some have stated it's a budget issue, but there must be some workaround to enable greater productivity. We might consider allocating only a certain amount of time per agenda item, or limit the number of comments and time on each topic per member. This is open ended, other members may have ideas of their own.



Bicycle and Pedestrian Advisory Committee Annual Work Plan Topic Request Form

Requestor Name:

Vern Shure

Date:

12/6/2025

Proposed Topic Title:

Parking Enforcement

Background and Discussion: Please provide justification regarding how this topic pertains to the Bicycle and Pedestrian Advisory Committee's roles and responsibilities:

Cars in certain parts of Santa Clara routinely park in bike lanes and red zones. The area around Scott and Warburton is bad for red zone parking. Octavius Drive, Augustine Drive, and Lakeside Drive are bad for parking in bike lanes. There are probably many other areas where this is an issue which BPAC members are personally familiar with. The BPAC should look into options for addressing this behavior. It would obviously require police involvement for enforcement. Would better signage help?



Bicycle and Pedestrian Advisory Committee Annual Work Plan Topic Request Form

Requestor Name:

Vern Shure

Date:

12/6/2025

Proposed Topic Title:

E-bike Safety on San Tomas Aquino Trail

Background and Discussion: Please provide justification regarding how this topic pertains to the Bicycle and Pedestrian Advisory Committee's roles and responsibilities:

E-Bikes and other motorized vehicles grossly exceed the posted speed limit of 15 mph on the San Tomas Aquino Trail in Santa Clara. They often appear to be traveling at over 2X the posted speed limit, and have greater volume and mass than ordinary bikes. We are subject to the state of CA e-bike law which says class three e-bikes, which can go up to 28 mph, are banned from bike trails and bike lanes. Some e-bikes may be modified to exceed the ordinary limits. The vast majority of e-bikes on this trail are going faster than the 20 mph associated with class one and two e-bikes, which means they should not even be on this trail. Likewise, if they are class three e-bikes they should not even be in bike lanes. The BPAC should take on this issue and see what we can do to address it. E-bikes are not going away, but the laws should be enforced.



Bicycle and Pedestrian Advisory Committee Annual Work Plan Topic Request Form

Requestor Name:

Ken Kraqtz

Date:

12/12/2025

Proposed Topic Title:

Upgrade zoning maps to include more mixed use & bus service

Background and Discussion: Please provide justification regarding how this topic pertains to the Bicycle and Pedestrian Advisory Committee's roles and responsibilities:

Request City Planning Department and City Council to update zoning maps to include more mixed use (housing over retail and commercial spaces) in order to increase affordable housing and to allow residents options to walk or bicycle to shopping and other commercial services rather than using motor vehicles on those errands. Change zoning to allow compatible businesses and industries to locate in residential areas. The strict zoning separates these functions too much.

Reduce parking facilities for residents' motor vehicles at those developments below current requirements. Provide more bus service from those mixed use developments to present industries and commercial areas in the north side of the city.

These items will increase transit use as well as walking and bicycling to those destination rather than driving. This will also reduce the amount of parking lots in the city, almost all heat absorbing black top (asphaltic concrete), that will in turn reduce the temperatures of the city thereby increasing interest in cycling and walking.

Will reduce number of automobiles on the road and reduce the number of miles motor vehicles are on the road thereby increasing pedestrian and bicyclist's health and safety (fewer pollutants and fewer crashes). This is also a goal of Vision zero.



Bicycle and Pedestrian Advisory Committee Annual Work Plan Topic Request Form

Requestor Name:

Ken Kratz

Date:

12/12/2025

Proposed Topic Title:

City Planning for pedestrian and cyclists

Background and Discussion: Please provide justification regarding how this topic pertains to the Bicycle and Pedestrian Advisory Committee's roles and responsibilities:

Recommend City Council to adopt new city planning requirements to foster a better environment for pedestrians and cyclists. Here are some ideas to implement:

1. Mandate height restriction on new buildings, three stories or less, on the north side of the city, north of 101 freeway. This restriction will allow summer breezes to flow inland to the rest of the city. Create computer model to test installation of buildings to determine impact of tall buildings to wind patterns that effectively cool the balance of the city in the summer.
2. Provide transit stops within 5 to 10 minute walk of housing and jobs throughout the city.
3. Reduce or eliminate on-site parking. Reduce on-site parking requirements for development that are near transit lines particularly near rail lines. Allow higher densities at such sites. To compensate, provide more bus stops or relocate bus stops to in front of businesses.
4. Provide maximum 60' wide street right-of-way to allow 12' sidewalks minimum in new developments. Increased sidewalk width will allow outdoor dining.
5. Create policies to allow more street cart vendors.
6. Allow more live-work units within the city.
7. Allow housing on commercial and industrial if compatible.
8. Neighborhoods to be better linked to regional transit systems.
9. Hire an urban planner to specifically look at neighborhood (start at District level with goal to look at the smaller scale neighborhoods) amenities and deficiencies in order to reduce motor vehicle trips, to determine compatibility of public and private enterprises with neighborhood. Urban planner should also have regional knowledge for opportunities to combine mobility and destination assets of importance to residents.
10. Developers to invest in mass transit as a condition for approval.



Bicycle and Pedestrian Advisory Committee Annual Work Plan Topic Request Form

Requestor Name:

Ken Kratz

Date:

12/12/2025

Proposed Topic Title:

Improve environment for pedestrians and cyclists

Background and Discussion: Please provide justification regarding how this topic pertains to the Bicycle and Pedestrian Advisory Committee's roles and responsibilities:

To improve the environment for cyclists and pedestrians, I suggest:

1. Monitor and mitigate excessive noise levels on some city streets.
2. Study levels of small particulate matter on city streets to identify areas for improvement and/or avoidance by cyclists and pedestrians. If possible, mitigate (e.g., sound walls, less abrasive pavements).
3. Increase tree canopy throughout the city.
4. Install white colored pavements on wide multi-lane roads, primarily those roads with little tree cover. Bike lanes to be exempt due to heating effect from reflective surface of white colored pavements.



Bicycle and Pedestrian Advisory Committee Annual Work Plan Topic Request Form

Requestor Name:

Ken Kratz

Date:

12/12/2025

Proposed Topic Title:

Tool lending facility and repair classes

Background and Discussion: Please provide justification regarding how this topic pertains to the Bicycle and Pedestrian Advisory Committee's roles and responsibilities:

Start a tool lending at the city library or, in a separate building to be administered by the city, similar to the one in Berkeley Ca. where they charge a nominal fee to use bicycle, gardening, home improvement, and auto repair tools.

This facility will allow patrons low cost access to specialized tools that bicyclists need only occasionally. Facility could also include a bicycle parts program.

The current program at the library mainly includes electrical tools for home energy auditing. The facility I propose will help residents who live in smaller living spaces access to tools that are only need occasionally, circumventing the need to purchase and store the tool.

Provide at low or no cost classes on bicycle repair and maintenance along with bicycle safety checks at the community recreation center and other city facilities.

Maintains bicycles thereby improving safety of cyclists.



Bicycle and Pedestrian Advisory Committee Annual Work Plan Topic Request Form

Requestor Name:

Ken Kratz

Date:

12/12/2025

Proposed Topic Title:

Traffic engineering solutions to common problems

Background and Discussion: Please provide justification regarding how this topic pertains to the Bicycle and Pedestrian Advisory Committee's roles and responsibilities:

Here are some traffic engineering solutions to common problems experienced by pedestrians and cyclists throughout the city:

1. Provided mid-block pedestrian crossings throughout the city to increase safety of pedestrians.
2. Provide an alternative route on the STACT that is shorter to bypass the restricted zone around Levis stadium. I suggest an elevated pathway to be constructed to the east of the present trail, connecting to the trail south of the stadium and running through the existing Great America Park and over Tasman Drive and reconnects with the trail north of Tasman.

There could be an elevated spur trail that enters the convention center at the second floor level; there is already a foundation under the floor of the convention center to allow this connection (through the glazing; a connection (stairway and elevator) could also be constructed to the light rail station at the convention center to allow attendees to access the center.
3. Require CSC Engineering Standard Standard Detail ST-27, "Steel Plate Benching" to include all streets not just streets 35 mph or above. This will help cyclists. Also require asphaltic concrete ramps at slope of 1:12 at plates located over City sidewalks; this will help pedestrians from tripping and will allow wheel chairs to comfortably pass over the plate.
4. Provide K-rail barriers or other barrier on sidewalks to protect pedestrians where parked motor vehicles are removed in order to provide a bike lane.
5. Provide bike lanes on all streets that have lane lines (those streets with three or more motor vehicle lanes).
6. . Open the San Tomas and Lawrence Expressways for pedestrians; make improvements where needed.
7. Install more speed humps.



Bicycle and Pedestrian Advisory Committee Annual Work Plan Topic Request Form

Requestor Name:

Diane Harrison

Date:

January 20, 2021

Proposed Topic Title:

Upgrade this form

Background and Discussion: Please provide justification regarding how this topic pertains to the Bicycle and Pedestrian Advisory Committee's roles and responsibilities:

There are 2 current problems. Topics discussed are often falling into oblivion after discussion. And staff is putting way too much work into these topics. This new form (see Word file) solves both. It will be now up to the member to present each topic with greater details and a proposed action, if applicable. Staff may still add comments, put the item into Granicus form if desired, and present the item if the member is absent.

Also, I am currently seeing topics coming back over & over. This is not due to forgetfulness, but to the fact that a problem continues to exist with no progress toward a solution. Many of our discussion topics are, at their core, an appeal for action, from the city, from Council, from staff. Forming a subcommittee keeps the issue alive, but if the subcommittee work falls into oblivion, like wayfinding, that's even more discouraging than discussion items doing so.



Bicycle and Pedestrian Advisory Committee Annual Work Plan Topic Request Form

Requestor Name:

Ken Kratz

Date:

12/12/2025

Proposed Topic Title:

revise Annual Work Plan Topic Request Form

Background and Discussion: Please provide justification regarding how this topic pertains to the Bicycle and Pedestrian Advisory Committee's roles and responsibilities:

There are 2 current problems. Topics discussed are often falling into oblivion after discussion. And staff is putting way too much work into these topics. This new form (see Word file) solves both. It will be now up to the member to present each topic with greater details and a proposed action, if applicable. Staff may still add comments, put the item into Granicus form if desired, and present the item if the member is absent.

Also, I am currently seeing topics coming back over & over. This is not due to forgetfulness, but to the fact that a problem continues to exist with no progress toward a solution. Many of our discussion topics are, at their core, an appeal for action, from the city, from Council, from staff. Forming a subcommittee keeps the issue alive, but if the subcommittee work falls into oblivion that's even more discouraging than discussion items doing so.

Here is my suggested format:

CITY OF SANTA CLARA
Department of Public Works

Bicycle and Pedestrian Advisory Committee (BPAC)
Annual Work Plan Topic Request Form

Requester Name: _____ Date: _____

Proposed Work Plan Topic: _____

Action Item (Y/N): ___ If Yes, what is desired action (word as possible motion):

Background and Discussion: Please provide your presentation, including background information and a description of the proposed work plan topic. (2 pages maximum):

City staff stuff



Bicycle and Pedestrian Advisory Committee Annual Work Plan Topic Request Form

Requestor Name:

Ken Kratz

Date:

12/12/2025

Proposed Topic Title:

update BPAC on new laws and efforts to reduce speed limits & advocate for local control to lower speed limits.

Background and Discussion: Please provide justification regarding how this topic pertains to the Bicycle and Pedestrian Advisory Committee's roles and responsibilities:

Motor vehicle speeds on many city streets are in excess of posted speed limits and therefore pose a greater safety risk for pedestrians and cyclists (e.g., Benton Street between San Tomas and Lawrence Expressways).

To begin solving this problem, the City Traffic Engineering staff shall periodically update BPAC on current staff efforts to reduce speed limits on those problematic streets. Update shall include past traffic studies, current studies and proposed studies on those streets and how certain streets are selected by the Traffic Engineering staff for review.

Update to include current and proposed State legislation regulating speed limits and how those regulations can be changed, especially to reflect the differences in needs between rural roads and suburban and urban streets.

Speed limit regulations were formalized in the 1960s to regulate traffic speeds particularly on rural roads according to the author of this YouTube video, "Not Just Bikes, The Wrong Way to Set Speed Limits" (link: <https://www.google.com/search?client=firefox-b-1-d&q=Not%20Just%20bikes%20speed%20limits%20site%20youtube.com&ved=2ahUKEwiyjequy8aHAXU7IkQIHSjmN48Q2wF6BAgcEAE&ei=HpOkZqPCGLvEkPIPqMzf-Qg#fpstate=ive&vld=cid:8ba32321,vid:bgIWCuCMSWc,st:0>)

Can the City set its own speed limits on our city streets, particularly around schools, businesses, commercial properties, parks, public activity centers and other public spaces where cyclists and pedestrians frequent. Are there any repercussions for the City if it sets its own speed limits? If so what are they?

Increases safety for cyclists and pedestrians.



Bicycle and Pedestrian Advisory Committee Annual Work Plan Topic Request Form

Requestor Name:

Ken Kratz

Date:

12/12/2025

Proposed Topic Title:

BPAC to lead the review and the creation of new city standards for new development.

Background and Discussion: Please provide justification regarding how this topic pertains to the Bicycle and Pedestrian Advisory Committee's roles and responsibilities:

As the city continues to grow and is approaching urban densities, I propose the BPAC lead the review and the creation of new city standards for new development that will create a vibrant and safe city, inclusive of pedestrians and cyclists as well as public and private motor vehicle traffic. Those standards I suggest be based on the criteria promulgated by Jane Jacobs in her book *Death and Life of Great American Cities*. The author of that book suggests four criteria that should be present to provide a safe, vibrant and financially solvent city, the descriptions of these criteria are found on the YouTube channel "OptimiCities" (link: <https://www.youtube.com/@optimcities>) or search "Optimicities Jane Jacobs." Here are the four criteria:

- 1) Mixed use development.
- 2) Short blocks.
- 3) Aged buildings.
- 4) Concentration.

Here is a short YouTube video the OptimCities channel on this subject entitled "High Density, Doesn't Mean Overcrowding":

[https://www.youtube.com/watch?](https://www.youtube.com/watch?v=U6_we9SkgC0&list=PLpcQZ54yN0XJtr1uE3VskC5QDpTgtYqtB&index=4)

[v=U6_we9SkgC0&list=PLpcQZ54yN0XJtr1uE3VskC5QDpTgtYqtB&index=4](https://www.youtube.com/watch?v=U6_we9SkgC0&list=PLpcQZ54yN0XJtr1uE3VskC5QDpTgtYqtB&index=4)



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Bicycle and Pedestrian Advisory Committee Annual Work Plan Topic Request Form

Requestor Name:

Ken Kratz

Date:

12/12/2025

Proposed Topic Title:

Proposal for raised sidewalk installations to cross city streets

Background and Discussion: Please provide justification regarding how this topic pertains to the Bicycle and Pedestrian Advisory Committee's roles and responsibilities:

Install raised sidewalk to cross city streets at intersections, with or without crosswalks, and at mid block locations, with our without crosswalks. Raised sidewalks help those in wheelchairs in particular since they will not have to go up and down curb ramps nor have to cross storm water runoff in the gutter.

City staff, BPAC, Senior Advisory Commission, Parks and Recreation Department, and Santa Clara Unified School District to help recommend locations to the City Council.

Increases pedestrian safety, particularly those in wheelchairs, as well as the safety of other street users because raised sidewalks that cross a city street provide traffic calming.



Bicycle and Pedestrian Advisory Committee Annual Work Plan Topic Request Form

Requestor Name:

Ken Kratz

Date:

12/12/2025

Proposed Topic Title:

Bicycle Safety and Bike Citation Diversion Classes

Background and Discussion: Please provide justification regarding how this topic pertains to the Bicycle and Pedestrian Advisory Committee's roles and responsibilities:

City to mandate compulsory attendance at a Bicycle Safety and Bike Citation Diversion Class offered by the County or Stanford University for all BPAC members, City traffic engineers, and police department personnel who issue moving violations. The County's Public Health Department website and Stanford University Department of Public Safety websites have information on this type of program:

(<https://publichealth.sccgov.org/health-information/active-and-safe-transportation/juvenile-traffic-diversion-program> &

<https://transportation.stanford.edu/bicycle/attend-event/bike-safety-diversion-class>)

From the Stanford University Department of Public Safety:

Bike Safety Classes are offered as part of the Bike Diversion Program. These classes are part of a program initiative in partnership with Santa Clara County Superior Court, Traffic Division, to educate cyclists about fundamental bicycle safety and the Rules of the Road.

From the The County's Public Health Department's website: The Juvenile Traffic Diversion program is similar to adult traffic school. It offers a two-hour safety education class for youth who are cited for bicycle, pedestrian, and other non-motor vehicle violations.

Youth under age 18 may attend class along with their parent/guardian in lieu of paying the fines and fees related to the citation. The program is a joint effort between the Public Health Department, Stanford Health Care, and local law enforcement agencies and is unique to Santa Clara County.

Helps improve cyclists.



Bicycle and Pedestrian Advisory Committee Annual Work Plan Topic Request Form

Requestor Name:

Ken Kratz

Date:

12/12/2025

Proposed Topic Title:

Proposed zoning and development requirements to enhance walking and cycling

Background and Discussion: Please provide justification regarding how this topic pertains to the Bicycle and Pedestrian Advisory Committee's roles and responsibilities:

BPAC to work with the City's Planners to propose new zoning and development requirements in order to reduce private automobile use within the city and to increase use of alternatives.

Alternatives to single occupant private automobile use to include, but not be limited to, encouraging public transit use, bicycling, walking and carpooling and other ride-share programs.

For instance, in order to achieve those goals, City Planners and the BPAC can propose reducing on-site parking (current parking regulations require a minimum of parking spaces be provided) by providing developer incentives for new construction. Incentives to reduce parking on-site may include increasing the allowable floor area ratio (FAR) that usually results in an increase in building heights or building footprint.

Reduces motor vehicle trip demand on existing city streets thereby making a safer and more enjoyable environment for cyclists and pedestrians.



Bicycle and Pedestrian Advisory Committee Annual Work Plan Topic Request Form

Requestor Name:

Ken Kratz

Date:

12/12/2025

Proposed Topic Title:

Plant street trees and provide city tree maintenance

Background and Discussion: Please provide justification regarding how this topic pertains to the Bicycle and Pedestrian Advisory Committee's roles and responsibilities:

Install street trees throughout the City where needed and the City to takeover the maintenance of all streets trees within the street right-of-way that are currently privately maintained.

City Arborist to review and make recommendations on this proposal for review by the City Council..

Increases the city tree canopy and improves tree inspection and care that, in turn, helps to promote cycling though reducing air temperatures throughout the city (reducing the heat island effect) and improving air quality (filters particulates from the air).



Bicycle and Pedestrian Advisory Committee Annual Work Plan Topic Request Form

Requestor Name:

Ken Kratz

Date:

12/12/2025

Proposed Topic Title:

Install tree wells in city streets

Background and Discussion: Please provide justification regarding how this topic pertains to the Bicycle and Pedestrian Advisory Committee's roles and responsibilities:

Install tree wells in existing streets, particularly those streets that are very wide where existing street trees are too small to provide shade in the street, or streets that have no trees and property owners do not want a tree in front of their property.

Tree wells to be designed to not obstruct storm water runoff. It may be possible to use some of the storm water for irrigating the trees thereby reducing runoff and need to increase storm water infrastructure (e.g., additional or larger storm water pipes)

Trees shall be selected that provide shade in summer, filtration of air particulates, and reduce velocity of winter winds.

Trees to be selected that provide dense shade, filtering of particulates in the air, have minimal leaf litter, and do not have invasive roots. Trees to be selected in consultation with the City Arborist.

Helps to encourage cycling by moderating temperatures of the outdoor environment as well as reducing particulates in the air. The tree wells may also reduce motor vehicle speeds on our streets making it safer for cyclists and pedestrians t also is according to a study performed by John Hopkins University, School of Public Health.



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Bicycle and Pedestrian Advisory Committee Annual Work Plan Topic Request Form

Requestor Name:

Ken Kratz

Date:

12/12/2025

Proposed Topic Title:

City to require trees on private property

Background and Discussion: Please provide justification regarding how this topic pertains to the Bicycle and Pedestrian Advisory Committee's roles and responsibilities:

Create and/or strengthen City standards and ordinances for minimum number of trees to be planted on private property based on factors such as lot size and outdoor activities. This proposal is for new development and retroactive for existing development. Inspection and enforcement must be an aspect of this proposal.

Helps to promote cycling and pedestrian activities in the city by reducing air temperatures throughout the city (reducing the heat island effect in the city) as well as improving air quality (filtration of particulates from the air).



Bicycle and Pedestrian Advisory Committee Annual Work Plan Topic Request Form

Requestor Name:

Ken Kratz

Date:

12/12/2025

Proposed Topic Title:

Install parking meters and adopt a parking lot tax

Background and Discussion: Please provide justification regarding how this topic pertains to the Bicycle and Pedestrian Advisory Committee's roles and responsibilities:

Install motor vehicle parking meters and/or parking lot tax in existing and proposed commercial and business areas as well as existing and proposed regional transit centers to increase revenue for street improvements for pedestrians and cyclists.

Consider installing parking meters in existing and proposed business and commercial areas and, at the same time, applying a parking lot tax on existing and proposed businesses and commercial properties that have on-site parking, based on area of lot devoted to motor vehicle parking.

This may be an equitable way to increase City revenue between these two types of businesses, those that that rely on on-street parking and those businesses that have on-site parking, to provide much needed cycling and pedestrian improvements.

Parking meter revenue and parking lot tax to be used primarily to improve access to commercial and business properties. Some improvements may include sidewalk installations along the frontage of the property where they do not exist and sidewalk installations to connect with on-site sidewalks, particularly those connections that occur at the corner of corner lots (e.g., the recently completed sidewalk connection to the strip mall located at the northeast corner of Lawrence Expressway and Homestead Road).

Helps equitably fund cyclist and pedestrian improvements in the city.



Bicycle and Pedestrian Advisory Committee Annual Work Plan Topic Request Form

Requestor Name:

Ken Kratz

Date:

12/12/2026

Proposed Topic Title:

Development review process: review & implement Impact Fees for vehicle trip generation

Background and Discussion: Please provide justification regarding how this topic pertains to the Bicycle and Pedestrian Advisory Committee's roles and responsibilities:

City staff (Traffic Engineering and Planning Department staff) to report to BPAC the current impact fees and infrastructure requirements for developers in regard to trip generation and needed bicycle and pedestrian improvements. BPAC to review reports and approve fees and improvements before development project is approved by the City.

It is intended this proposal would place the BPAC in the formal development review process conducted by the Planning Department and Engineering Departments.

Helps to provide sufficient cycling and pedestrian infrastructure for those activities.



Bicycle and Pedestrian Advisory Committee Annual Work Plan Topic Request Form

Requestor Name:

Ken kratz

Date:

12/12/2025

Proposed Topic Title:

Proposal to standardize city sidewalk widths, building setbacks and building materials

Background and Discussion: Please provide justification regarding how this topic pertains to the Bicycle and Pedestrian Advisory Committee's roles and responsibilities:

As the City is beginning to urbanize through its recent approval of plans for denser development, I propose that BPAC to help create new standards, with the Planning Department personnel as desired, building facade material standards as well as minimum sidewalk width guideline for use in the public right-of-way based on adjacent land use (density of land use and height of buildings).

BPAC recommendations to be reviewed by appropriate city commissions prior to being submitted to the City Council for approval.

In proposed commercial, business, mixed use, and multi-floor housing areas, sidewalks to be a minimum of 1ten feet (10') wide. For every floor added to a building above the second floor, add an additional foot to the width of the sidewalk up to thirty feet wide sidewalk. Trees to be provided within the sidewalk width.

If housing is on the ground floor of a any multi-story building, the city sidewalk to be a minimum fifteen feet (15') wide. Trees to be provided within the sidewalk width.

Those widths I suggest are based to some extent on the widths suggested by Jane Jacobs in her book Death and Life of Great American Cities.

Building setbacks, either at the base of the building or the set back of upper floors, shall allow at least two (2) hours of direct sunlight at city right-of-way during the year, inclusive of public sidewalks.

Building materials shall not include widow wall construction consisting of reflective glass on the building's facade that can reflect solar radiation onto pedestrians and cyclists using public spaces (streets, parks, public squares, etc.).

Solar heat gain in buildings to be mitigated by facade design features such as sunshades (Brise soleil: https://en.wikipedia.org/wiki/Brise_soleil) or other means approved by the City. This will significantly reduce solar radiation reflected from buildings onto pedestrians and cyclists who are using city streets and facilities (the unprotected public). that reflected solar radiation makes those street activities uncomfortable to dangerous due to the possibility of heat exhaustion. This standard will prevent property and building owners from externalizing the solar heat gain problem onto the public.

The standards may be applied to private property that is open to the public.

Enhances walking in the city.



Bicycle and Pedestrian Advisory Committee Annual Work Plan Topic Request Form

Requestor Name:

Ken Kratz

Date:

12/12/2025

Proposed Topic Title:

Report on emergency response times

Background and Discussion: Please provide justification regarding how this topic pertains to the Bicycle and Pedestrian Advisory Committee's roles and responsibilities:

City staff to provide report comparing impact of cycling and pedestrian improvements to emergency response times in the City. Compare City of Santa Clara response times to other surrounding cities, particularly those that have similar or more bicycling and pedestrian improvements.

Helps BPAC and staff review impact to emergency response times when considering new pedestrian and cycling improvements. May confirm response times will not be affected or may contribute to decreasing response times to help injured cyclists and pedestrians.



Bicycle and Pedestrian Advisory Committee Annual Work Plan Topic Request Form

Requestor Name:

Ken Kratz

Date:

12/12/2025

Proposed Topic Title:

Coordination of volunteer efforts to improve cycling

Background and Discussion: Please provide justification regarding how this topic pertains to the Bicycle and Pedestrian Advisory Committee's roles and responsibilities:

A proposal directing City staff (e.g., Engineering Department and/or the Planning Department) to outreach and coordinate volunteer organizations within the City to improve cycling in our city. Some of those improvements may include volunteers providing cycling safety, bicycle inspection and bicycle repair clinics (clinics to provide access to salvaged bicycle parts and repair instruction) on an ongoing basis (multiple events each year).

Some of volunteer organizations in our city to be contacted include the Youth Activity Center, Santa Clara Unified School District, Santa Clara Senior Center, and the Santa Clara Community Action Program. Those organizations oversee volunteer programs that could work with the current Safe Routes to School program (SRTS), an event that unfortunately occurs only once a year, or could propose other bicycle related programs, particularly ones that are held more often than the SRTS program.

This proposal could be presented to the City's Task Force on Diversity, Equity and Inclusion for review and, if approved, submitted to the City Council for action, approval and funding. That task force is charged with making recommendations and proposing policies to help disenfranchised communities within our city.

(<https://www.santaclaraca.gov/our-city/government/city-committees/task-force-on-diversity-equity-and-inclusion>). The purpose of the task force dovetails with this proposal; the cost to purchase and maintain a bicycle should be no obstacle for anyone of limited means from riding a bicycle in our city.

Helps to create community around cycling and improves safety for all cyclists, including those of limited means.



Bicycle and Pedestrian Advisory Committee Annual Work Plan Topic Request Form

Requestor Name:

Ken Kratz

Date:

12/12/2025

Proposed Topic Title:

San Tomas Aquino Creek Trail enhancements & restoration

Background and Discussion: Please provide justification regarding how this topic pertains to the Bicycle and Pedestrian Advisory Committee's roles and responsibilities:

Provide enhancements to the San Tomas Aquino Creek Trail:

1. Plant trees along the trail to provide shade and to provide some protection from wind.
2. Provide bicycle repair stations. Free access to tools to do minor repairs to bicycles. Station to include pump and tire irons and other hand tools to repair flats and make minor adjustments. The Central Library has a station
3. Install Drinking fountains. provide more of these; I think there may be one or two along the trail.
4. Provide public restrooms
5. Install way-finding signs to local business, attractions and city parks. Signs around light rail stations at Tasman are needed.
6. Install Bicycle repair stations. Free access to tools to do minor repairs to bicycles. Station to include pump and tire irons and other hand tools to repair flats and make minor adjustments. The Central Library has a station.
7. Provide bicycle repair clinic at the intersection with Agnes Road. This is within walking distance of many nearby multifamily residences.
8. Provide benches at corner with Agnew Road as well as at the Intel bridge fin order to watch pedestrian activity.
9. Restore trail ecological information stations; they are faded.
10. Build more pedestrian bridges tying STACT to the public right-of-way so that businesses can make connections to the right-of-way if desired.
11. Restore trail signs. .

Justification: Activity supports cycling and pedestrians in the City.



Bicycle and Pedestrian Advisory Committee Annual Work Plan Topic Request Form

Requestor Name:

Ken Kratz

Date:

12/12/2025

Proposed Topic Title:

neighborhood walkways

Background and Discussion: Please provide justification regarding how this topic pertains to the Bicycle and Pedestrian Advisory Committee's roles and responsibilities:

Investigate and provide neighborhood walkways that provide a shorter walk to get to destinations such as schools, businesses, government facilities. Survey residents to determine where walkways should be installed.

These walkway improvements are normally located between buildings/homes and allow a pedestrian to walk quickly to a destination rather than follow the sidewalks that line the route taken by motor vehicles.

Increases pedestrian activity in the city and reduces dependence on motor vehicles, thereby increasing safety for cyclists and other pedestrians.



Bicycle and Pedestrian Advisory Committee Annual Work Plan Topic Request Form

Requestor Name:

Ken Kratz

Date:

12/12/2025

Proposed Topic Title:

Street environment enhancements

Background and Discussion: Please provide justification regarding how this topic pertains to the Bicycle and Pedestrian Advisory Committee's roles and responsibilities:

Provide street enhancements that make pedestrian and cycling activity more enjoyable. Suggested enhancements to the street, sidewalk, and adjoining properties include:

1. Increase the number of trees of trees along our streets. Trees should be planted and under the care and maintenance of the City rather than private property owners in order to assure quality of installation. Increase City maintenance staff and provide equipment to support this activity.
2. Increase city sidewalk widths to 15' minimum in front of residential multifamily complexes and businesses for all new construction, particularly in the new downtown plan and along El Camino Real. Increase sidewalk width as building height increases; limit building heights to five stories to allow daylight to reach street level for most of the day. Require developers of new building construction to hire a licensed architect to perform daylight studies to determine how day-lighting patterns at street level will be impacted by the new construction.
3. Require new buildings that front the wider sidewalks mentioned above, #2, to have irregularity of building line/setback line in order to increase visual interest in the street.
4. In new development projects and retrofit existing developments if possible, require designs that include short blocks to accommodate more pedestrian activity; use Jane Jacob's ideas mentioned in her book "Death and Life of Great American Cities." as guidelines to make these improvements. Require new developments to provide roundabouts and other traffic calming features at intersecting streets near the development.

Justification: Enhances pedestrian and cycling activity in the City.



Bicycle and Pedestrian Advisory Committee Annual Work Plan Topic Request Form

Requestor Name:

Ken Kratz

Date:

12/12/2025

Proposed Topic Title:

subsidies for cycling and walking to work

Background and Discussion: Please provide justification regarding how this topic pertains to the Bicycle and Pedestrian Advisory Committee's roles and responsibilities:

Provide subsidies to public and private employees who walk or cycle to work. City to provide all types of assistance including monetary subsidies to encourage public and private employees to walk or cycle to work. Some suggestions are:

Employers to provide zero interest loans to those employees who want to purchase a bicycle for commuting to work.

Employers to provide stipend for bicycle repair for those employees who cycle to work.

Employers to provide funds for guaranteed rides home during inclement weather.

Employers to provide funds to purchase annual passes for public transit for those employees who commute by public transit.

Employers to provide fifteen minute flex-time for those employees cycling to work from October to April (when there are fewer daylight hours).

Justification: increases cycling and pedestrian activity in the city.



Bicycle and Pedestrian Advisory Committee Annual Work Plan Topic Request Form

Requestor Name:

Ken Kratz

Date:

12/12/2025

Proposed Topic Title:

public access to commercial and business properties open to the public

Background and Discussion: Please provide justification regarding how this topic pertains to the Bicycle and Pedestrian Advisory Committee's roles and responsibilities:

City to create planning and engineering standards and requirements for better access to private property from the city sidewalk. The need for access enhancements particularly apply to commercial and business properties open to the public. Standards and requirements are to be required for all new construction and also existing developed properties.

Access and walkways should be required for all properties, new and existing that are open to the public, so that pedestrians are not forced to use the motor vehicle driveway nor walk in the landscaping in order to enter a property.

An example of improved access to a property is located at the north-east corner of Homestead Road and Lawrence Expressway. At that location a portion of cyclone property-line fence was removed at the corner thereby creating an opening for pedestrians. A concrete path and pavement striping on the parking lot pavement directs the pedestrian to the businesses on-site. That opening not only reduces the distance the pedestrian has to travel to enter the property but also eliminates the pedestrian/motor-vehicle conflicts at the driveways to the property.

Justification: enhance pedestrian safety



Bicycle and Pedestrian Advisory Committee Annual Work Plan Topic Request Form

Requestor Name:

Ken Kratz

Date:

12/12/2025

Proposed Topic Title:

Bicyclist demographic survey

Background and Discussion: Please provide justification regarding how this topic pertains to the Bicycle and Pedestrian Advisory Committee's roles and responsibilities:

I request staff do periodic surveys, at least once a year, in the field to determine cycling activity at key locations throughout the city. Surveys shall include but not be limited to rough estimates of age groups witnessed cycling (e.g., youths and adults).

Bicycle counts in the bike racks of the schools as well as commuter cyclists exiting trains, light rail and buses shall be conducted yearly. All results to be reported to the BPAC for discussion and recommendations too improve conditions as necessary for those cycling populations under review.

Communities of Concern, as identified in the CSC Bicycle Plan Update 2018, shall be included in these surveys.

Helps BPAC identify and recommend improvements to bicycle facilities and programs that may help those groups under review.



Bicycle and Pedestrian Advisory Committee Annual Work Plan Topic Request Form

Requestor Name:

Ken Kratz

Date:

12/12/2025

Proposed Topic Title:

Public/Private partnerships

Background and Discussion: Please provide justification regarding how this topic pertains to the Bicycle and Pedestrian Advisory Committee's roles and responsibilities:

request that staff provide information to the BPAC on public/private partnerships that may exist or can be developed to fund or provide services for bicycle and pedestrian related improvements and programs.

Benefits cyclists and pedestrians in our city. May accelerate the installation or development of bicycle and pedestrian improvements and programs.



Bicycle and Pedestrian Advisory Committee Annual Work Plan Topic Request Form

Requestor Name:

Ken Kratz

Date:

12/12/2025

Proposed Topic Title:

luminaire base and parking lot sign pole standards for locking bicycles

Background and Discussion: Please provide justification regarding how this topic pertains to the Bicycle and Pedestrian Advisory Committee's roles and responsibilities:

Standardize larking lot luminaire base and parking lot sign pole standards to allow easier locking of bicycles. this request applies to new and existing facilities.

Many bases of parking lot luminaires are of concrete a few feet high that cannot accommodate most commercially available locks. Luminaire bases should be thinner to allow bicycles to be locked to the pole.

Parking lot signs in parking lots (e.g., handicap parking signs, no parking signs, etc.) have signs that can be removed that will allow the theft of a bicycle by lifting the bicycle over the pole. Provide fittings that are tamper proof.

Justification: Enhances cycling activity in the City.



Bicycle and Pedestrian Advisory Committee Annual Work Plan Topic Request Form

Requestor Name:

Ken Kratz

Date:

12/12/2025

Proposed Topic Title:

Bicycle repair clinics

Background and Discussion: Please provide justification regarding how this topic pertains to the Bicycle and Pedestrian Advisory Committee's roles and responsibilities:

Encourage and provide free or low cost bicycle repair clinics throughout the City. Clinics can be non-profit or for profit businesses or provided by the City. They can be located on public or private property. They can be periodic events or permanent features of the City depending on interest and need.

Some suggested locations are:

1. Local bicycle shops and strip malls. Subsidize the clinic at local bicycle shops and other locations such as strip malls. Bicycle shops are most advantageous because not only tools and parts are readily available there, but also the knowledgeable mechanics to teach the students. Two of those bicycle shops are near the San Tomas Aquino Creek Trail, making them ideal candidates for this service.
2. City parks and libraries. A program could be part of the Parks and Recreation Department or could be a small business, profit or non-profit, operating out of a small building in the park. Best location would be on the perimeter of the park next to a City street so that it will be seen by the passing public. This operation would not compete with the bicycle shops in town since it would be providing loaner tools and instruction to cyclists to fix their own bicycles.
3. City schools. A program could be part of the elementary, middle and high school curriculum or an after school activity. It could eventually be run by a small business, profit or non-profit, operating out of a small building on the perimeter of the school next to a City street so that it will be seen by the passing public. This operation would not compete with the bicycle shops in town since it would be providing loaner tools and instruction to cyclists to fix their own bicycles. Make this part of the Safe Routes to School program. Spare bicycle parts can be salvaged from bicycles disposed during the City's Clean-up Campaign and stored in a locked area on the school grounds.
4. San Thomas Aquino Creek Trail. Locate a repair clinic in the vicinity of Agnew Road where it crosses the trail. This is where the Bike to Work Day tables and repair station are set up yearly; it looks like a good place to start. Add other stations as needed.

Justification: Activity supports cycling in the City.



Bicycle and Pedestrian Advisory Committee Annual Work Plan Topic Request Form

Requestor Name:

Ken Kratz

Date:

12/12/2025

Proposed Topic Title:

Residency requirement proposal

Background and Discussion: Please provide justification regarding how this topic pertains to the Bicycle and Pedestrian Advisory Committee's roles and responsibilities:

Bicycle and Pedestrian Advisory Committee (BPAC) to make a recommendation to the City Council to require BPAC members and the City's traffic engineering staff, those staff that support the committee, to live within the city.

According to my on-line search, other cities such as Cupertino and San Jose require BPAC members to live in their respective cities. Some charter cities (the City of Santa Clara is a charter city) require staff to live in or near the city. Santa Clara should follow these examples to require members and staff to live in the city.

These residency requirements are needed because, from my knowledge gained from traffic engineering staff at the BPAC meetings, traffic engineers have repeatedly claimed they use their judgment in making decisions about what improvements or policies are needed in the city. That judgment would be better informed if they lived within the city because they would be more aware of the amount and type of traffic on our streets and typical destinations and routes to get to those destinations (e.g., going to school, commercial destinations, etc.).

Residents know the drawbacks of excessive motor vehicle traffic, such as access, safety, environmental quality (noise and particulate pollution) issues that affect all residents, particularly cyclists and pedestrians.

Justification: would improve the health and safety of city residents, particularly cyclists and pedestrians.

**City of Santa Clara, Department of Public Works
Bicycle and Pedestrian Advisory Committee (BPAC)
Requested Agenda Topics Considered for FY 26/27 Workplan**

Requested Agenda Topic	Requester Name	Ranking	Points	Rankings						
				Betsy Megas	Guillermo Jenaro	Hung Duong	Jonathan Marinaro	Jorge Haro	Ken Kratz	Vernon Shure
Topic 1 - Sidewalk Improvement and Expansion	Duong	1	213	1	11	1	4	2	2	4
Topic 2 - Millikin Basics+ Elementary School – Bicycle and Pedestrian Safety Engineering	Jenaro	2	212	2	1	6	6	5	1	5
Topic 6 - E-bike Safety on San Tomas Aquino Trail	Shure	3	201	5	3	18	2	4	4	1
Topic 3A - Policy on Bike Riding on Sidewalks / Topic 3B - Right-of-way on city creek trails / Topic 3C - Establish right-of-way rules for city sidewalks	Shure / Kratz / Kratz	4	197	3	8	17	1	3	3	6
Topic 4 - BPAC Processes and Governance	Shure	5	180	6	26	7	5	6	6	2
Topic 5 - Parking Enforcement	Shure	6	167	4	7	2	16		5	3
Topic 15 - Proposal for raised sidewalk installations to cross city streets	Kratz	7	137	10	5	12	28	1	11	
Topic 32 - Bicycle repair clinics	Kratz	8	129	8	9	22	11	9	16	
Topic 29 - Bicyclist demographic survey	Kratz	9	123	25	10	16	12	10	8	
Topic 24 - San Tomas Aquino Creek Trail enhancements & restoration	Kratz	10	115	9	19	26	3	7	25	
Topic 25 - Neighborhood walkways	Kratz	11	112	13	15	5	22	8	29	
Topic 10 - Tool lending facility and repair classes	Kratz	12	104	7	16	21	7		15	
Topic 18A - Plant street trees and provide city tree maintenance / Topic 18B - Install tree wells in city streets / Topic 18C - City to require trees on private property	Kratz / Kratz / Kratz	13	99	12	12	24	10		13	
Topic 13 - Update BPAC on new laws and efforts to reduce speed limits & advocate for local control to lower speed limits.	Kratz	14	98	18	4	9	17		24	
Topic 11 - Traffic engineering solutions to common problems	Kratz	15	93	16	2	8	29		22	
Topic 9 - Improve environment for pedestrians and cyclists	Kratz	16	92	20	14	3	15		26	
Topic 26 - Street environment enhancements	Kratz	17	86	11	23	14	24		12	
Topic 27 - Subsidies for cycling and walking to work	Kratz	18	74	15	32	15	20		14	
Topic 17 - Proposed zoning and development requirements to enhance walking and cycling	Kratz	19	72	22	24	11	13		28	
Topic 16 - Bicycle Safety and Bike Citation Diversion Classes	Kratz	20	70	21	17	10	19		33	
Topic 22 - Report on emergency response times	Kratz	21	70	27	6	30	18		19	
Topic 21 - Proposal to standardize city sidewalk widths, building setbacks and building materials	Kratz	22	69	28	25	13	8		27	
Topic 8 - City Planning for pedestrian and cyclists	Kratz	23	62	31	22	20	14		21	
Topic 12A - Upgrade this form / Topic 12B - Revise Annual Work Plan Topic Request Form	Kratz / Kratz	24	62	17	27	32	25		7	
Topic 19 - Install parking meters and adopt a parking lot tax	Kratz	25	54	30	29	4	30		23	
Topic 23 - Coordination of volunteer efforts to improve cycling	Kratz	26	52	14	18	29	26		31	
Topic 28 - Public access to commercial and business properties open to the public	Kratz	27	49	26	31	27	27		10	
Topic 31 - Luminaire base and parking lot sign pole standards for locking bicycles	Kratz	28	49	24	33	23	9		32	
Topic 33 - Residency requirement proposal	Kratz	29	49	33	13	33	33		9	
Topic 30 - Public/Private partnerships	Kratz	30	48	23	20	28	21		30	
Topic 7 - Upgrade zoning maps to include more mixed use & bus service	Kratz	31	47	32	21	19	31		20	
Topic 20 - Development review process: review & implement Impact Fees for vehicle trip generation	Kratz	32	45	29	30	25	23		18	
Topic 14 - BPAC to lead the review and the creation of new city standards for new development	Kratz	33	43	19	28	31	32		17	

Meeting	Item#	Agenda Item	Requestor	Score
August	1	Public Presentations	Staff	
	2	Consent Calendar - Minutes	Staff	
	3	Police Verbal Update	Staff	
	4	Public Works Verbal Update	Staff	
	5	VTA BPAC Verbal Update	Staff	
	6	Santa Clara Station Area Task Force Update	Staff	
	7	BPAC Election of Chair and Vice Chair	Staff	
	8	Complete Streets Review for 2027 Annual Paving Project	Staff	
	9	Brown Act Training	Staff	
	10	Benton Street Bikeway Project (Complete Streets Review)	Staff	
	11	Santa Clara Caltrain/BART Station Multimodal Crossing Feasibility Study	Staff	
	12	Bike Plan Policy	Staff	
	13	Sidewalk Improvement and Expansion	H. Duong	213
	14	Millikin Basics+ Elementary School – Bicycle and Pedestrian Safety Engineering	G. Jenaro	212
	15	Subcommittee Reports	BPAC	
October	1	Public Presentations	Staff	
	2	Consent Calendar - Minutes	Staff	
	3	Police Verbal Update	Staff	
	4	Public Works Verbal Update	Staff	
	5	VTA BPAC Verbal Update	Staff	
	6	Santa Clara Station Area Task Force Update	Staff	
	7	Consent Calendar - Approve 2027 BPAC Meeting Dates	Staff	
	8	Annual Work Plan Topic Process	Staff	
	9	North Santa Clara Transit Station Access and Circulation Study	Staff	
	10	Creek Trail Master Plan (Final Plan for Review and Approval Recommendation)	Staff	
	11	Wayfinding Project Introduction	Staff	
	12	E-bike Safety on San Tomas Aquino Trail	V. Shure	201
	13	Policy on Bike Riding on Sidewalks; Right-of-way on city creek trails; Establish right-of-way rules for city sidewalks	V. Shure; K. Kratz; K. Kratz	197
	14	Subcommittee Reports	BPAC	

FY 2026/27 BPAC Work Plan

Meeting	Item#	Agenda Item	Requestor	Score
January	1	Public Presentations	Staff	
	2	Consent Calendar - Minutes	Staff	
	3	Police Verbal Update	Staff	
	4	Public Works Verbal Update	Staff	
	5	VTA BPAC Verbal Update	Staff	
	6	Santa Clara Station Area Task Force Update	Staff	
	7	Annual Update on New Traffic Laws	Staff	
	8	FY27/28 TDA & TFCA Funding Recommendations	Staff	
	9	Annual Work Plan Topic Recommendations	Staff	
	10	2028 Annual Paving Project - List of Streets	Staff	
	11	Review Wayfinding Design	Staff	
	12	BPAC Processes and Governance	V. Shure	180
	13	Proposal for raised sidewalk installations to cross city streets	K. Kratz	137
	14	Subcommittee Reports	BPAC	
March	1	Public Presentations	Staff	
	2	Consent Calendar - Minutes	Staff	
	3	Police Verbal Update	Staff	
	4	Public Works Verbal Update	Staff	
	5	VTA BPAC Verbal Update	Staff	
	6	Santa Clara Station Area Task Force Update	Staff	
	7	Annual FY Work Plan Approval	Staff	
	8	Final Wayfinding Plan Approval Recommendation	Staff	
	9	Final North Santa Clara Transit Station Access Plan Approval Recommendation	Staff	
	10	Santa Clara Caltrain/BART Station Multimodal Crossing Feasibility Study	Staff	
	11	Parking Enforcement	V. Shure	167
	12	Bicycle Repair Clinics	K. Kratz	129
	13	Subcommittee Reports	BPAC	
June	1	Public Presentations	Staff	
	2	Consent Calendar - Minutes	Staff	
	3	Police Verbal Update	Staff	
	4	Public Works Verbal Update	Staff	
	5	VTA BPAC Verbal Update	Staff	
	6	Santa Clara Station Area Task Force Update	Staff	
	7	Bicyclist Demographic Survey	K. Kratz	123
	8	San Tomas Aquino Creek Trail Enhancements & Restoration	K. Kratz	115
	9	Subcommittee Reports	BPAC	



Agenda Report

26-191

Agenda Date: 3/23/2026

REPORT TO BICYCLE AND PEDESTRIAN ADVISORY COMMITTEE

SUBJECT

Selection of Valley Transportation Authority Bicycle and Pedestrian Advisory Committee Representative (Megas)

BACKGROUND

The Santa Clara Valley Transportation Authority (VTA) requests that cities appoint members to the VTA Bicycle and Pedestrian Advisory Committee (BPAC). The VTA BPAC is comprised of one representative from each of the 16 member agencies, which includes the City of Santa Clara. VTA BPAC members serve two-year terms starting on July 1 and ending on June 30 of the second year and may serve successive terms. The VTA BPAC advises the VTA Board on matters pertaining to funding and planning issues for bicycle and pedestrian projects and serves as the countywide bicycle advisory committee for Santa Clara County. Typical duties involve review of regional plans related to bicycling and walking, review of funding proposals for projects and consideration of specialized bicycle and pedestrian issues of regional or cross-jurisdictional significance.

The appointee will serve a term which will begin after the City Council appoints the City's VTA BPAC member and the VTA Board of Directors ratifies the appointment. Members of the VTA BPAC must live or work in Santa Clara County during their term. VTA requires that the appointee be either a representative of the City's BPAC or an individual interested in bicycle or pedestrian issues and not a City or VTA staff person.

DISCUSSION

At the March 23, 2026 BPAC meeting, the BPAC should nominate and select one candidate to serve as the City of Santa Clara's VTA BPAC representative. The City's representative will serve on the VTA BPAC from July 1, 2026 to June 30, 2028. Staff confirmed that the VTA BPAC does not have alternate members.

The candidate must be a current BPAC member, or an individual interested in bicycle or pedestrian issues. The selected candidate will then be recommended to the City Council for appointment.

RECOMMENDATION

The BPAC to select a candidate for City Council approval to serve as the Santa Clara representative on the VTA BPAC from July 1, 2026 to June 30, 2028.

Written by: Ralph Garcia, Senior Civil Engineer, Public Works

Reviewed by: Carol Shariat, Interim Transportation Manager, Public Works

Approved by: Michael Liw, Assistant Director/City Engineer, Public Works



City of Santa Clara

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Agenda Report

26-180

Agenda Date: 3/23/2026

REPORT TO BICYCLE AND PEDESTRIAN ADVISORY COMMITTEE

SUBJECT

Public Works Verbal Update (Liw/Shariat)

DISCUSSION

Staff will provide a verbal update on the following items:

1. Vision Zero Council Meeting
2. Benton Street Bikeway Project
3. Benton/Brokaw Feasibility Study
4. Traffic Division Vacancies
5. SBLX
6. World Cup Matches

Approved by: Michael Liw, Assistant Director/City Engineer, Public Works



Agenda Report

26-221

Agenda Date: 3/23/2026

REPORT TO BICYCLE AND PEDESTRIAN ADVISORY COMMITTEE

SUBJECT

Complete Streets Review for the Uncontrolled Crosswalk Improvements Phase II Project (Shariat)

BACKGROUND

The Department of Public Works (DPW) is preparing the design for the Uncontrolled Crosswalk Improvements Phase II Project. An uncontrolled crosswalk is a striped pedestrian street crossing where motorists do not have a traffic control device (i.e. stop sign, traffic signal, flashing beacons) controlling their movement through the crosswalk. There are approximately 200 uncontrolled crosswalk locations throughout the City and staff periodically receive requests from residents to install some type of control measure at various locations.

The City performed studies at 35 high priority uncontrolled crosswalk locations to identify potential improvements and to develop a priority ranking order for implementation of any improvements. Improvements that were considered for installation include pedestrian warning beacons, traffic signs, roadway striping, street lighting, Americans with Disabilities Act (ADA) curb ramps and curb extensions. The City has separated the projects into multiple phases to complete the identified improvements at these 35 locations. The first phase of the project was completed in June 2024. This second phase will implement improvements at the following 13 uncontrolled crosswalk locations throughout the City (Attachment 1):

- Calle De Primavera/Calle De Escuela
- Agnew/Davis
- Agnew/Avina Circle
- Agnew/Garrity (West)
- Mission College/Our Lady's Way (South)
- Walsh/Northwestern
- Monroe/Quinn
- Homestead/Los Olivos
- Benton/Curtis
- Newhall/Bohannon
- Winchester/Tulip
- Winchester/Dolores
- Kiely/Mauricia

As set forth by the City of Santa Clara Complete Streets Policy, relevant transportation projects shall be coordinated with the BPAC to gather comments and recommendations regarding Complete Streets features that should be incorporated into the project.

DISCUSSION

The project is currently at the 65 percent design phase (Attachment 2). The project is considered a relevant transportation project to be coordinated with BPAC, and as such, these plans are attached for BPAC to review and provide feedback.

Written by: Carol Shariat, Acting Transportation Manager, Public Works

Approved by: Michael Liw, Assistant Director/City Engineer, Public Works

ATTACHMENT

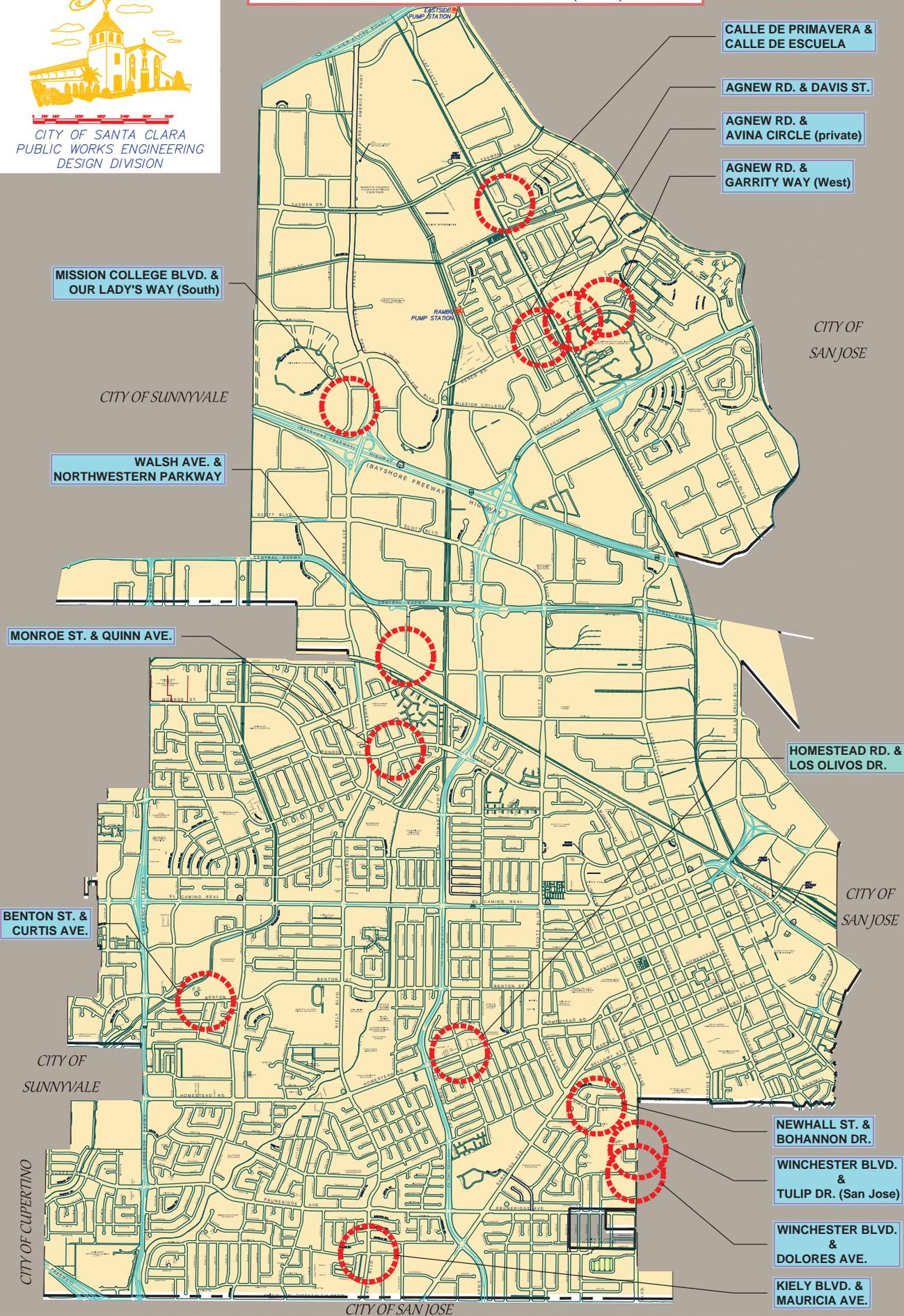
1. Project Location Map

2. 65% Project Plans



CITY OF SANTA CLARA
PUBLIC WORKS ENGINEERING
DESIGN DIVISION

UNCONTROLLED CROSSWALKS PHASE II PROJECT LOCATION MAP EXHIBIT (N.T.S.)



CITY OF SUNNYVALE

CITY OF
SAN JOSE

CITY OF
SAN JOSE

CITY OF
SUNNYVALE

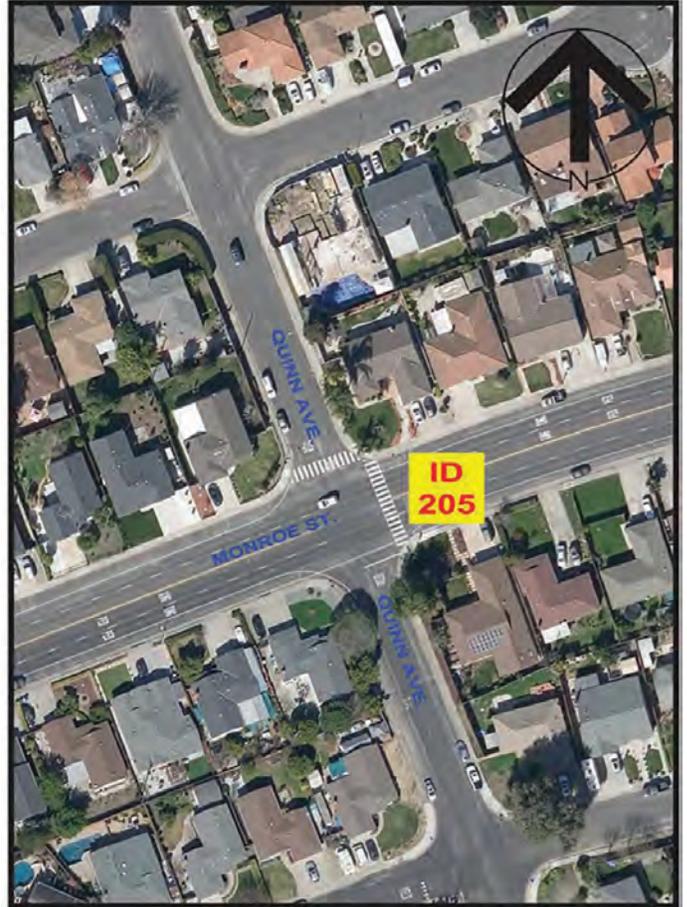
CITY OF CUPERTINO

CITY OF SAN JOSE

UNCONTROLLED CROSSWALKS PHASE 2
LOCATION MAP EXHIBIT



KIELY BLVD. & MAURICIA AVE.



MONROE ST. & QUINN AVE.



HOMESTEAD RD. & LOS OLIVOS DR.



AGNEW RD. & WEST OF GARRITY WAY

UNCONTROLLED CROSSWALKS PHASE 2
LOCATION MAP EXHIBIT



NEWHALL ST. & BOHANNON DR.



AGNEW RD. & AVINA CIRCLE (PVT.)



BENTON ST. & CURTIS AVE.

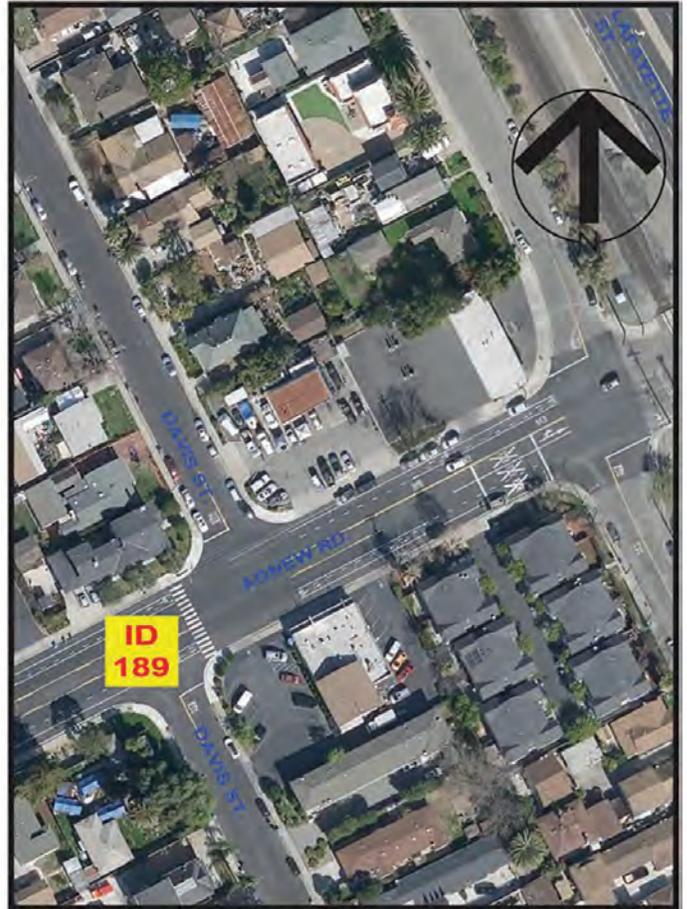


WINCHESTER BLVD. & TULIP RD (SAN JOSE)
WINCHESTER BLVD. & DOLORES AVE.

UNCONTROLLED CROSSWALKS PHASE 2
LOCATION MAP EXHIBIT



CALLE DE PRIMAVERA & CALLE DE ESCUELA



AGNEW RD. & DAVIS ST.



MISSION COLLEGE BLVD. & OUR LADY OF PEACE



WALSH AVE. & NORTHWESTERN PARKWAY



PUBLIC WORKS DEPARTMENT

UNCONTROLLED CROSSWALKS IMPROVEMENTS - PHASE II

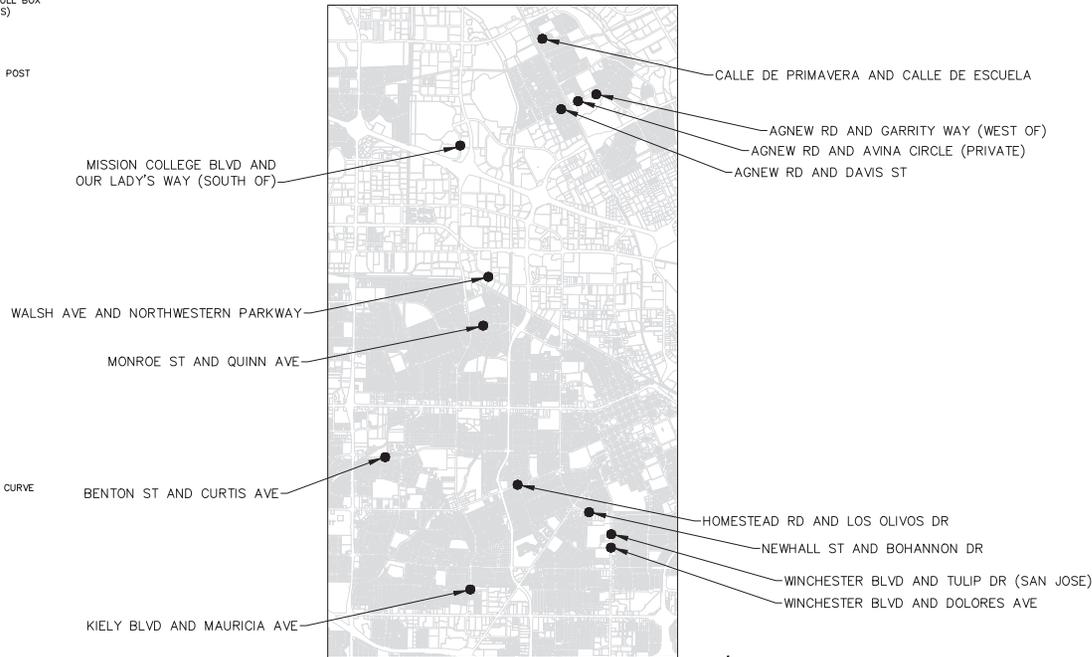
SHEET INDEX		
SHEET NUMBER	SHEET TITLE	DESCRIPTION
1	CV-01	COVER SHEET
2-3	GN-01 - GN-02	GENERAL NOTES
4-9	CD-01 - CD-06	CONSTRUCTION DETAIL SHEETS
10-11	E-01 - E-02	EROSION CONTROL SHEETS
12-24	P-01 - P-13	CIVIL SHEETS
25-37	SS-01 - SS-13	SIGNING AND STRIPING SHEETS
38-52	TS-01 - TS-15	RRFB AND LIGHTING SHEETS

LEGEND SYMBOLS

PROPOSED	EXISTING	DESCRIPTION
---	---	CONDUIT
---	---	FACE OF CURB
---	---	LIP OF GUTTER
---	---	BACK OF SIDEWALK
---	---	FRONT OF SIDEWALK
---	---	DRIVEWAY
---	---	RIGHT-OF-WAY
---	---	PUBLIC UTILITY EASEMENT
---	---	TRAFFIC SIGNAL CONDUIT
---	---	INTERCONNECT LINE
---	---	FIBER OPTIC LINE
---	---	OVERHEAD ELECTRICAL LINE
---	---	TELEPHONE LINE
---	---	SANITARY SEWER LINE AND MANHOLE
---	---	STORM SEWER LINE AND MANHOLE
---	---	WATER LINE
---	---	ELECTRICAL LINE
---	---	GAS LINE
---	---	RECLAIMED WATER LINE
---	---	POWER POLE
---	---	FIRE HYDRANT
---	---	LUMINAIRE
---	---	PULL BOX (NO. 6 AND NO. 6E)
---	---	INTERCONNECT-RELATED PULL BOX (TYPE AS NOTED ON PLANS)
---	---	TRAFFIC SIGNAL POLE
---	---	CITY FIBER LINE
---	---	SVP FIBER LINE
---	---	PEDESTRIAN PUSH BUTTON POST

ABBREVIATIONS

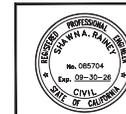
AB	ABANDON CONDUIT AND REMOVE CONDUCTORS
AWG	AMERICAN WIRE GAUGE
BC	INSTALL PULL BOX IN EXISTING CONDUIT RUN
B/C	BACK OF CURB
B/SW	BACK OF SIDEWALK
CB	INSTALL CONDUIT INTO EXISTING PULL BOX
CC	CONNECT NEW CONDUIT TO EXISTING CONDUIT
C/L	CENTERLINE
DH	DETECTOR HANDHOLE
EX.	EXISTING
FDU	FIBER DISTRIBUTION UNIT
FL	FLOW LINE
FDC	FACE OF CURB
FTP	FIBER TERMINATION PANEL
F/W	FRONT OF SIDEWALK
IC	INTERCONNECT
MIN	MINIMUM
N.T.S.	NOT TO SCALE
PBAP	PUSH BUTTON ASSEMBLY POST
PCC	PORTLAND CEMENT CONCRETE OR POINT OF COMPOUND CURVE
PPB	PEDESTRIAN PUSH BUTTON
PEU	PHOTO ELECTRIC UNIT
P/L	PROPERTY LINE
PUE	PUBLIC UTILITY EASEMENT
PVC	POLYVINYL CHLORIDE
RC	EQUIPMENT OR MATERIAL TO BE REMOVED AND BECOME PROPERTY OF THE CONTRACTOR
R/W	RIGHT OF WAY
SC	SPLICE NEW TO EXISTING CONDUCTORS
SIC	SIGNAL INTERCONNECT CABLE
SMFO	SINGLE MODE FIBER OPTIC
SVTS	SILICON VALLEY INTELLIGENT TRANSPORTATION SYSTEMS
SVP	SILICON VALLEY POWER
TBD	TO BE DETERMINED



LOCATION MAP
SCALE: N.T.S.



65% DESIGN PLANS
NOT FOR CONSTRUCTION
JANUARY 2026



ACCEPTANCE OF PLANS:
THE CITY OF SANTA CLARA HEREBY ACCEPTS THESE PLANS FOR CONSTRUCTION, AS BEING IN GENERAL COMPLIANCE WITH PLANS PREPARATION REQUIREMENTS OF THIS GOVERNMENT, RESPONSIBILITY FOR THE COMPLETENESS AND ACCURACY OF THE PLANS AND RELATED DESIGN RESIDES WITH THE ENGINEER AND ENGINEERING FIRM OF RECORD.

MICHAEL JACK LIW DATE: _____
ASSISTANT DIRECTOR OF PUBLIC WORKS / CITY ENGINEER

REVIEWED BY:

ELECTRIC DEPARTMENT DATE: _____

WATER AND SEWER UTILITIES DEPARTMENT DATE: _____

STREET & AUTOMOTIVE SERV. MAINTENANCE DIV. DATE: _____

TRAFFIC ENGINEERING DIVISION DATE: _____

DESIGN DIVISION DATE: _____

STATEMENT OF PEER REVIEW CERTIFICATION

THE UNDERSIGNED HEREBY CERTIFIES THAT A PROFESSIONAL PEER REVIEW OF THESE PLANS AND THE REQUIRED DESIGNS WAS CONDUCTED BY ME, A PROFESSIONAL ENGINEER WITH EXPERTISE AND EXPERIENCE IN THE APPROPRIATE FIELDS OF ENGINEERING EQUAL OR GREATER THAN THE ENGINEER OF RECORD, AND THAT APPROPRIATE CORRECTIONS HAVE BEEN MADE.

NIKITA A. PETROV DATE: 1/9/2026
NAME DATE

UNCONTROLLED CROSSWALKS IMPROVEMENTS - PHASE II

PROJECT No. CE 24-25-07 TRACING No. 12,444-D

GENERAL NOTES

1. THE CONTRACTOR IS RESPONSIBLE FOR CONTACTING THE RESPECTIVE UTILITY COMPANIES AND AFFECTED AGENCIES FOR VERIFICATION OF LOCATION AND DEPTHS OF EXISTING UTILITIES. THE CONTRACTOR SHALL NOTIFY UNDERGROUND SERVICE ALERT (USA), AT 811, TWO (2) WORKING DAYS BUT NOT MORE THAN FOURTEEN (14) CALENDAR DAYS PRIOR TO COMMENCING ANY EXCAVATION.
2. CONTRACTOR SHALL COOPERATE AND COORDINATE WITH OTHER CONTRACTORS WORKING WITHIN OR ADJACENT TO THE LIMITS OF WORK TO AVOID ANY DELAY AND CONFLICT.
3. FOR ALL TRAFFIC SIGNAL, CABINET AND CONDUIT INSTALLATION OPERATIONS, THE CONTRACTOR SHALL POTHOLE TO VERIFY THE LOCATION OF UNDERGROUND UTILITIES AT THE DIRECTION OF THE ENGINEER. THE CONTRACTOR SHALL NOTE THAT NOT ALL UTILITIES ARE SHOWN ON THE PLANS. THE CONTRACTOR SHALL POTHOLE ALL USA MARKINGS, WHICH MAY NOT NECESSARILY BE SHOWN ON THE PLANS. ALL INSTALLED CONDUIT AND PULL BOXES SHALL MEET CLEARANCE DISTANCES FROM EXISTING UTILITIES INDICATED IN SILICON VALLEY POWER STANDARD UG1250.
4. THE CONTRACTOR SHALL CONTACT THE CITY OF SANTA CLARA FIVE (5) WORKING DAYS BEFORE ANY SIGNAL AND/OR COMMUNICATION SHUTDOWNS ARE REQUIRED FOR THE INSTALLATION OF EQUIPMENT OR CABLING AS PART OF THIS PROJECT. SEE THE SPECIFICATIONS FOR MORE DETAILS.
5. ALL NEW CONDUIT WITH 120 V (OR HIGHER) CIRCUITS SHALL CONTAIN ONE (1) NO. 8 AWG SOLID COPPER BONDING WIRE. ALL CONDUIT WITH NO METALLIC CONDUCTORS SHALL HAVE A NO. 8 AWG THW LOCATING WIRE. THE WIRE INSULATING JACKET SHALL BE COLOR-CODED GREEN.
6. LOCATIONS OF POLES, PULL BOXES, EQUIPMENT, AND POSITIONING OF DETECTORS ARE SCHEMATIC UNLESS OTHERWISE NOTED. BEFORE BEGINNING ANY CONSTRUCTION OPERATION, THE CONTRACTOR SHALL CHECK FOR CONFLICTS WITH UNDERGROUND UTILITIES, OVERHEAD UTILITIES, OR OTHER OBSTACLES. IF EQUIPMENT OR POLES MUST BE RELOCATED, THE NEW LOCATION IS SUBJECT TO WRITTEN APPROVAL BY THE CITY PRIOR TO INSTALLATION. CONDUIT ALIGNMENTS, PULL BOX LOCATIONS, AND FOUNDATION LOCATIONS STAKED BY THE CONTRACTOR ARE SUBJECT TO APPROVAL BY THE CITY BEFORE BEGINNING WORK ON THAT ITEM. NEW SUBSTRUCTURES SHALL BE INSTALLED SUCH THAT CLEARANCE REQUIREMENTS FROM SVP FACILITIES NOTED IN SVP STANDARD DESIGN DOCUMENT UG 1250 (SHOWN ON SHEET 10) ARE MET.
7. ALL WORK, MATERIAL, AND EQUIPMENT SHALL CONFORM TO THE REQUIREMENTS OF THE CITY OF SANTA CLARA'S STANDARD DETAILS FOR PUBLIC WORKS CONSTRUCTION, THE CALIFORNIA DEPARTMENT OF TRANSPORTATION STANDARD PLANS, DATED 2023, AND SPECIFICATIONS, DATED 2023, THE SPECIFICATIONS FOR THIS PROJECT, AND THE 2023 CALIFORNIA ELECTRICAL CODE.
8. CONDUIT INSTALLATION SHALL BE BY TRENCHING METHODS ACROSS TRAVELED WAYS, EXCEPT AS OTHERWISE ALLOWED IN THE SPECIAL PROVISIONS OR AS INDICATED ON THE PLANS.
9. TRAFFIC CONTROL MUST BE PERFORMED IN ACCORDANCE WITH THE 2014 CALIFORNIA MANUAL FOR UNIFORM TRAFFIC CONTROL DEVICES.
10. THE CONTRACTOR SHALL ADJUST TO NEW GRADE ALL (EXISTING AND NEW) PULL BOXES LOCATED WITHIN NEW (OR REPLACED) CURB RAMPS, WHILE PROTECTING EXISTING UTILITIES AND EQUIPMENT. CONTRACTOR SHALL CONTACT DAVE PADILLA, T&D DIVISION MANAGER AT (408) 615-5630 OR DPADILLA@SVPPOWER.COM A MINIMUM OF 4 WEEKS PRIOR TO CONSTRUCTION TO COORDINATE ADJUSTING OF EXISTING SVP ELECTRIC FACILITIES.
11. DO NOT APPLY THERMOPLASTIC STRIPING USING HAND CART METHODS. THERMOPLASTIC TRAFFIC STRIPES WILL BE MEASURED BY THE LINEAR FOOT REGARDLESS OF THE NUMBER OF INDIVIDUAL STRIPES COMPRISING THE DETAIL.
12. CONTRACTOR SHALL REPLACE ALL PAVERS IN KIND WHERE INSTALLATION OF INFRASTRUCTURE REQUIRES REMOVAL AND REPLACEMENT OF EXISTING PAVERS.
13. ADJUST WATER METER BOX, WATER VALVE BOX, AND SEWER CLEANOUT COVERS TO FINISHED GRADE.
14. CONTACT WATER DEPARTMENT FACILITIES TECHNICIAN AT (408) 615-2052 A MINIMUM OF 48 HOURS PRIOR TO START OF CONSTRUCTION.
15. WHEN WORKING IN OR AROUND EXISTING PULL BOXES WITH ENERGIZED CABLE, FIBER OPTIC OR ANY OTHER COMMUNICATION CABLE, THE CONTRACTOR SHALL COORDINATE WITH PUBLIC WORKS INSPECTOR TO NOTIFY APPLICABLE DEPARTMENTS.
16. CONTRACTOR IS RESPONSIBLE TO ARRANGE FOR INSPECTION OF ELECTRICAL EQUIPMENT THAT IS FURNISHED AND/OR INSTALLED BY THE CONTRACTOR, INCLUDING CONDUCTOR TERMINATIONS AND TORQUE TEST IN PANEL, IN A DE-ENERGIZED CONDITION.
17. ALL WORK MUST COMPLY WITH THE CITY OF SANTA CLARA STREET & SURFACE IMPROVEMENTS STANDARD DETAILS AND 2023 CALTRANS STANDARD SPECIFICATIONS AS SPECIFIED.
18. TRAFFIC CONTROL MUST BE PERFORMED IN ACCORDANCE WITH THE LATEST REVISION OF THE CALIFORNIA MANUEL FOR UNIFORM TRAFFIC CONTROL DEVICES. CONTRACTOR MUST HAVE AN APPROVED TRAFFIC CONTROL PLAN PRIOR TO BEGINNING WORK. PREPARE AND SUBMIT A TRAFFIC CONTROL PLAN TO THE ENGINEER FOR REVIEW AND APPROVAL. UPDATE AND RESUBMIT AS NEEDED UNTIL THE PLAN IS APPROVED.
19. COMPLY WITH THE REQUIREMENTS FOR WATER POLLUTION CONTROL OUTLINED IN SECTION 13 OF THE 2024 CALTRANS STANDARD SPECIFICATIONS.
20. PREPARE A LEAD COMPLIANCE PLAN TO COVER REMOVAL OF THE EXISTING STRIPING THAT IS SHOWN TO BE IN CONFLICT WITH THE PROPOSED STRIPING OUTSIDE THE LIMITS OF SLURRY SEAL. RESIDUE FROM THE REMOVAL OF PAINTED OR THERMOPLASTIC TRAFFIC STRIPES AND PAVEMENT MARKINGS CONTAINS LEAD FROM THE PAINT OR THERMOPLASTIC. THE AVERAGE LEAD CONCENTRATIONS ARE LESS THAN 1,000 MG/KG TOTAL LEAD AND 5 MG/L SOLUBLE LEAD. THIS RESIDUE:
 - a. IS A NONHAZARDOUS WASTE
 - b. DOES NOT CONTAIN HEAVY METALS IN CONCENTRATIONS EXCEEDING THE THRESHOLDS ESTABLISHED BY THE HEALTH AND SAFETY CODE AND 22 CA CODE OF REGS
 - c. IS NOT REGULATED UNDER THE FEDERAL RESOURCE CONSERVATION AND RECOVERY ACT (RCRA), 42 USC 6901 ET SEQ. MANAGEMENT OF THIS MATERIAL EXPOSES WORKERS TO HEALTH HAZARDS THAT MUST BE ADDRESSED IN YOUR LEAD COMPLIANCE PLAN.
21. PROPOSED FEE IS CONSIDERED TO BE FULL COMPENSATION FOR PERFORMING ALL WORK AS SHOWN ON THE PLANS AND AS REQUIRED BY THE SPECIFICATIONS. NO ADDITIONAL PAYMENTS WILL BE MADE.
22. ALL CONSTRUCTION MUST BE TO THE CITY OF SANTA CLARA STANDARDS OR 2024 CALTRANS STANDARD PLANS, AS NOTED ON THESE PROJECT PLANS AND ACCEPTED BY THE PUBLIC WORKS INSPECTOR. STANDARD PLANS ARE AVAILABLE AT THE OFFICE OF THE PUBLIC WORKS INSPECTOR AND ON THE CITY'S WEBSITE.
23. CONTRACTOR SHALL MEET WITH CITY OF SANTA CLARA AT LEAST 48 HOURS PRIOR TO START OF CONSTRUCTION, 24 HOURS NOTICE REQUIRED ON ALL INSPECTIONS.
24. CONTRACTOR IS RESPONSIBLE TO MAKE ALL ARRANGEMENTS FOR SITE INSPECTIONS AND INSURE THAT ALL CURRENT STANDARDS FOR THE CITY OF SANTA CLARA ARE FOLLOWED PRIOR TO THE BEGINNING ANY PHASE OF CONSTRUCTION WORK.

25. CONSTRUCTION SHALL BE LIMITED TO BETWEEN THE HOURS OF 7:00 A.M. AND 6:00 P.M., MONDAY THRU FRIDAY AND INSPECTION REQUESTS SHALL BE LIMITED TO NORMAL CITY BUSINESS HOURS: 8:00 A.M. TO 5:00 P.M., MONDAY THRU FRIDAY.
26. SLURRY SEAL LIMITS AS SHOWN PER THESE PLANS. TREATMENT AND EXACT LIMITS SHALL BE MARKED AND CONFIRMED IN THE FIELD BY THE CITY INSPECTOR PRIOR TO CONSTRUCTION. SEE CITY OF SANTA CLARA STANDARD SPECIFICATIONS DATED JUNE 2014 FOR DETAILS.
27. TIE OUT ALL MANHOLES, WATER VALVE COVERS, CLEAN OUTS, PULL BOXES, 50 BOXES, AND MONUMENTS, THEN PROVIDE DATA TO THE CITY REPRESENTATIVE 14 DAYS PRIOR TO ANY SURFACE TREATMENT.
28. CONTRACTOR SHALL PROTECT MONUMENTS IN PLACE AND SHALL NOTIFY THE CITY INSPECTOR IF ANY ARE IDENTIFIED WITHIN THE LIMITS OF THE WORK.
29. CONTRACTOR SHALL PROTECT IN PLACE ALL EXISTING PAINT OR THERMOPLASTIC OUTSIDE OF PAVEMENT REHABILITATION LIMITS, OR LIMITS OF PROPOSED STRIPING AS SHOWN ON SIGNING AND STRIPING SHEETS.
30. APPLY SLURRY SEAL FOR ENTIRE ROADWAY WIDTH FROM EXISTING LIP OF GUTTER TO THE LIMITS SHOWN ON THE PLANS.
31. CONTRACTOR TO MAINTAIN DRIVEWAY ACCESS AT ALL TIMES.
32. CRACK SEAL ANY CRACKS $\frac{1}{4}$ " OR GREATER IN SLURRY LOCATIONS. CLEAN FREE OF DEBRIS, DIRT, ETC.
33. CONTRACTOR SHALL BE RESPONSIBLE FOR ANY MONUMENTATION AND BENCHMARKS WHICH WILL BE DISTURBED OR DESTROYED BY CONSTRUCTION. SUCH POINTS SHALL BE REFERENCED AND REPLACED WITH APPROPRIATE MONUMENTATION BY A LICENSED LAND SURVEYOR OR REGISTERED CIVIL ENGINEER AUTHORIZED TO PRACTICE LAND SURVEYING. A CORNER RECORD OR RECORD OF SURVEY AS APPROPRIATE, SHALL BE FILED BY THE LICENSED LAND SURVEYOR OR REGISTERED CIVIL ENGINEER AS REQUIRED BY THE MOST CURRENT VERSION OF THE LAND SURVEYORS ACT.
34. ALL UTILITY BOXES, LIDS, COVERS, FRAMES, AND GRATES SHALL BE PROTECTED IN PLACE UNLESS NOTED OTHERWISE.
35. THE CONTRACTOR SHALL REVIEW AND VERIFY ALL DIMENSIONS AND FIELD CONDITIONS THAT MAY AFFECT CONSTRUCTION. ANY DISCREPANCIES ON THE DRAWINGS SHALL BE IMMEDIATELY BROUGHT TO THE ATTENTION OF THE ENGINEER BEFORE COMMENCING WORK. NO FIELD CHANGES OR DEVIATIONS FROM DESIGN ARE TO BE MADE WITHOUT PRIOR APPROVAL OF THE ENGINEER AND IF APPLICABLE THE CITY. NO CONSIDERATION WILL BE GIVEN TO CHANGE ORDERS FOR WHICH THE CITY, ENGINEER, AND OWNER WERE NOT CONTACTED PRIOR TO CONSTRUCTION OF THE AFFECTED ITEM.
36. CONTRACTOR SHALL CALL 811 AN ADEQUATE AMOUNT OF TIME PRIOR TO COMMENCING CONSTRUCTION OR ANY EXCAVATION.
37. CONTRACTOR SHALL USE EXTREME CAUTION AS THE SITE CONTAINS VARIOUS KNOWN AND UNKNOWN PUBLIC UTILITIES.
38. THE CONTRACTOR IS RESPONSIBLE FOR COORDINATING ANY ADJUSTMENTS AND RELOCATIONS OF EXISTING UTILITIES THAT CONFLICT WITH THE PROPOSED IMPROVEMENTS, INCLUDING BUT NOT LIMITED TO, ADJUSTING EXISTING MANHOLES TO MATCH PROPOSED GRADE.
39. THE CONTRACTOR SHALL BE RESPONSIBLE TO OBTAIN ALL REQUIRED CONSTRUCTION PERMITS, APPROVALS, AND BONDS PRIOR TO CONSTRUCTION.
40. THE CONTRACTOR SHALL HAVE AVAILABLE AT THE JOB SITE AT ALL TIMES A COPY OF THE CONTRACT DOCUMENTS INCLUDING PLANS, PROJECT AND CITY SPECIFICATIONS, AND SPECIAL CONDITIONS, COPIES OF ANY REQUIRED CONSTRUCTION PERMITS, AND INSPECTION REPORTS.
41. ALL SHOP DRAWINGS AND OTHER DOCUMENTS THAT REQUIRE ENGINEER REVIEW SHALL BE SUBMITTED BY THE CONTRACTOR SUFFICIENTLY IN ADVANCE OF CONSTRUCTION OF THAT ITEM, SO THAT NO LESS THAN 10 BUSINESS DAYS FOR REVIEW AND RESPONSE IS AVAILABLE.
42. ALL NECESSARY INSPECTIONS AND/OR CERTIFICATIONS REQUIRED BY CODES, JURISDICTIONAL AGENCIES, AND/OR UTILITY SERVICE COMPANIES SHALL BE PERFORMED PRIOR TO USE OF THE FACILITY AND THE FINAL CONNECTION OF SERVICES.
43. CONTRACTOR'S BID PRICE SHALL INCLUDE ALL INSPECTION FEES.
44. CONTRACTOR IS RESPONSIBLE FOR ALL MATERIALS TESTING AND CERTIFICATION, UNLESS SPECIFIED OTHERWISE BY THE OWNER. ALL MATERIALS TESTING SHALL BE COORDINATED WITH THE APPROPRIATE CITY INSPECTOR AND COMPLY WITH CITY STANDARD SPECIFICATIONS AND 2024 CALTRANS STANDARD SPECIFICATIONS. TESTING SHALL BE PERFORMED BY AN APPROVED INDEPENDENT AGENCY FOR TESTING MATERIALS. OWNER SHALL APPROVE THE AGENCY NOMINATED BY THE CONTRACTOR FOR MATERIALS TESTING.
45. ALL COPIES OF MATERIALS TEST RESULTS SHALL BE SENT TO THE CITY AND ENGINEER DIRECTLY FROM THE TESTING AGENCY DAILY.
46. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO SHOW, BY THE STANDARD TESTING PROCEDURES OF THE MATERIALS, THAT THE WORK CONSTRUCTED MEETS THE PROJECT REQUIREMENTS AND CITY SPECIFICATIONS.
47. ALL CONTRACTORS MUST CONFINE THEIR ACTIVITIES TO THE WORK AREA. NO ENCROACHMENTS OUTSIDE OF THE WORK AREA WILL BE ALLOWED. ANY DAMAGE RESULTING THEREFROM SHALL BE CONTRACTOR'S SOLE RESPONSIBILITY TO REPAIR.
48. THE CONTRACTOR SHALL PROTECT ALL EXISTING STRUCTURES, UTILITIES, MANHOLES, POLES, GUY WIRES, VALVE COVERS, VAULT LIDS, FIRE HYDRANTS, COMMUNICATION BOXES/PEDESTALS, SURVEY/MONUMENTS, AND OTHER FACILITIES TO REMAIN AND SHALL REPAIR ANY DAMAGES AT NO COST TO THE OWNER.
49. THE CONTRACTOR SHALL IMMEDIATELY REPAIR OR REPLACE ANY PHYSICAL DAMAGE TO PRIVATE PROPERTY OR PUBLIC IMPROVEMENTS, INCLUDING BUT NOT LIMITED TO: FENCES, WALLS, SIGNS, PAVEMENT, CURBS, UTILITIES, SIDEWALKS, GRASS, TREES, LANDSCAPING, AND IRRIGATION SYSTEMS, ETC... TO ORIGINAL CONDITION OR BETTER AT NO COST TO THE CITY.
50. THE CONTRACTOR WILL BE SOLELY AND COMPLETELY RESPONSIBLE FOR WORKING CONDITIONS ON THE JOB SITE WITHIN THE LIMITS OF WORK, INCLUDING SAFETY OF ALL PERSONS AND PROPERTY, DURING PERFORMANCE OF WORK. WORK SHALL NOT BEGIN UNTIL ADEQUATE TEMPORARY BARRICADES, BARRIERS, WARNING AND CONTROL DEVICES ARE IN PLACE AS REQUIRED BY THE CONTRACT. CONTRACTOR IS RESPONSIBLE TO CLEAN AND CLEAR AREA OF CONSTRUCTION EACH DAY AND REMOVE ANY HAZARD TO THE SAFETY OF RESIDENTS AND PUBLIC.
51. SIGNS RELATED TO SITE OPERATION OR SAFETY ARE NOT INCLUDED IN THESE PLANS.
52. CONTRACTOR STAGING AREA SHALL BE PROVIDED BY THE CONTRACTOR AND AGREED ON BY THE OWNER AND CONTRACTOR PRIOR TO BEGINNING OF CONSTRUCTION. CONTRACTOR IS RESPONSIBLE FOR ALL PERMITTING REQUIREMENTS FOR THE CONSTRUCTION STORAGE AND STAGING OPERATIONS AND LOCATIONS.
53. THE CONTRACTOR IS RESPONSIBLE FOR CONSTRUCTION SEQUENCING AND PHASING, AND SHALL CONTACT THE APPROPRIATE CITY OFFICIALS, INCLUDING BUILDING OFFICIAL, ENGINEERING INSPECTOR, AND FIRE MARSHAL TO LEARN OF ANY REQUIREMENTS.
54. THE CONTRACTOR SHALL BE RESPONSIBLE FOR PROVIDING AS-BUILT PLANS TO THE ENGINEER AND CITY IDENTIFYING ALL DEVIATIONS AND VARIATIONS FROM THESE PLANS MADE DURING CONSTRUCTION.

65% DESIGN PLANS
NOT FOR CONSTRUCTION
JANUARY 2026



Kimley»Horn

4637 Chabot Drive, Suite 200 Tel. No. (925) 398-4840
Pleasanton, California 94588

PROJECT No. CE 24-25-07		CITY OF SANTA CLARA PUBLIC WORKS DEPARTMENT		APPROVED BY	
ACCOUNT No.		UNCONTROLLED CROSSWALKS IMPROVEMENTS - PHASE II		TITLE	
DATE	No.	REVISION	BY	DATE	01/07/2026
				INITIAL	DATE
				JTK	01/26
				AMT	01/26
				SAR	01/26
				CHECKED BY	
				AS BUILT BY	
GENERAL NOTES				DWG No.	GN-01
				TRACING No.	12,444-D
				2	52

TRAFFIC CONTROL SPECIFICATION

- PUBLIC CONVENIENCE AND SAFETY SHALL BE ACCORDING TO SECTION 600 OF THE 2018 GREENBOOK STANDARD SPECIFICATIONS FOR PUBLIC WORKS CONSTRUCTION AND REGIONAL SUPPLEMENT AMENDMENTS.
- THE CONTRACTOR SHALL SUBMIT TRAFFIC CONTROL PLANS FOR WORK TO BE DONE TWO WEEKS BEFORE STARTING WORK, FOR THE CITY ENGINEER'S APPROVAL. THE CONTRACTOR SHALL COMPLY WITH THE TRAFFIC CONTROL PLANS. TRAFFIC CONTROL SHALL CONFORM TO THE CALIFORNIA MUTCD, LATEST EDITION.
- THE CONTRACTOR SHALL BE FULLY RESPONSIBLE FOR THE ADEQUACY OF ANY TRAFFIC PLAN USED. CONTRACTOR SHALL NOTIFY THE CITY AT LEAST TWO (2) WORKING DAYS BEFORE STARTING ANY CONSTRUCTION DETOUR.
- A SEPARATE TRAFFIC CONTROL PLAN FOR ANY WORK ON ARTERIAL AND COLLECTOR STREETS SHALL BE SUBMITTED FOR APPROVAL PRIOR TO THE START OF ANY WORK ON THESE STREETS. THE CONTRACTOR SHALL ALSO SUBMIT SEPARATE TRAFFIC CONTROL PLANS FOR WORK WITHIN ANY MAJOR INTERSECTIONS, STATE RIGHTS-OF-WAY, AREAS WHERE SCHOOLS ARE PRESENT AND OTHER CONGESTED AREAS. THE TRAFFIC CONTROL PLAN SHALL INCLUDE PHASING, EXISTING STRIPING, AND TEMPORARY STRIPING DURING CONSTRUCTION, AND FINISHED PERMANENT STRIPING.
- ALL TRAFFIC CONTROL PLANS SHALL BE PREPARED AND SIGNED BY A LICENSED CSL CONTRACTOR OR QUALIFIED CONTRACTOR WITH A GENERAL "A" LICENSE. THE CONTRACTOR SHALL EMPLOY FULL TIME DEDICATED PERSONNEL WHOSE SOLE RESPONSIBILITY IS TO DO TRAFFIC CONTROL ONLY. THE CONTRACTOR IS PROHIBITED FROM USING OTHER CREW PERSONNEL TO PERFORM THIS JOB.
- IT IS THE RESPONSIBILITY OF THE CONTRACTOR PERFORMING WORK ON A CITY STREET TO INSTALL AND MAINTAIN THE APPROVED TRAFFIC CONTROL DEVICES AND SUCH ADDITIONAL TRAFFIC CONTROL DEVICES (E.G. FLAG PERSONS) AS MAY BE REQUIRED TO INSURE SAFE MOVEMENT OF TRAFFIC MOTORISTS, BICYCLISTS, PEDESTRIAN AND CONSTRUCTION STAKING STAFF THROUGH AND AROUND THE WORK AREA AND PROVIDE MAXIMUM PROTECTION AND SAFETY TO CONSTRUCTION WORKERS.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR ADEQUATE BARRICADING OF THE WORK AREA AND CONTROLLING OF TRAFFIC NEAR THE PROJECT AS SPECIFIED IN SUB SECTION 600 OF THE 2018 GREENBOOK STANDARD SPECIFICATIONS OR AS DIRECTED BY THE ENGINEER.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR PRESERVING ORIGINAL LOCATIONS AND DIMENSIONS OF ALL EXISTING STRIPING OBLITERATED BY THE WORK. THE CONTRACTOR SHALL BE RESPONSIBLE FOR MAINTAINING SUFFICIENT STRIPING CONTROL POINTS TO BE ABLE TO RESTORE LANE DIMENSIONS. THE CONTRACTOR SHALL INSTALL AND MAINTAIN TEMPORARY OVERLAY YELLOW OR WHITE MARKERS, WHICHEVER IS APPLICABLE, WHERE STRIPING CANNOT BE RESTORED BY THE END OF THE WORKDAY. THE CONTRACTOR SHALL REPLACE ALL ROADWAY STRIPING AND PAVEMENT MARKINGS REMOVED BY THE CONSTRUCTION WORK.
- AT LEAST FIVE (5) WORKING DAYS BEFORE COMMENCING WORK, THE CONTRACTOR SHALL SUBMIT THEIR CONSTRUCTION SCHEDULE TO THE CITY FOR APPROVAL. THIS SCHEDULE SHALL ALLOW RESIDENTS OR BUSINESSES ALONG THE STREETS WHERE THERE IS CONSTRUCTION ACTIVITY TO HAVE AMPLE "ON STREET" PARKING (IF PARKING IS ALLOWED) WITHIN A REASONABLE DISTANCE FROM THEIR HOMES OR PLACE OF BUSINESS. BASED UPON THE CONSTRUCTION SCHEDULE, THE CONTRACTOR SHALL NOTIFY RESIDENTS AND BUSINESSES OF THE PROPOSED WORK, AND POST TEMPORARY "NO PARKING" SIGNS AT LEAST 72 HOURS IN ADVANCE OF IMPLEMENTING ANY PARKING RESTRICTIONS. NOTIFICATION SHALL BE IN THE FORM OF "DOOR KNOCKER" NOTICES AND MUST BE IN ENGLISH AND SPANISH. THE CONTRACTOR SHALL SUBMIT A SAMPLE OF THE "NO PARKING" SIGN AND "DOOR KNOCKER" NOTICES TO THE CITY FOR APPROVAL.
- ALL "NO PARKING SIGNS" SHALL BE MOUNTED ON "A-FRAMES" ONLY. SIGNS TAPED ON TRAFFIC CONES, TREES OR OTHER STRUCTURES SHALL NOT BE PERMITTED.
- THE CITY RESERVES THE RIGHT TO OBSERVE THE TRAFFIC CONTROL PLANS IN USE AND TO MAKE CHANGES AS FIELD CONDITIONS WARRANT. ANY CHANGES WILL SUPERSEDE THE PLANS AND WILL BE DONE SOLELY AT THE CONTRACTOR'S EXPENSE.
- THE CONTRACTOR SHALL ARRANGE WITH RECOLOGY, SANTA CLARA COUNTY TO MAINTAIN TRASH PICK-UP SERVICES FOR THOSE PROPERTY OWNERS AFFECTED BY THE WORK. THE CONTRACTOR SHALL NOTIFY THE TRANSIT AGENCIES OF ANY DELAYS TO THEIR BUSES. THE CONTRACTOR SHALL NOTIFY ALL SCHOOLS (PUBLIC AND/OR PRIVATE) WHERE THE WORK MAY AFFECT SCHOOL OPERATIONS.
- CONTRACTOR IS REQUIRED TO COMPLETE AND SUBMIT THE TRAFFIC CONTROL ESTIMATE FORM. PAYMENT WILL BE ADJUSTED BASED ON THE STREETS COMPLETED FROM THIS LIST.
- FULL COMPENSATION FOR FURNISHING ALL LABOR, MATERIALS, TOOLS, EQUIPMENT, AND INCIDENTALS, AND ALL NECESSARY TRAFFIC CONTROL AS REQUIRED BY THE CITY IS CONSIDERED INCLUDED IN THE CONTRACT PRICE PAID FOR "PUBLIC CONVENIENCE AND SAFETY (TRAFFIC CONTROL)" AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.

ARBORIST NOTES

- ATTENTION DIRECTED TO SECTION 2020 LANDSCAPING AND IRRIGATION, SUBSECTION 6.7, PROTECTION OF EXISTING TREES, OF THE CITY STANDARD.
- NO CUTTING OF ANY PART OF CITY TREES, INCLUDING ROOTS, SHALL BE DONE WITHOUT SECURING APPROVAL AND DIRECT SUPERVISION FROM THE ARBORIST EMPLOYED BY THE CITY OF SANTA CLARA.
 - NO CUTTING OF ANY PART OF PRIVATE TREES, INCLUDING ROOTS, SHALL BE DONE WITHOUT DIRECT SUPERVISION OF A CERTIFIED ARBORIST (CERTIFICATION OF INTERNATIONAL SOCIETY OF ARBORICULTURE).
 - WHEN CONSTRUCTION OCCURS WITHIN DRIP LINE OF EXISTING TREES, CONTRACTOR IS TO PILE THE SOIL ON THE SIDE AWAY FROM THE TREE. WHEN THIS IS NOT POSSIBLE, PLACE SOIL ON PLYWOOD, A TARP, OR THICK BED OF MULCH. THIS IS TO HELP PREVENT CUTTING INTO THE SOIL SURFACE WHEN THE BACKHOE OR TRACTOR BLADE REFILLS THE TRENCH.
 - REFILL OPEN TRENCHES IMMEDIATELY WITHIN HOURS OF EXCAVATION IF THEY OCCUR WITHIN THE DRIP LINE OF EXISTING TREES. IF THIS IS NOT POSSIBLE AND WEATHER IS HOT, DRY, OR WINDY, CONTRACTOR MUST KEEP ROOT ENDS MOIST BY COVERING THEM WITH WET BURLAP. IF TEMPERATURE IS 80°F OR GREATER, THE BURLAP MUST BE INSPECTED EVERY HOUR AND RE-WET AS NECESSARY TO MAINTAIN A CONSISTENT COOL MOIST CONDITION. IF TEMPERATURE IS BELOW 80°F, THE BURLAP MUST BE INSPECTED EVERY FOUR HOURS AND RE-WET AS NECESSARY TO MAINTAIN A CONSTANT COOL MOIST CONDITION. SMALL ROOTS CAN DRY OUT AND DIE IN 10-15 MINUTES. LARGER ROOTS CAN SUCCEUMB IN AN HOUR OR LESS UNDER UNFAVORABLE WEATHER CONDITIONS.
 - WHEN ROOTS 2" OR LARGER MUST BE CUT, SHOVEL BY HAND NEAR THE ROOTS AND SAW THE ROOTS. ACCIDENTALLY BROKEN ROOTS SHOULD BE SAVED A COUPLE OF INCHES BEHIND THE RAGGED END, CRUSHED OR TORN ROOTS ARE MORE LIKELY TO ALLOW DECAY TO BEGIN; SHARPLY CUT ROOTS PRODUCE A FLUSH OF NEW ROOTS HELPING THE TREE TO RECOVER FROM ITS INJURY.
 - CONTRACTOR SHALL NOTIFY CITY ARBORIST 72 HOURS IN ADVANCE OF ANY WORK REQUIRING DIGGING AROUND OR WITHIN THE DRIP LINE OF THE EXISTING TREES.
 - A CLEAR SYSTEM OF FLAGGING MUST BE PROVIDED AROUND THE TREES WITHIN 20' OF PROPOSED GRADING AS REQUIRED BY CITY ARBORISTS.
 - MATERIALS, EQUIPMENT, TEMPORARY BUILDING, FUELS, PAINTS AND OTHER CONSTRUCTION ITEMS ARE NOT TO BE PLACED WITHIN THE DRIP LINE OF EXISTING TREES.
 - FENCING SHALL BE PLACED AT THE DRIP LINE OF THE EXISTING TREES, OR IF POSSIBLE, 1.5 TIMES THE RADIUS OF THE DRIP LINE OUT FROM THE TRUNK OF THE TREE. IN ADDITION, WRAP ALL TREES WITH STRAW WADDLE UP TO FIRST BRANCHES THEN WRAP SNOW FENCING AROUND THE WADDLE ON ALL TREES IN THE CONSTRUCTION ZONE TO PROTECT THEM FROM BARK DAMAGE CAUSED BY CONSTRUCTION EQUIPMENT.
 - NO TRENCHING SHALL BE DONE WITHIN THE DRIP LINE OF EXISTING TREES WITHOUT THE APPROVAL OF THE CITY ARBORIST. OPEN TRENCHING IN THE ROOT ZONE OF A PUBLIC TREE IS PROHIBITED EXCEPT IN CASES WHERE THE TRENCHING FALLS OUTSIDE THE DRIP LINE OF THE TREE INVOLVED. EXCEPTIONS WILL BE ALLOWED IF, IN THE OPINION OF THE CITY ARBORIST, THE IMPACT OF TRENCHING UPON THE TREE WILL BE NEGLIGIBLE.
 - ANY CUTTING OF EXISTING ROOTS SHALL BE DONE WITH LIGHT, APPROVED EQUIPMENT UNDER THE DIRECT SUPERVISION OF THE CITY ARBORIST. ORIGINAL GRADE AT BASE OF EXISTING TREES IS NOT TO CHANGE. IF NECESSARY, DRY WELLS ARE TO BE USED.
 - GRADING SHOULD NOT CREATE DRAINAGE PROBLEMS FOR TREES BY CHANNELING WATER INTO THEM, OR CREATING SUNKEN AREAS.
 - ALL GRADING WITHIN DRIP LINE OF TREES SHALL BE DONE WITH LIGHT, APPROVED EQUIPMENT UNDER THE DIRECT SUPERVISION OF THE CITY ARBORIST. ORIGINAL GRADE AT BASE OF EXISTING TREES IS NOT TO CHANGE. IF NECESSARY, DRY WELLS ARE TO BE USED IF GRADE IS TO RISE.
 - WHEN TRENCHING IS ALLOWED, THE CONTRACTOR MUST FIRST CUT ROOTS WITH A VERMEER ROOT CUTTER PRIOR TO ANY TRENCHING TO AVOID TUGGING OR PULLING OF ROOTS.
 - TREES THAT ARE DETERMINED TO BE REMOVED BY THE CITY ARBORIST DUE TO CONSTRUCTION SHALL BE REPLACED BY THE CONTRACTOR. CITY ARBORIST TO DETERMINE SPECIES, SIZE AND SPACING.
 - PLACE 1"-2" OF MULCH AROUND ALL EXISTING TREES (OUT TO THEIR DRIP LINE) THAT ARE TO BE PRESERVED PRIOR TO ANY CONSTRUCTION. THIS WILL HELP MAINTAIN MOISTURE UNDER THE TREE WITHIN THE FENCING AREA.
 - NO BORE PITS ALLOWED WITHIN THE DRIP LINE OF ANY TREE.
 - THE CONTRACTOR SHALL NOT INSTALL ANY PROJECT ELEMENTS (EQUIPMENT, INFRASTRUCTURE) WITHIN 6 FEET OF ANY EXISTING TREES.

65% DESIGN PLANS
NOT FOR CONSTRUCTION
JANUARY 2026

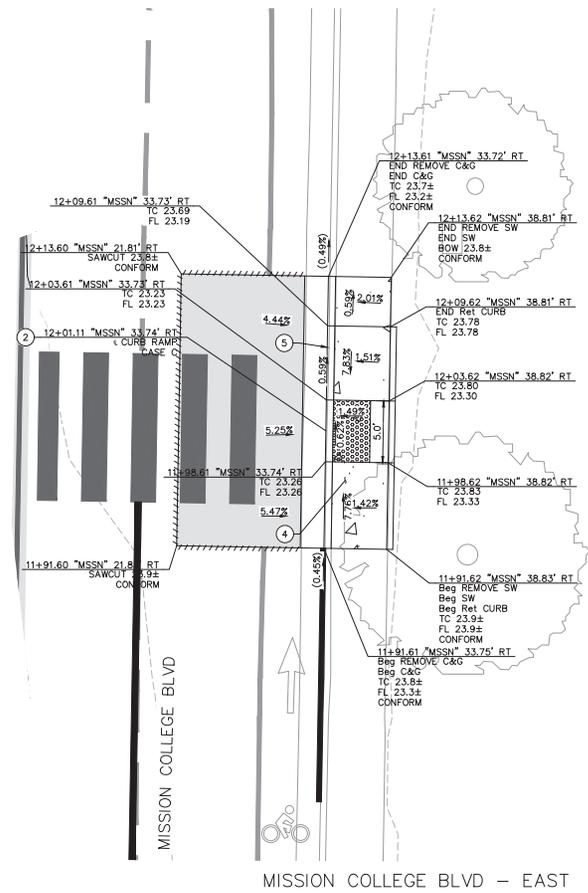
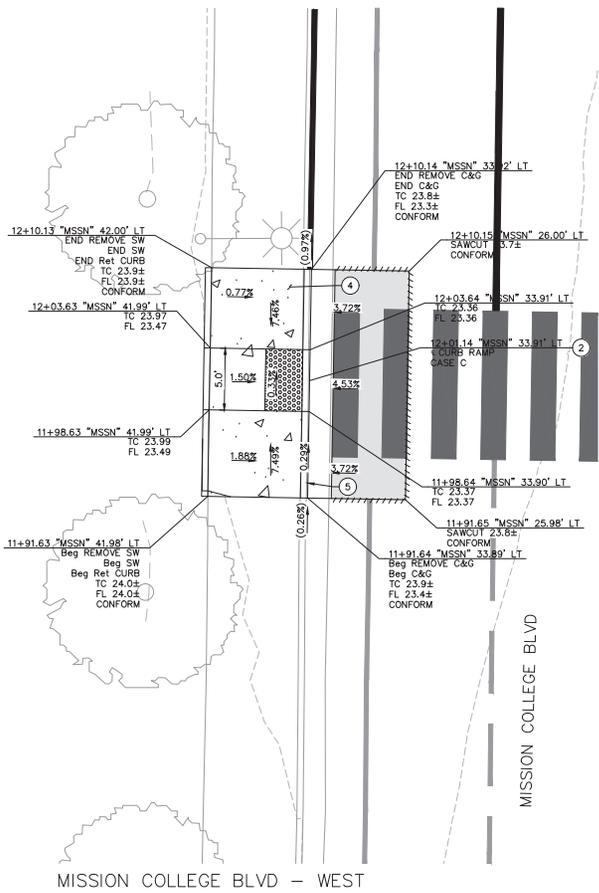


Kimley»Horn

4637 Chabot Drive, Suite 200 Tel. No. (925) 398-4840
Pleasanton, California 94588

PROJECT No. CE 24-25-07			
ACCOUNT No.			
DATE	No.	REVISION	BY

CITY OF SANTA CLARA			
PUBLIC WORKS DEPARTMENT			
UNCONTROLLED CROSSWALKS IMPROVEMENTS - PHASE II			
GENERAL NOTES			
APPROVED BY			
TITLE			
DATE	INITIAL	DATE	DWG No.
01/07/2026			GN-02
DESIGNED BY	JTK	01/26	TRACING No. 12,444-D
DRAWN BY	AMT	01/26	
CHECKED BY	SAR	01/26	
AS BUILT BY			



MISSION COLLEGE BLVD - WEST

MISSION COLLEGE BLVD - EAST

65% DESIGN PLANS
NOT FOR CONSTRUCTION
JANUARY 2026

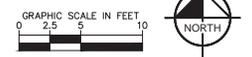


CONSTRUCTION NOTES

- ② INSTALL CALTRANS CASE C CURB RAMP PER CALTRANS STANDARD DETAIL A58A.
- ④ INSTALL SIDEWALK PER CITY OF SANTA CLARA STANDARD DETAIL ST-12.
- ⑤ INSTALL MONOLITHIC CURB AND GUTTER PER CITY OF SANTA CLARA STANDARD DETAILS ST-11 AND ST-12.

LEGEND

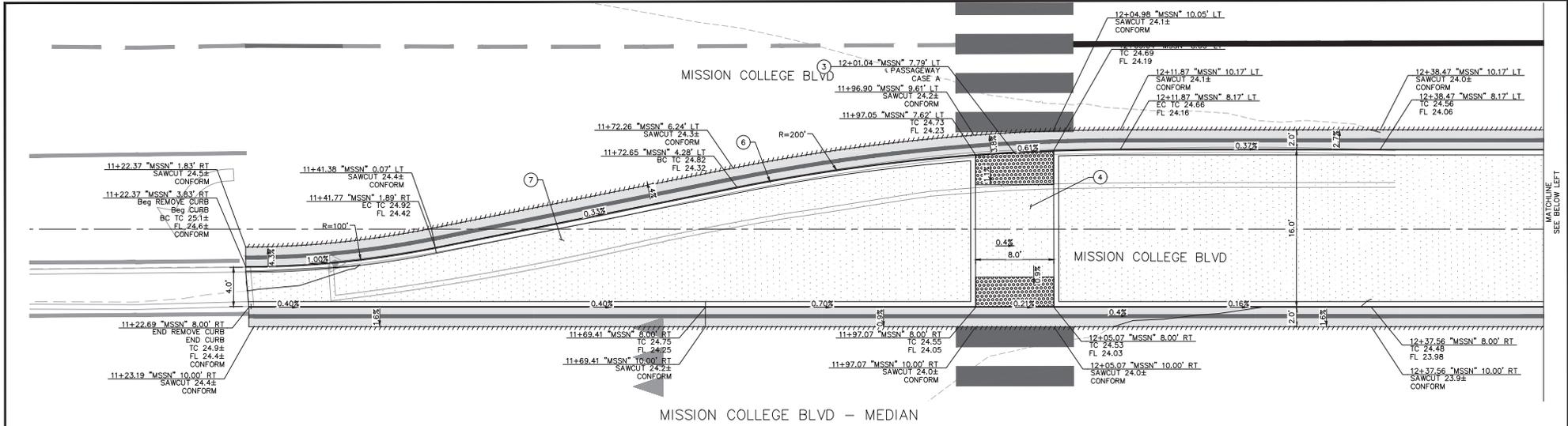
- DETECTABLE WARNING SURFACE
- CONCRETE SIDEWALK
- MEDIAN PAVING
- 6" HMA (TYPE A) DEEP LIFT
- SAWCUT LIMITS



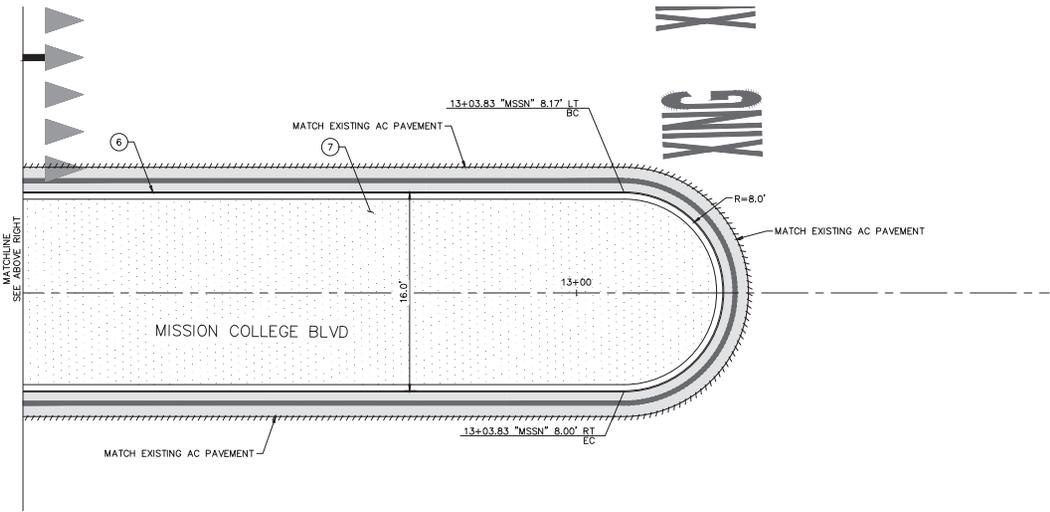
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Pleasanton, California 94588

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DRAWN BY	AMT	DATE	01/26
CHECKED BY	SAR	DATE	01/26
AS BUILT BY			
		DWG No.	12,444-D
		TRACING No.	



MISSION COLLEGE BLVD - MEDIAN



MISSION COLLEGE BLVD - MEDIAN

CONSTRUCTION NOTES

- 3 INSTALL CALTRANS TYPE A PASSAGEWAY PER CALTRANS STANDARD DETAIL A88B.
- 4 INSTALL SIDEWALK PER CITY OF SANTA CLARA STANDARD DETAIL ST-12.
- 6 INSTALL TYPE B-6 CONCRETE MEDIAN CURB PER CITY OF SANTA CLARA STANDARD DETAIL ST-13.
- 7 INSTALL MEDIAN COBBLESTONE PER CITY OF SANTA CLARA STANDARD DETAIL ST-13A.

LEGEND

- DETECTABLE WARNING SURFACE
- CONCRETE SIDEWALK
- MEDIAN PAVING
- 6" HMA (TYPE A) DEEP LIFT
- SAWCUT LIMITS

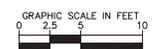
65% DESIGN PLANS
NOT FOR CONSTRUCTION
JANUARY 2026



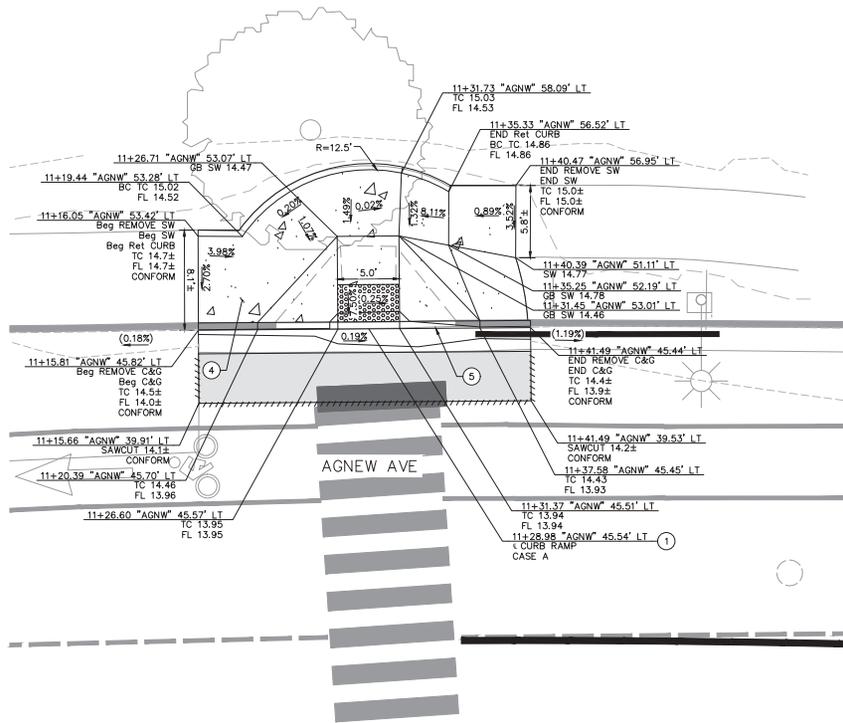
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Pleasanton, California 94588

PROJECT No. CE 24-25-07			
ACCOUNT No.			
DATE	No.	REVISION	BY

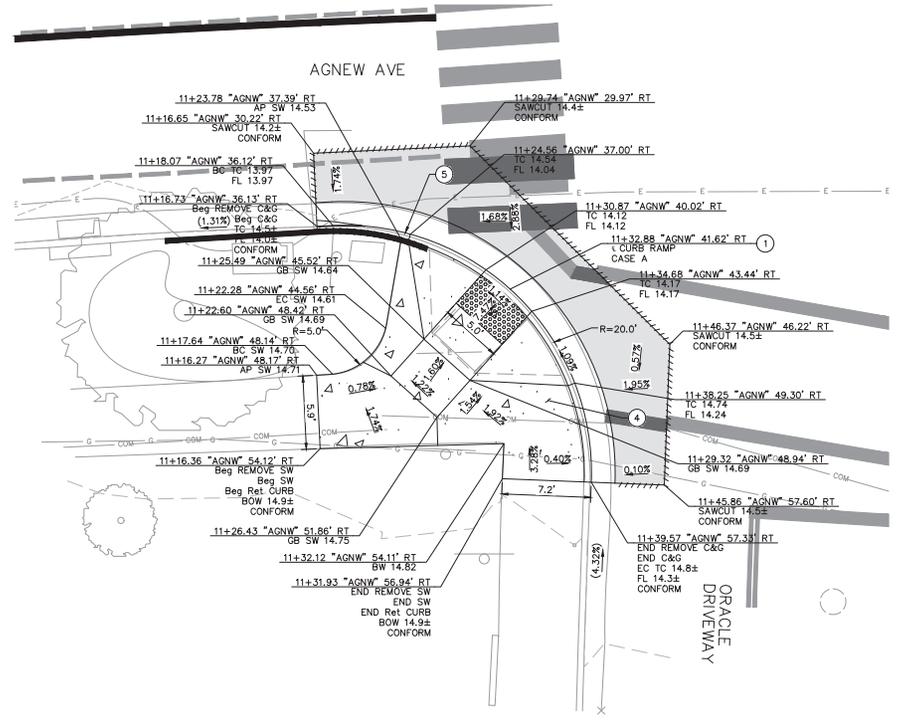
CITY OF SANTA CLARA PUBLIC WORKS DEPARTMENT		APPROVED BY	
		TITLE	
UNCONTROLLED CROSSWALKS IMPROVEMENTS - PHASE II		DATE 01/07/2026	DWG No.
DESIGNED BY	JTK	DATE	01/26
DRAWN BY	AMT	DATE	01/26
CHECKED BY	SAR	DATE	01/26
AS BUILT BY			
		TRACING No. 12,444-D	



MATCHLINE
SEE BELOW LEFT



AGNEW RD & GARRITY WAY
NW CORNER



AGNEW RD & GARRITY WAY
SW CORNER

65% DESIGN PLANS
NOT FOR CONSTRUCTION
JANUARY 2026



CONSTRUCTION NOTES

- 1 INSTALL CALTRANS CASE A CURB RAMP PER CALTRANS STANDARD DETAIL AS8A.
- 4 INSTALL SIDEWALK PER CITY OF SANTA CLARA STANDARD DETAIL ST-12.
- 5 INSTALL MONOLITHIC CURB AND GUTTER PER CITY OF SANTA CLARA STANDARD DETAILS ST-11 AND ST-12.

LEGEND

- DETECTABLE WARNING SURFACE
- CONCRETE SIDEWALK
- MEDIAN PAVING
- 6" HMA (TYPE A) DEEP LIFT
- SAWCUT LIMITS



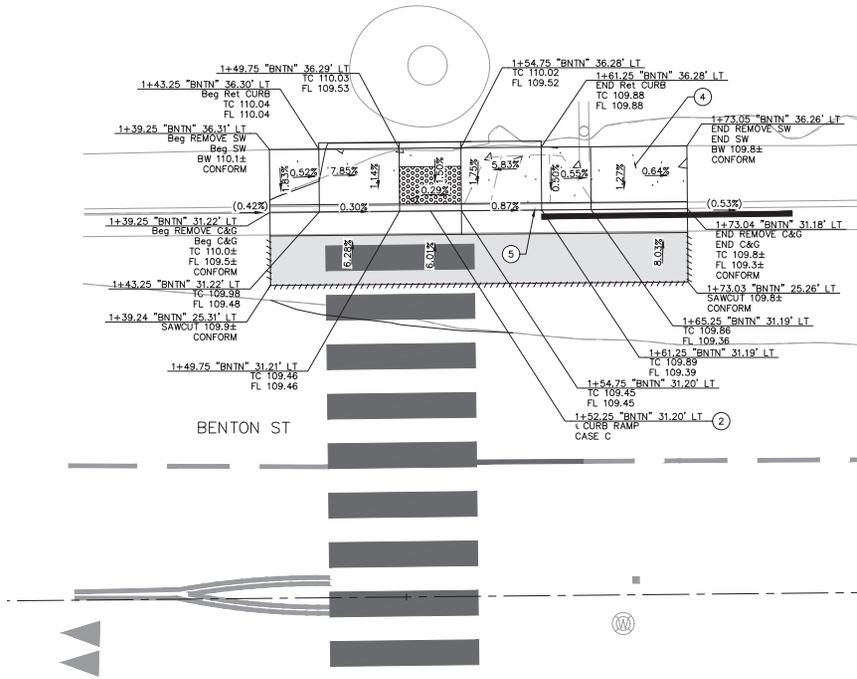
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Pleasanton, California 94588

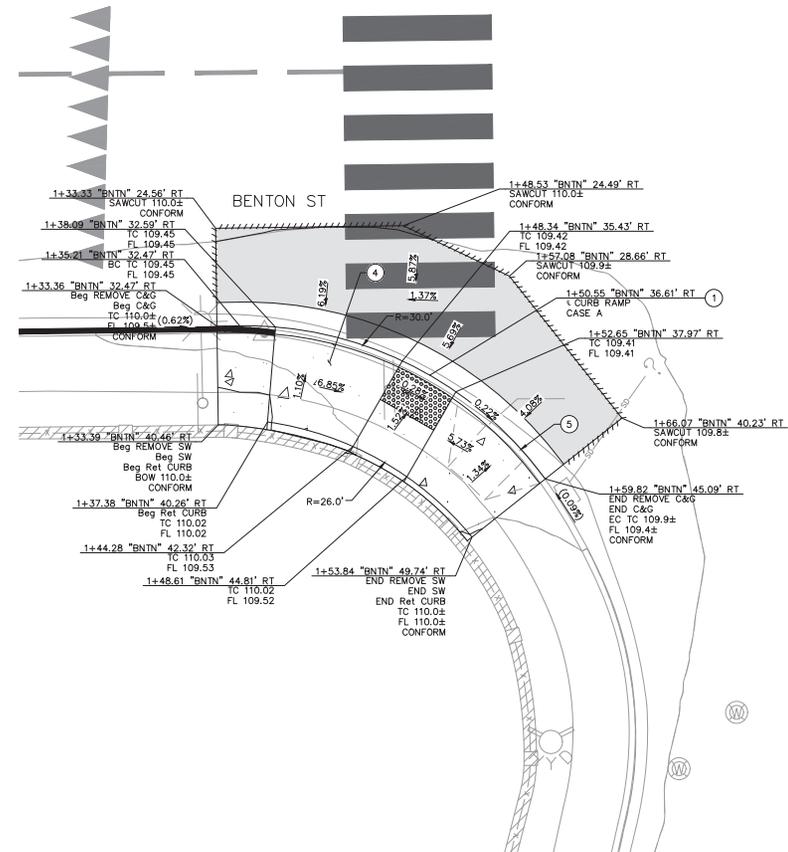
PROJECT No. CE 24-25-07			
ACCOUNT No.			
DATE	No.	REVISION	BY

CITY OF SANTA CLARA PUBLIC WORKS DEPARTMENT			
UNCONTROLLED CROSSWALKS IMPROVEMENTS - PHASE II			
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APPROVED BY			
TITLE			
DATE	01/07/2026	DWG No.	
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AS BUILT BY			
TRACING No.			12,444-D
SHEET			7 OF 52



BENTON ST & CURTIS AVE
N CURB RAMP



BENTON ST & CURTIS AVE
SW CORNER

65% DESIGN PLANS
NOT FOR CONSTRUCTION
JANUARY 2026

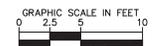


CONSTRUCTION NOTES

- 1 INSTALL CALTRANS CASE A CURB RAMP PER CALTRANS STANDARD DETAIL AB8A.
- 2 INSTALL CALTRANS CASE C CURB RAMP PER CALTRANS STANDARD DETAIL AB8A.
- 4 INSTALL SIDEWALK PER CITY OF SANTA CLARA STANDARD DETAIL ST-12.
- 5 INSTALL MONOLITHIC CURB AND GUTTER PER CITY OF SANTA CLARA STANDARD DETAILS ST-11 AND ST-12.

LEGEND

- DETECTABLE WARNING SURFACE
- CONCRETE SIDEWALK
- MEDIAN PAVING
- 6" HMA (TYPE A) DEEP LIFT
- SAWCUT LIMITS

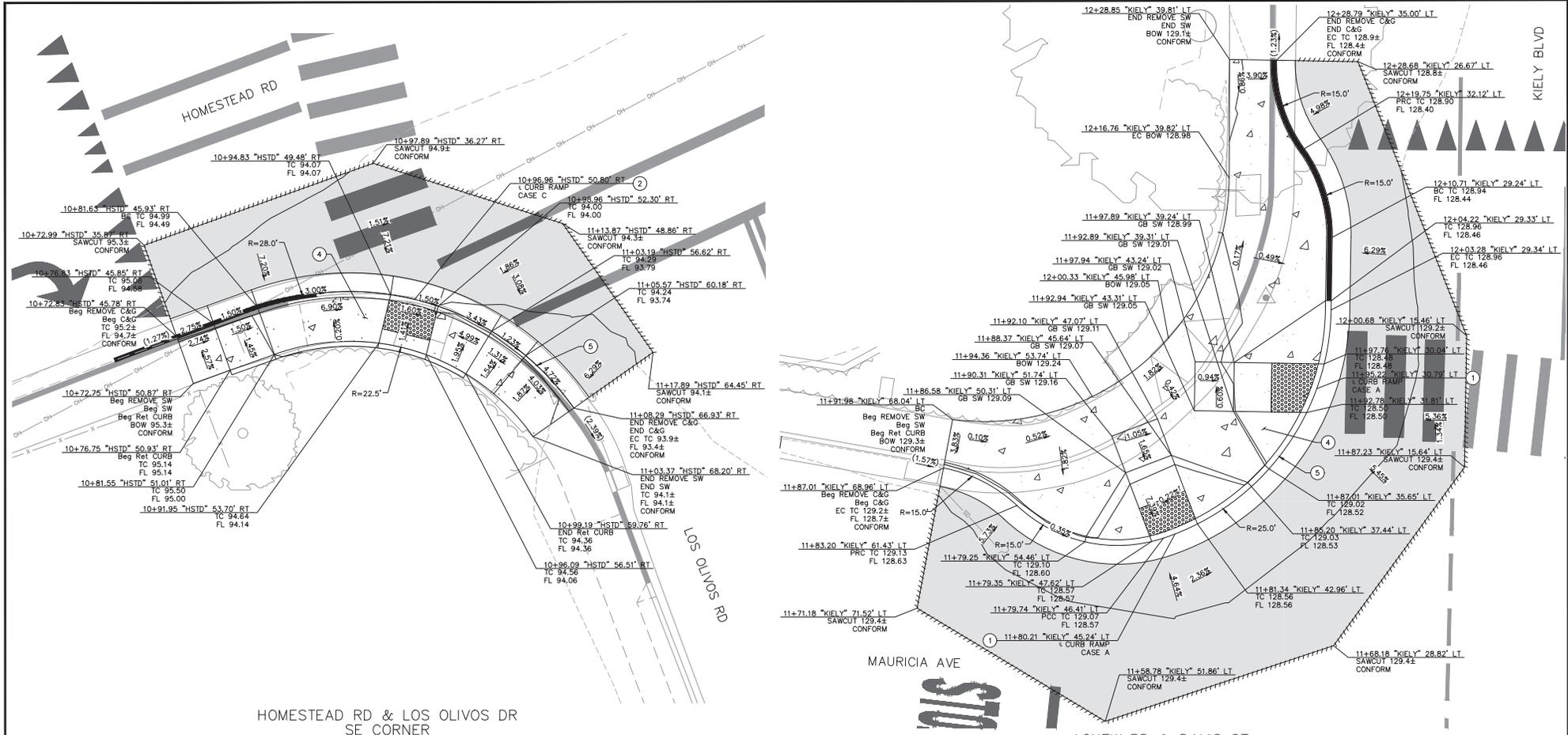


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Pleasanton, California 94588

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DESIGNED BY	JTK	01/26	DWG No. 12,444-D
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CHECKED BY	SAR	01/26	
AS BUILT BY			



HOMESTEAD RD & LOS OLIVOS DR SE CORNER

AGNEW RD & DAVIS ST SE CORNER

65% DESIGN PLANS
NOT FOR CONSTRUCTION
JANUARY 2026



CONSTRUCTION NOTES

- 1 INSTALL CALTRANS CASE A CURB RAMP PER CALTRANS STANDARD DETAIL AB8A.
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- 4 INSTALL SIDEWALK PER CITY OF SANTA CLARA STANDARD DETAIL ST-12.
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LEGEND

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- CONCRETE SIDEWALK
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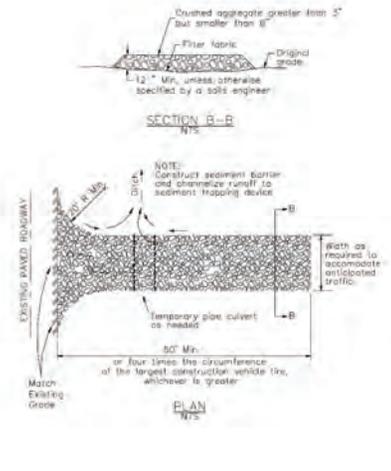
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CHECKED BY	SAR	DATE	01/26
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DWG No.		TRACING No.	
12,444-D		9 of 52	



3

Stabilized Construction Entrance/Exit

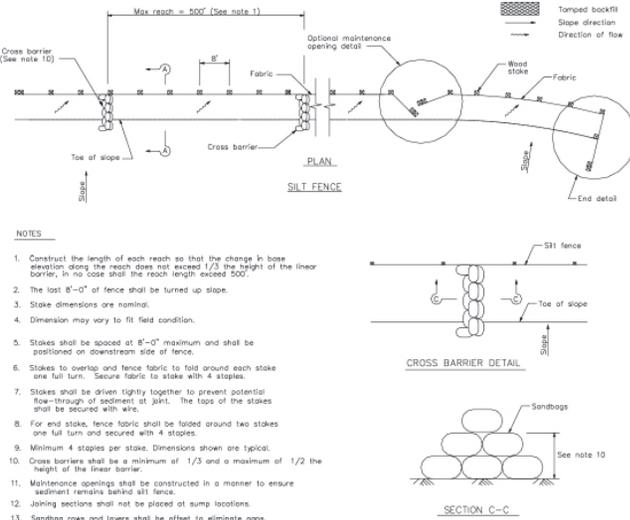
CASQA Detail TC-1



1

Silt Fence

CASQA Detail SE-1

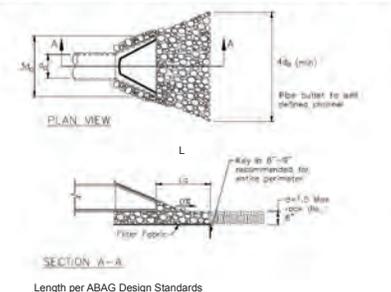


- NOTES**
1. Construct the length of each reach so that the change in base elevation along the reach does not exceed 1/3 the height of the linear barrier. In no case shall the reach length exceed 500'.
 2. The last 8'-0" of fence shall be turned up slope.
 3. Stake dimensions are nominal.
 4. Dimension may vary to fit field condition.
 5. Stakes shall be spaced at 8'-0" maximum and shall be positioned on downstream side of fence.
 6. Stakes to overlap and fence fabric to fold around each stake one full turn. Secure fabric to stake with 4 staples.
 7. Stakes shall be driven tightly together to prevent potential flow-through of sediment at joint. The tops of the stakes shall be secured with wire.
 8. For end stake, fence fabric shall be folded around two stakes one full turn and secured with 4 staples.
 9. Minimum 4 staples per stake. Dimensions shown are typical.
 10. Cross barriers shall be a minimum of 1/3 and a maximum of 1/2 the height of the linear barrier.
 11. Maintenance openings shall be constructed in a manner to ensure sediment remains behind silt fence.
 12. Joining sections shall not be placed at sump locations.
 13. Sandbag rows and layers shall be offset to eliminate gaps.

4

Velocity Dissipation Devices

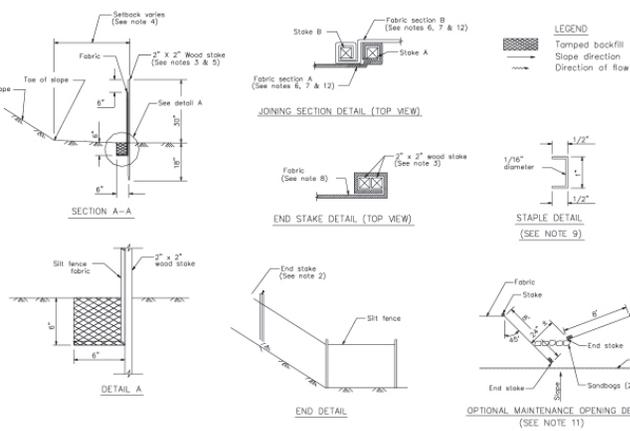
CASQA Detail EC-10



2

Silt Fence

CASQA Detail SE-1



STANDARD BEST MANAGEMENT PRACTICE NOTES

1. **Solid and Demolition Waste Management:** Provide designated waste collection areas and containers on site away from streets, gutters, storm drains, and waterways, and arrange for regular disposal. Waste containers must be watertight and covered at all times except when waste is deposited. Refer to Erosion Sediment Control Field Manual, 4th Edition (page C3) or latest.
2. **Hazardous Waste Management:** Provide proper handling and disposal of hazardous wastes by a licensed hazardous waste material hauler. Hazardous wastes shall be stored and properly labeled in sealed containers constructed of suitable materials. Refer to Erosion & Sediment Control Field Manual, 4th Edition (pages C-5 to C-6) or latest.
3. **Spill Prevention and Control:** Provide proper storage areas for liquid and solid materials, including chemicals and hazardous substances. Spill control materials must be kept on site where readily accessible. Spills must be cleaned up immediately and contaminated soil disposed properly. Refer to Erosion & Sediment Control Field Manual, 4th Edition (pages C-7 to C-8, C-13 to C-14) or latest.
4. **Vehicle and Construction Equipment Service and Storage:** An area shall be designated for the maintenance, where on-site maintenance is required, and storage of equipment that is protected from stormwater run-on and runoff. Measures shall be provided to capture any waste oils, lubricants, or other potential pollutants and these wastes shall be properly disposed of off site. Fueling and major maintenance/repair, and washing shall be conducted off-site whenever feasible. Refer to Erosion Sediment Control Field Manual, 4th Edition (page C9) or latest.
5. **Material Delivery, Handling and Storage:** In general, materials should not be stockpiled on site. Where temporary stockpiles are necessary and approved by the County, they shall be covered with secured plastic sheeting or tarp and located in designated areas near construction entrances and away from drainage paths and waterways. Barriers shall be provided around storage areas where materials are potentially in contact with runoff. Refer to Erosion & Sediment Control Field Manual, 4th Edition (pages C-11 to C-12) or latest.
6. **Handling and Disposal of Concrete and Cement:** When concrete trucks and equipment are washed on-site, concrete wastewater shall be contained in designated containers or in a temporary lined and watertight pit where wasted concrete can harden for later removal. If possible have concrete contractor remove concrete wash water from site. In no case shall fresh concrete be washed into the road right-of-way. Refer to Erosion Sediment Control Field Manual, 4th Edition (pages C-15 to C-16) or latest.
7. **Pavement Construction Management:** Prevent or reduce the discharge of pollutants from paving operations, using measures to prevent run-on and runoff pollution and properly disposing of wastes. Avoid paving in the wet season and reschedule paving when rain is in the forecast. Residue from saw-cutting shall be vacuumed for proper disposal. Refer to Erosion & Sediment Control Field Manual, 4th Edition (pages C-17 to C-18) or latest.
8. **Contaminated Soil and Water Management:** Inspections to identify contaminated soils should occur prior to construction and at regular intervals during construction. Remediating contaminated soil should occur promptly after identification and be specific to the contaminant identified, which may include hazardous waste removal. Refer to Erosion & Sediment Control Field Manual, 4th Edition (pages C-19 to C-20) or latest.
9. **Sanitary/Septic Water Management:** Temporary sanitary facilities should be located away from drainage paths, waterways, and traffic areas. Only licensed sanitary and septic waste haulers should be used. Secondary containment should be provided for all sanitary facilities. Refer to Erosion & Sediment Control Field Manual, 4th Edition (page C-21) or latest.
10. **Inspection & Maintenance:** Areas of material and equipment storage sites and temporary sanitary facilities must be inspected weekly. Problem areas shall be identified and appropriate additional and/or alternative control measures implemented immediately, within 24 hours of the problem being identified.

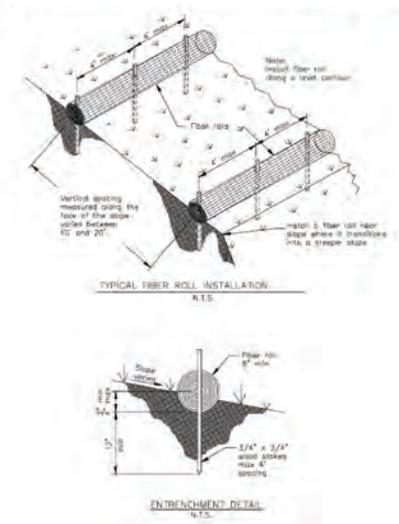
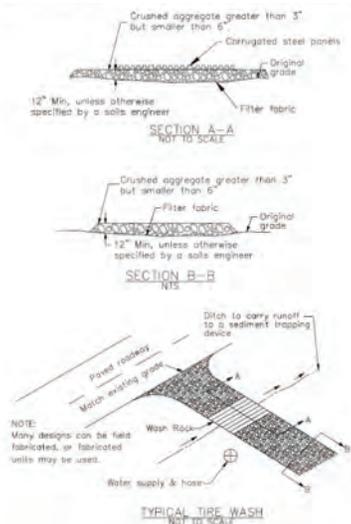
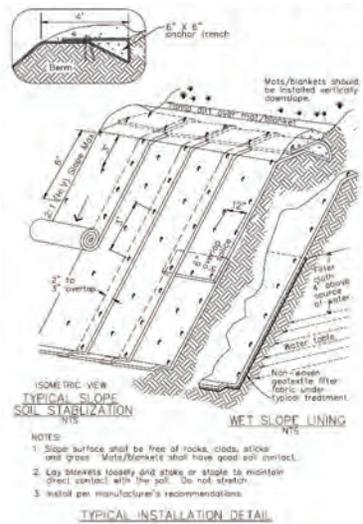
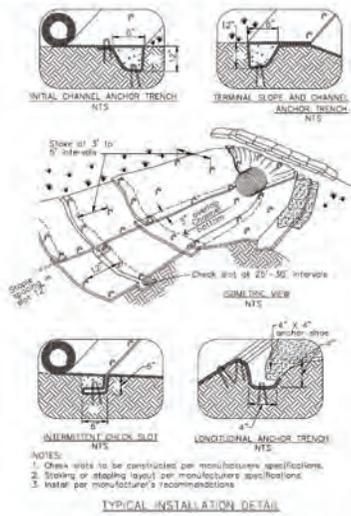
STANDARD EROSION CONTROL NOTES

1. **Sediment Control Management:**
 - Tracking Prevention & Clean Up:** Activities shall be organized and measures taken as needed to prevent or minimize tracking of soil onto the public street system. A gravel or proprietary device construction entrance/exit is required for all sites. Clean up of tracked material shall be provided by means of a street sweeper prior to an approaching rain event, or at least once at the end of each workday that material is tracked, or, more frequently as determined by the County Inspector. Refer to Erosion & Sediment Control Field Manual, 4th Edition (pages B-31 to B-33) or latest.
 - Storm Drain Inlet and Catch Basin Inlet Protection:** All inlets within the vicinity of the project and within the project limits shall be protected with gravel bags placed around inlets or other inlet protection. At locations where exposed soils are present, staked fiber roles or staked silt fences can be used. Inlet filters are not allowed due to clogging and subsequent flooding. Refer to Erosion Sediment Control Field Manual, 4th Edition (pages B-49 to B-51) or latest.
 - Storm Water Runoff:** No storm water runoff shall be allowed to drain in to the existing and/or proposed underground storm drain system or other above ground watercourses until appropriate erosion control measures are fully installed.
 - Dust Control:** The contractor shall provide dust control in graded areas as required by providing wet suppression or chemical stabilization of exposed soils, providing for rapid clean up of sediments deposited on paved roads, furnishing construction area entrances and vehicle wash down areas, and limiting the amount of areas disturbed by clearing and earth moving operations by scheduling these activities in phases.
 - Stockpiling:** Excavated soils shall not be placed in streets or on paved areas. Borrow and temporary stockpiles shall be protected with appropriate erosion control measures (tarps, straw bales, silt fences, ect.) to ensure silt does not leave the site or enter the storm drain system or neighboring watercourses.
2. **Erosion Control:** During the rainy season, all disturbed areas must include an effective combination of erosion and sediment control. It is required that temporary erosion control measures are applied to all disturbed soil areas prior to a rain event. During the non-rainy season, erosion control measures must be applied sufficient to control wind erosion at the site.
3. **Inspection & Maintenance:** Disturbed areas of the Project's site, locations where vehicles enter or exit the site, and all erosion and sediment controls that are identified as part of the Erosion Control Plans must be inspected by the Contractor before, during, and after storm events, and at least weekly during seasonal wet periods. Problem areas shall be identified and appropriate additional and/or alternative control measures implemented immediately, within 24 hours of the problem being identified.
4. **Project Completion:** Prior to project completion and signoff by the County Inspector, all disturbed areas shall be reseeded, planted, or landscaped to minimize the potential for erosion on the subject site.
5. It shall be the Owner's/Contractor's responsibility to maintain control of the entire construction operation and to keep the entire site in compliance with the erosion control plan.
6. Erosion and sediment control best management practices shall be operable year round or until vegetation is fully established on landscaped surfaces.

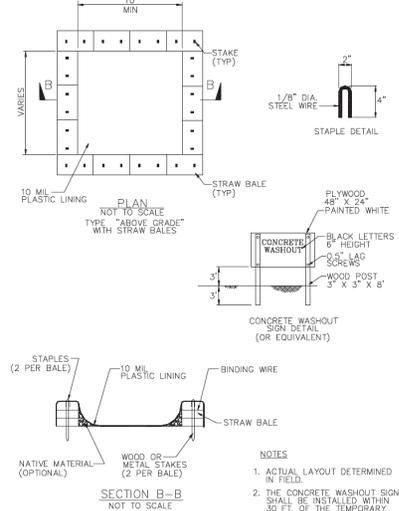
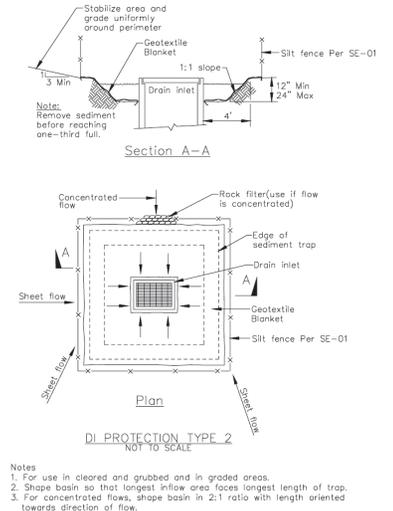
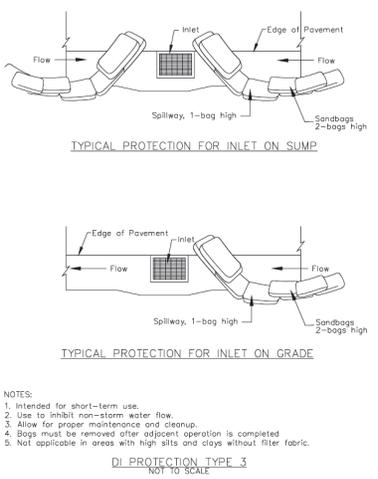
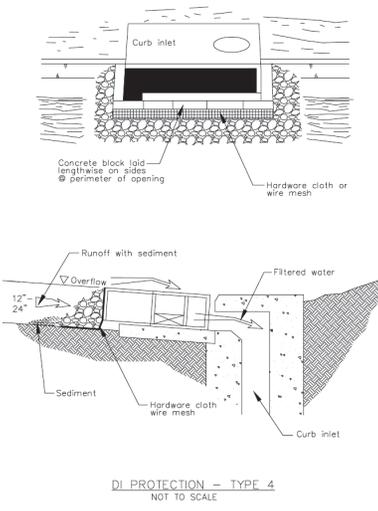
Source for Graphics: California Stormwater BMP Handbook, California Stormwater Quality Association, January 2003. Available from www.cabmphandbooks.com.

Project Information





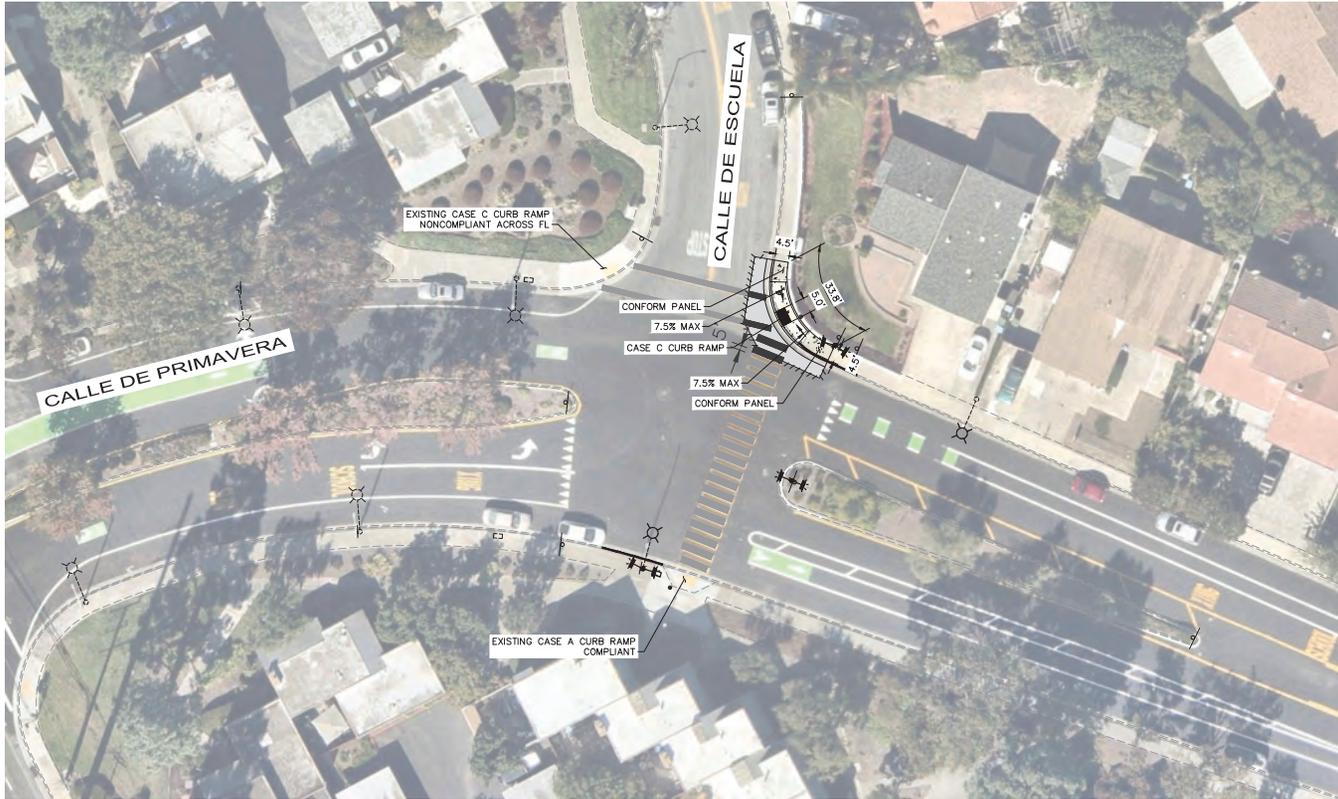
8 Storm Drain Inlet Protection
CASQA Detail SE-10



Source for Graphics: California Stormwater BMP Handbook, California Stormwater Quality Association, January 2003. Available from www.cabmphandbooks.com.

Project Information

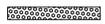
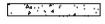
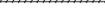


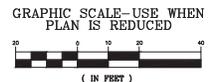


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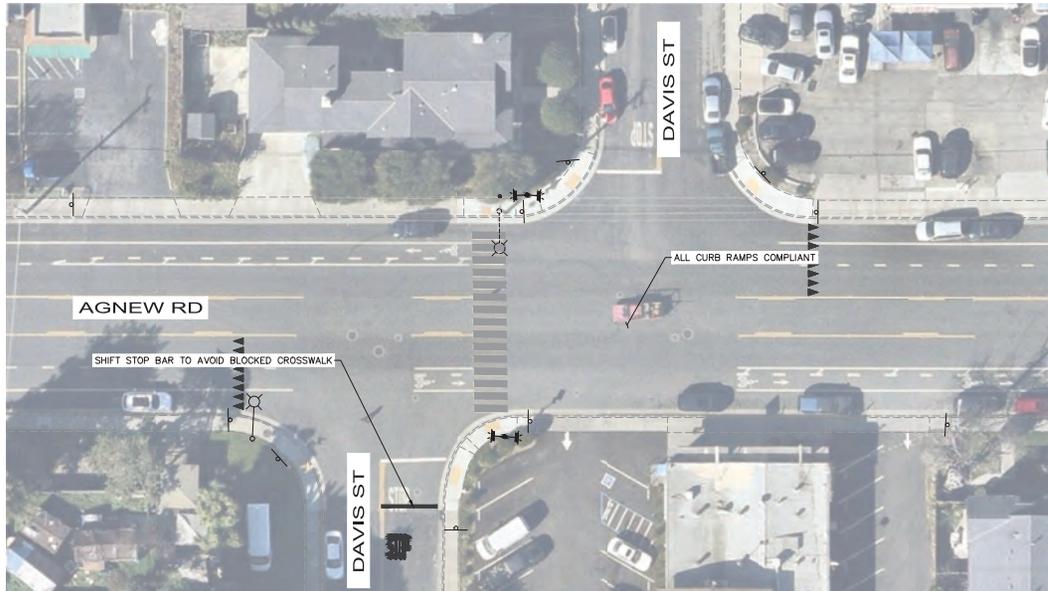


LEGEND

-  DETECTABLE WARNING SURFACE
-  CONCRETE SIDEWALK
-  MEDIAN PAVING
-  6" HMA (TYPE A) DEEP LIFT
-  SAWCUT LIMITS



	<h2 style="margin: 0;">Kimley»Horn</h2> <p style="font-size: small; margin: 0;">4637 Chabot Drive, Suite 200 Tel. No. (925) 398-4840 Pleasanton, California 94588</p>	PROJECT No. CE 24-25-07	CITY OF SANTA CLARA PUBLIC WORKS DEPARTMENT																																										
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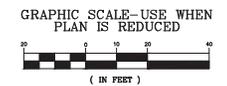


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ACCOUNT No.			
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CITY OF SANTA CLARA PUBLIC WORKS DEPARTMENT	
UNCONTROLLED CROSSWALKS IMPROVEMENTS - PHASE II	

APPROVED BY		TITLE	
DATE	01/07/2026	DWG No. P-02	
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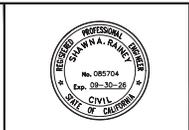
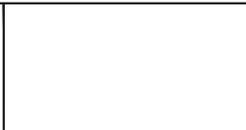
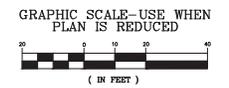


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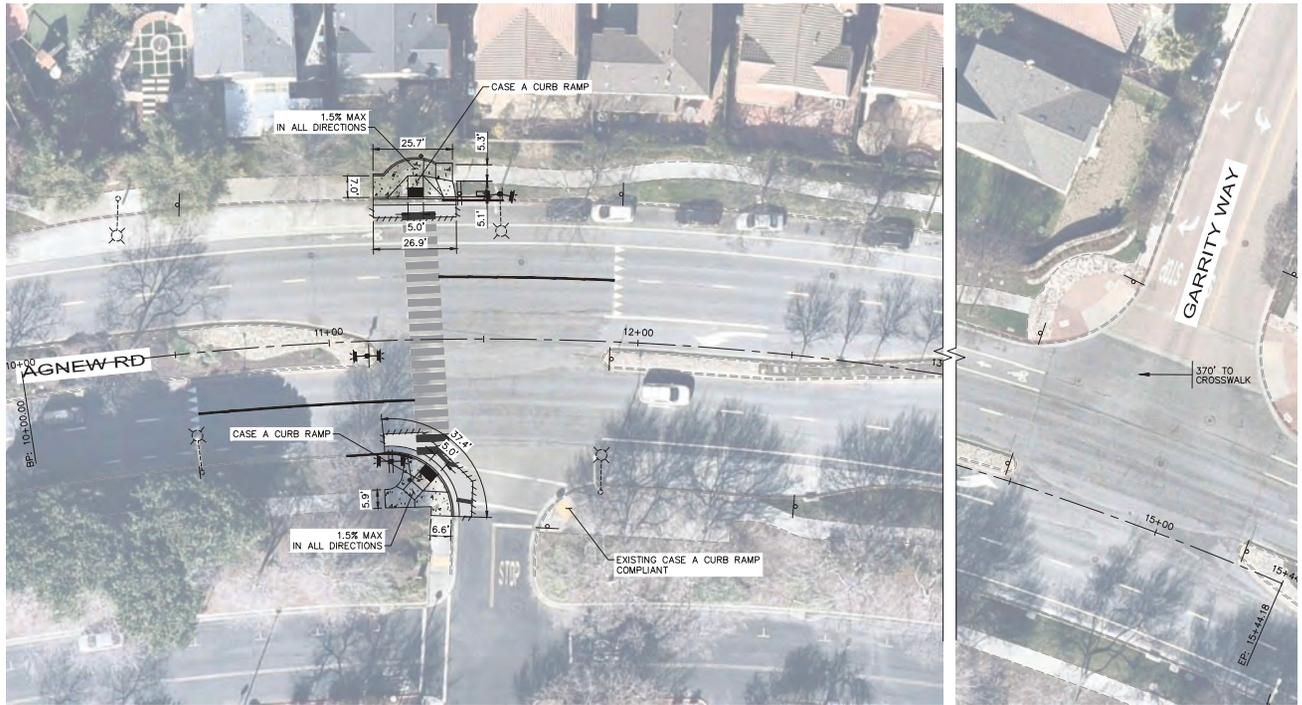


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CITY OF SANTA CLARA
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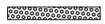
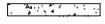
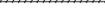
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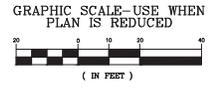


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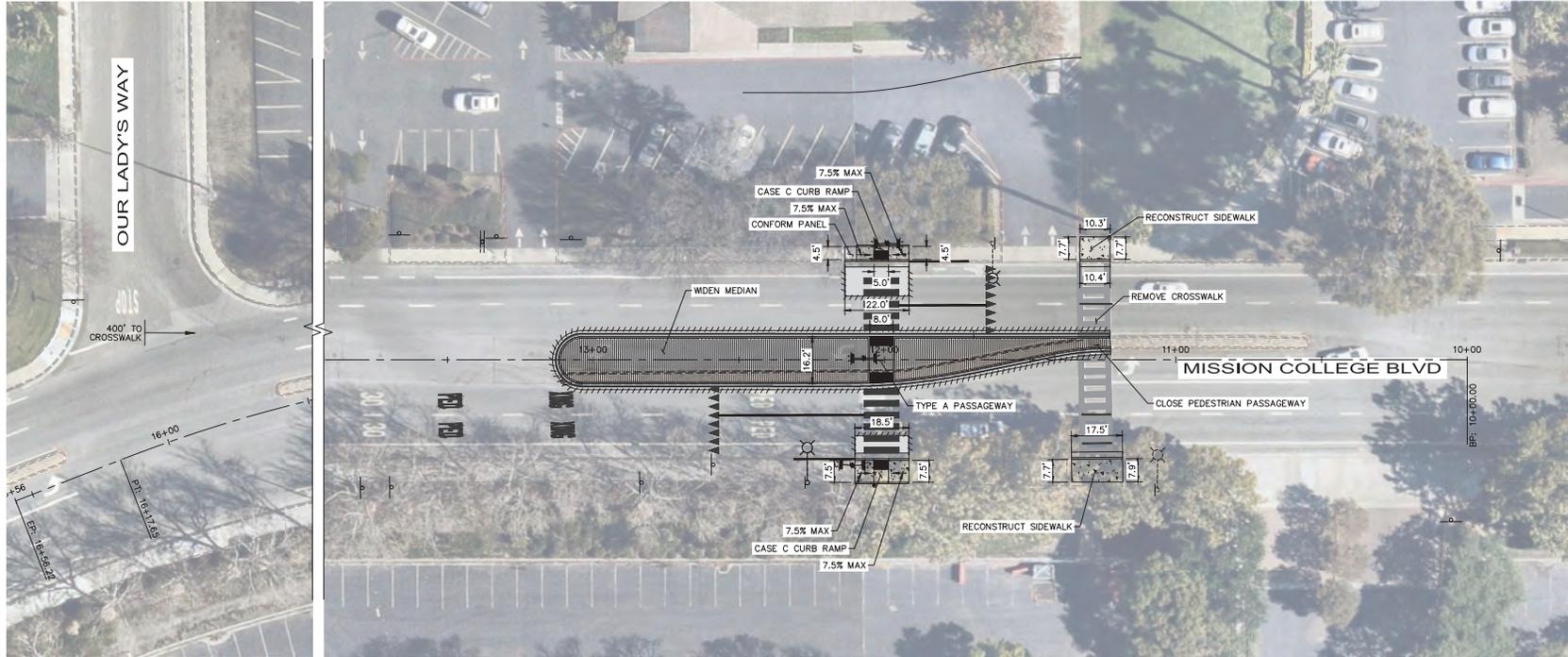


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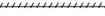
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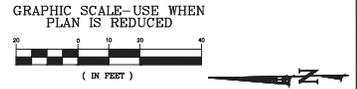


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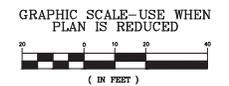


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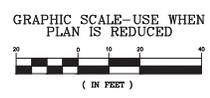


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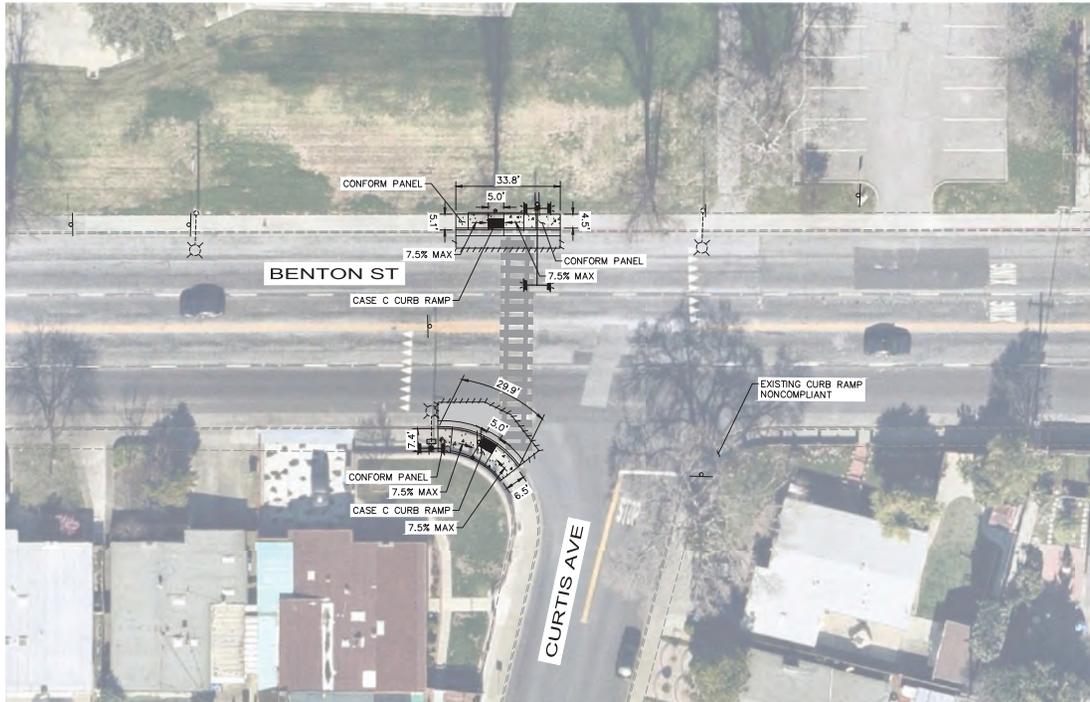


LEGEND

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-  SAWCUT LIMITS



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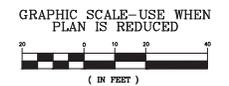


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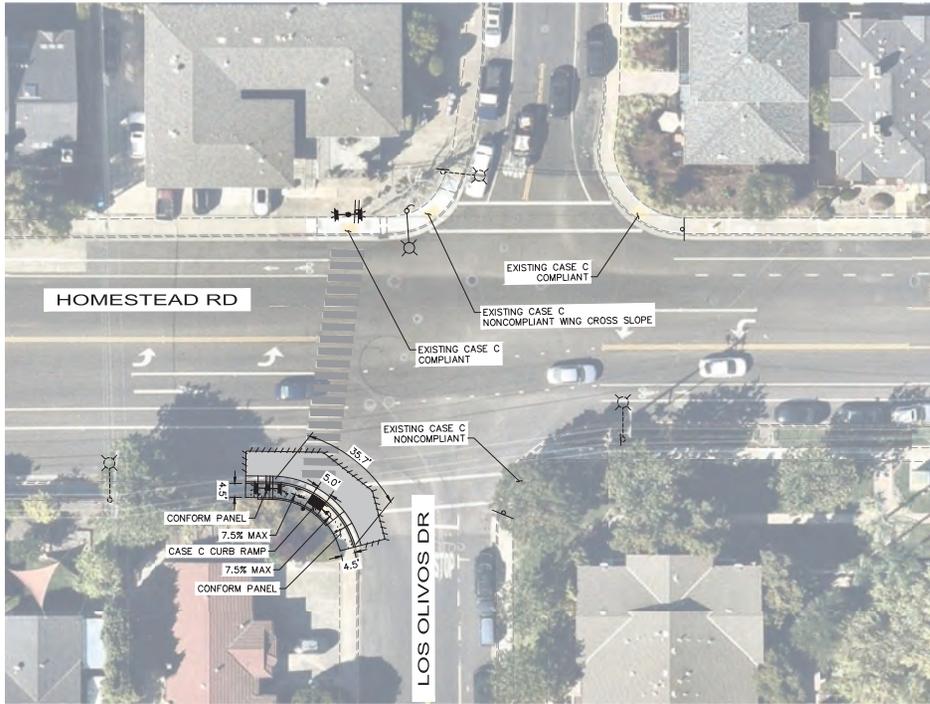


LEGEND

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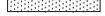
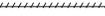
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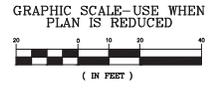


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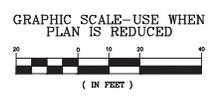


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- SAWCUT LIMITS



	<p>Kimley»Horn</p> <p>4637 Chabot Drive, Suite 200 Tel. No. (925) 398-4840 Pleasanton, California 94588</p>	PROJECT No. CE 24-25-07	<p>CITY OF SANTA CLARA PUBLIC WORKS DEPARTMENT</p> <p>UNCONTROLLED CROSSWALKS IMPROVEMENTS - PHASE II</p> <p>....</p>	APPROVED BY																																																															
		<table border="1" style="width: 100%; border-collapse: collapse;"> <tr> <th colspan="2">ACCOUNT No.</th> <th>DATE</th> <th>No.</th> <th>REVISION</th> <th>BY</th> </tr> <tr> <td> </td> <td> </td> <td> </td> <td> </td> <td> </td> <td> </td> </tr> <tr> <td> </td> <td> </td> <td> </td> <td> </td> <td> </td> <td> </td> </tr> <tr> <td> </td> <td> </td> <td> </td> <td> </td> <td> </td> <td> </td> </tr> <tr> <td> </td> <td> </td> <td> </td> <td> </td> <td> </td> <td> </td> </tr> </table>		ACCOUNT No.		DATE	No.	REVISION	BY																									<table border="1" style="width: 100%; border-collapse: collapse;"> <tr> <td colspan="2">TITLE</td> <td colspan="2">DATE</td> <td colspan="2">DWG No.</td> </tr> <tr> <td colspan="2"> </td> <td colspan="2">01/07/2026</td> <td colspan="2">P-10</td> </tr> <tr> <td>DESIGNED BY</td> <td>JTK</td> <td>DATE</td> <td>01/26</td> <td colspan="2">TRACING No.</td> </tr> <tr> <td>DRAWN BY</td> <td>AMT</td> <td>DATE</td> <td>01/26</td> <td colspan="2">12,444-D</td> </tr> <tr> <td>CHECKED BY</td> <td>SAR</td> <td>DATE</td> <td>01/26</td> <td colspan="2">21 OF 52</td> </tr> <tr> <td>AS BUILT BY</td> <td> </td> <td> </td> <td> </td> <td colspan="2"> </td> </tr> </table>	TITLE		DATE		DWG No.				01/07/2026		P-10		DESIGNED BY	JTK	DATE	01/26	TRACING No.		DRAWN BY	AMT	DATE	01/26	12,444-D		CHECKED BY	SAR	DATE	01/26	21 OF 52		AS BUILT BY		
ACCOUNT No.		DATE	No.	REVISION	BY																																																														
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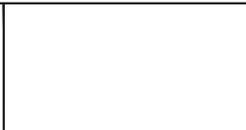
65% DESIGN PLANS
NOT FOR CONSTRUCTION
JANUARY 2026



LEGEND

-  DETECTABLE WARNING SURFACE
-  CONCRETE SIDEWALK
-  MEDIAN PAVING
-  6" HMA (TYPE A) DEEP LIFT
-  SAWCUT LIMITS

GRAPHIC SCALE—USE WHEN
PLAN IS REDUCED



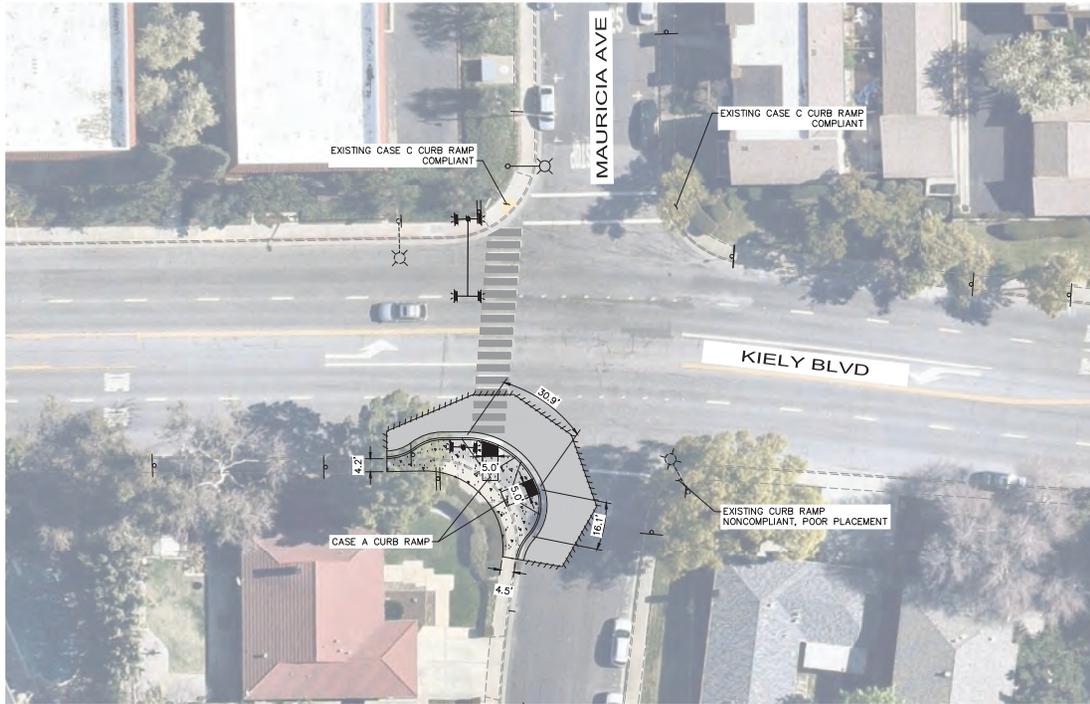
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Pleasanton, California 94588

PROJECT No. CE 24-25-07			
ACCOUNT No.			
DATE	No.	REVISION	BY

CITY OF SANTA CLARA PUBLIC WORKS DEPARTMENT	
UNCONTROLLED CROSSWALKS IMPROVEMENTS - PHASE II	

APPROVED BY			
TITLE			
DATE	INITIAL	DATE	DWG No.
01/07/2026			P-11
DESIGNED BY	JTK	01/26	TRACING No. 12,444-D
DRAWN BY	AMT	01/26	
CHECKED BY	SAR	01/26	
AS BUILT BY			

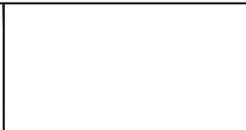
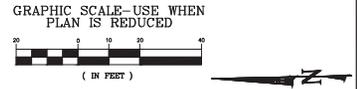


65% DESIGN PLANS
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JANUARY 2026



LEGEND

- DETECTABLE WARNING SURFACE
- CONCRETE SIDEWALK
- MEDIAN PAVING
- 6" HMA (TYPE A) DEEP LIFT
- SAWCUT LIMITS



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PROJECT No. CE 24-25-07			
ACCOUNT No.			
DATE	No.	REVISION	BY

CITY OF SANTA CLARA
PUBLIC WORKS DEPARTMENT

UNCONTROLLED CROSSWALKS
IMPROVEMENTS - PHASE II

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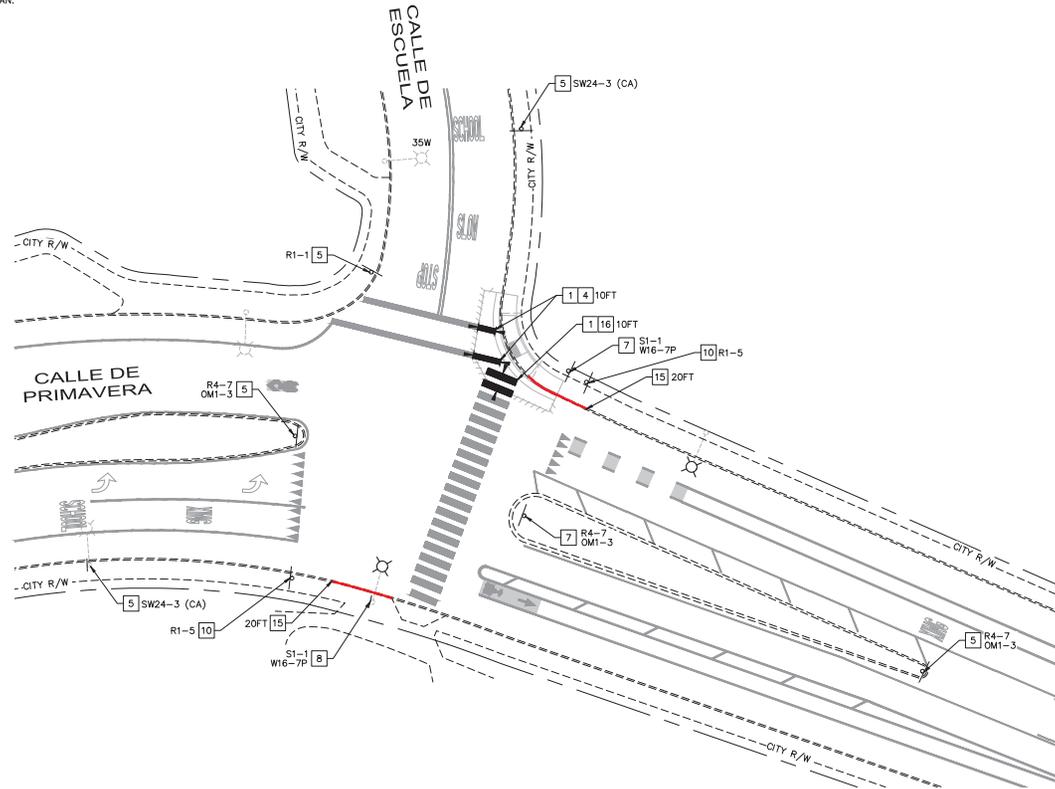
APPROVED BY			
TITLE			
DATE	INITIAL	DATE	DWG No.
01/07/2026			P-13
DESIGNED BY	JTK	01/26	TRACING No. 12,444-D
DRAWN BY	AMT	01/26	
CHECKED BY	SAR	01/26	
AS BUILT BY			

SIGNING AND STRIPING NOTES:

- 1 REMOVE EXISTING STRIPING OR PAVEMENT MARKING AS INDICATED ON PLAN.
- 4 INSTALL BASIC CROSSWALK PER 2025 CALTRANS STANDARD PLANS SHEET A24F.
- 5 EXISTING SIGN AND POLE TO REMAIN. SIGN TYPE AS INDICATED ON PLAN.
- 7 REMOVE AND SALVAGE EXISTING SIGN AND POLE. SIGN TYPE AS INDICATED ON PLAN.
- 8 REMOVE AND SALVAGE EXISTING SIGN FROM EXISTING POLE. SIGN TYPE AS INDICATED ON PLAN.
- 10 FURNISH AND INSTALL SIGN ON EXISTING POLE. SIGN TYPE AS INDICATED ON PLAN.
- 15 INSTALL RED CURB PAINT AS INDICATED ON PLAN.
- 16 INSTALL CONTINENTAL CROSSWALK PER 2025 CALTRANS STANDARD PLANS SHEET A24F.

LEGEND:

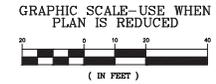
- +—+—+ LIMITS OF STRIPING REMOVAL
- +—+—+ DIMENSIONING



SIGNING SCHEDULE		
SIGN	INSTALL (EA)	REMOVE (EA)
SW24-2 (CA)	-	2
R1-5	2	-
R4-7	-	1
OMI-3	-	1
TOTAL	2	4

STRIPING SCHEDULE			
DETAIL	PATTERN OR LEGEND	INSTALL (LF)	REMOVE (LF)
	12" CONTINENTAL CROSSWALK	10	10
	BASIC CROSSWALK	10	10
	RED PAINT	40	-
TOTAL		60	20

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Pleasanton, California 94588

PROJECT No. CE 24-25-07			
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DATE	No.	REVISION	BY

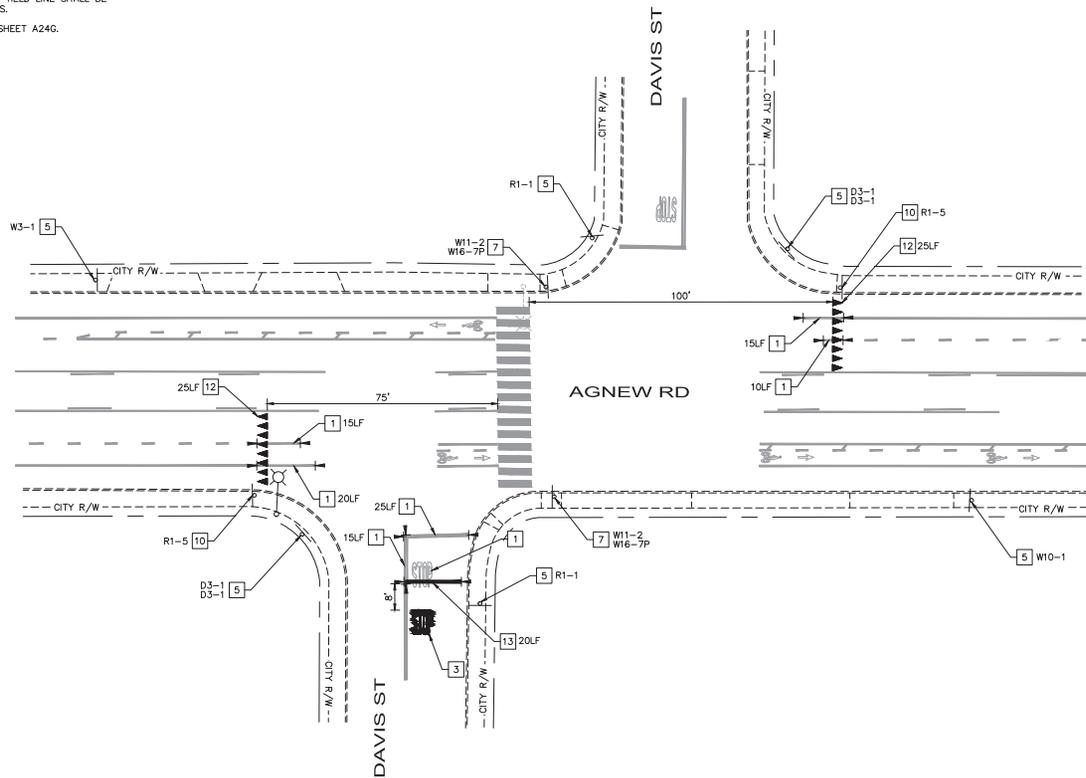
CITY OF SANTA CLARA PUBLIC WORKS DEPARTMENT		APPROVED BY	
UNCONTROLLED CROSSWALKS IMPROVEMENTS - PHASE II		TITLE	
SIGNING AND STRIPING - CALLE DE PRIMAVERA & CALLE DE ESCUELA		DATE 01/07/2026	DWG No. SS-01
DESIGNED BY JTK	DATE 01/26	INITIAL	DATE
DRAWN BY AMT	DATE 01/26		
CHECKED BY SAR	DATE 01/26		
AS BUILT BY			
TRACING No. 12,444-D		SHEET 25 OF 52	

SIGNING AND STRIPING NOTES:

- 1 REMOVE EXISTING STRIPING OR PAVEMENT MARKING AS INDICATED ON PLAN.
- 3 INSTALL PAVEMENT MARKING AS INDICATED ON PLAN. SEE CALTRANS STANDARD PLANS A24A THROUGH A24E FOR DETAILS.
- 5 EXISTING SIGN AND POLE TO REMAIN. SIGN TYPE AS INDICATED ON PLAN.
- 7 REMOVE AND SALVAGE EXISTING SIGN AND POLE. SIGN TYPE AS INDICATED ON PLAN.
- 10 FURNISH AND INSTALL SIGN ON EXISTING POLE. SIGN TYPE AS INDICATED ON PLAN.
- 12 INSTALL YIELD LINE PER 2025 CALTRANS STANDARD PLANS SHEET A24G. YIELD LINE SHALL BE SET 50' FROM EDGE OF CROSSWALK UNLESS OTHERWISE NOTED ON PLANS.
- 13 INSTALL LIMIT LINE (STOP LINE) PER 2025 CALTRANS STANDARD PLANS SHEET A24G.

LEGEND:

- +— LIMITS OF STRIPING REMOVAL
- +— DIMENSIONING

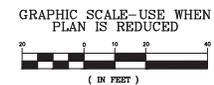


SIGNING SCHEDULE		
SIGN	INSTALL (EA)	REMOVE (EA)
W11-2	-	2
W16-7P	-	2
R1-5	2	-
TOTAL	2	4

STRIPING SCHEDULE			
DETAIL	PATTERN OR LEGEND	INSTALL (LF)	REMOVE (LF)
22	NO PASSING ZONES TWO DIRECTION	-	15
39	BIKE LANE LINE	-	35
39A	INTERSECTION LINE BIKE LANE	-	25
	LIMIT LINE	20	25
	YIELD LINE	50	-
TOTAL		70	100

PAVEMENT MARKING SCHEDULE				
PATTERN OF LEGEND	INSTALL		REMOVE	
	NO.	TOTAL SQ. FT.	NO.	TOTAL SQ. FT.
"STOP"	1	22	1	22
TOTAL	1	22	1	22

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PROJECT No. CE 24-25-07

ACCOUNT No.	DATE	No.	REVISION	BY

CITY OF SANTA CLARA
PUBLIC WORKS DEPARTMENT

UNCONTROLLED CROSSWALKS
IMPROVEMENTS - PHASE II
SIGNING AND STRIPING - AGNEW RD & DAVIS ST

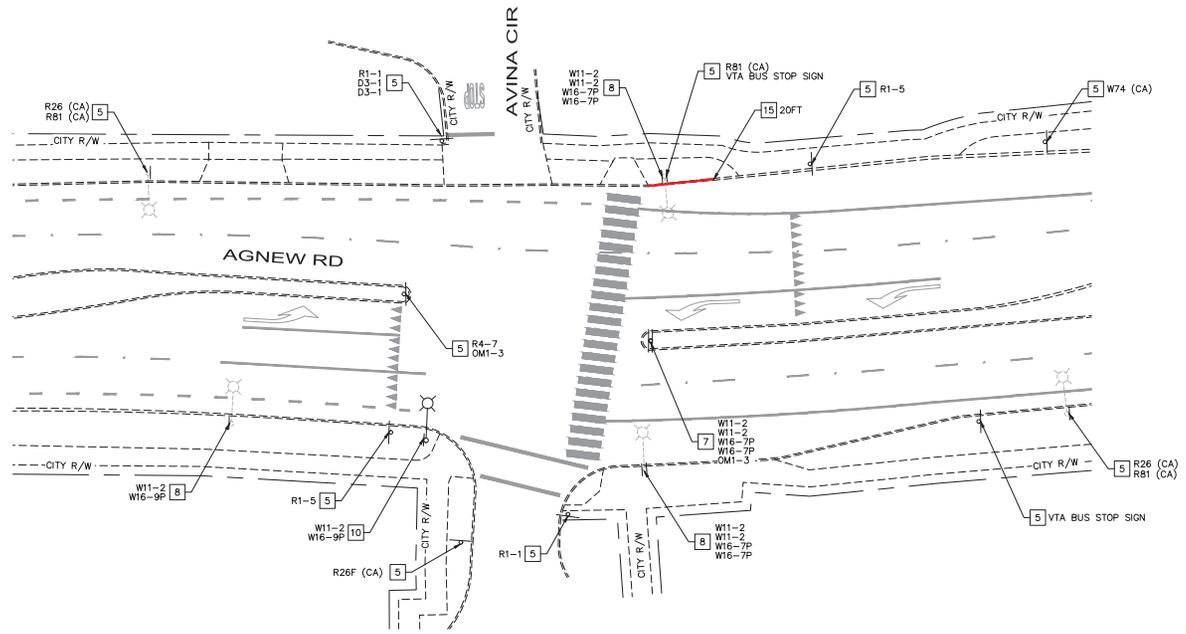
APPROVED BY		TITLE		DWG No.
DATE	01/07/2026	DATE	SS-02	
DESIGNED BY	JTK	DATE	01/26	TRACING No. 12,444-D
DRAWN BY	AMT	DATE	01/26	
CHECKED BY	SAR	DATE	01/26	

SIGNING AND STRIPING NOTES:

- 5 EXISTING SIGN AND POLE TO REMAIN. SIGN TYPE AS INDICATED ON PLAN.
- 7 REMOVE AND SALVAGE EXISTING SIGN AND POLE. SIGN TYPE AS INDICATED ON PLAN.
- 8 REMOVE AND SALVAGE EXISTING SIGN FROM EXISTING POLE. SIGN TYPE AS INDICATED ON PLAN.
- 10 FURNISH AND INSTALL SIGN ON EXISTING POLE. SIGN TYPE AS INDICATED ON PLAN.
- 15 INSTALL RED CURB PAINT AS INDICATED ON PLAN.

LEGEND:

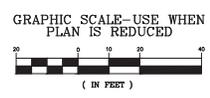
- +—+—+—+—+ LIMITS OF STRIPING REMOVAL
- +—+—+—+—+ DIMENSIONING



SIGNING SCHEDULE		
SIGN	INSTALL (EA)	REMOVE (EA)
W11-2	1	7
W16-7P	-	6
W16-9P	1	1
R4-7	-	1
OM1-3	-	2
TOTAL	2	17

STRIPING SCHEDULE			
DETAIL	PATTERN OR LEGEND	INSTALL (LF)	REMOVE (LF)
	RED PAINT	20	-
TOTAL		20	-

65% DESIGN PLANS
NOT FOR CONSTRUCTION
JANUARY 2026



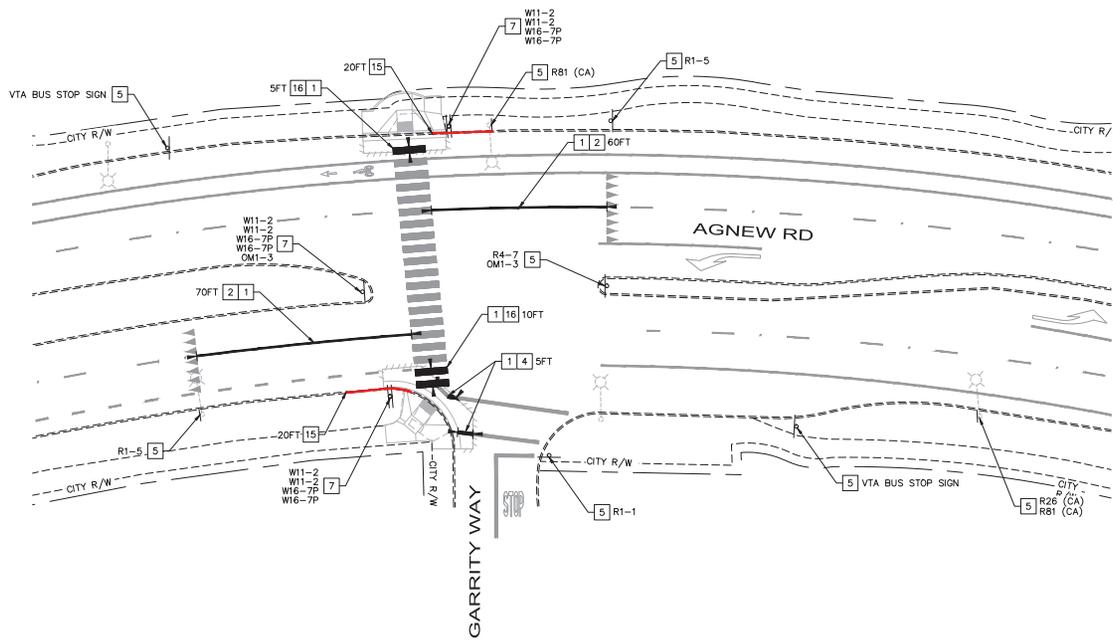
	<h2 style="margin: 0;">Kimley»Horn</h2> <p style="font-size: small; margin: 0;">4637 Chabot Drive, Suite 200 Tel. No. (925) 398-4840 Pleasanton, California 94588</p>	PROJECT No. CE 24-25-07 ACCOUNT No.	CITY OF SANTA CLARA PUBLIC WORKS DEPARTMENT UNCONTROLLED CROSSWALKS IMPROVEMENTS - PHASE II SIGNING AND STRIPING - AGNEW RD & AVINA CIR	APPROVED BY TITLE DATE 01/07/2026 INITIAL DATE DESIGNED BY JTK 01/26 DRAWN BY AMT 01/26 CHECKED BY SAR 01/26 AS BUILT BY
		DATE No. REVISION BY	DWG No. SS-03 TRACING No. 12,444-D	

SIGNING AND STRIPING NOTES:

- 1 REMOVE EXISTING STRIPING OR PAVEMENT MARKING AS INDICATED ON PLAN.
- 2 INSTALL CHANNELIZING LINE PER 2025 CALTRANS STANDARD PLANS SHEET A20D.
- 4 INSTALL BASIC CROSSWALK PER 2025 CALTRANS STANDARD PLANS SHEET A24F.
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LEGEND:

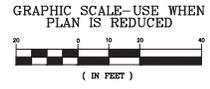
- |— LIMITS OF STRIPING REMOVAL
- |— DIMENSIONING



SIGNING SCHEDULE		
SIGN	INSTALL (EA)	REMOVE (EA)
W11-2	-	6
W16-7P	-	6
OM1-3	-	1
TOTAL	-	13

STRIPING SCHEDULE			
DETAIL	PATTERN OR LEGEND	INSTALL (LF)	REMOVE (LF)
①	LANE LINE	-	130
③⑨	CHANNELIZING LINE	130	-
	12" CONTINENTAL CROSSWALK	15	15
	BASIC CROSSWALK	5	5
	RED PAINT	40	-
	TOTAL	190	150

65% DESIGN PLANS
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JANUARY 2026



	<p>4637 Chabot Drive, Suite 200 Tel. No. (925) 398-4840 Pleasanton, California 94588</p>	PROJECT No. CE 24-25-07	CITY OF SANTA CLARA PUBLIC WORKS DEPARTMENT UNCONTROLLED CROSSWALKS IMPROVEMENTS - PHASE II SIGNING AND STRIPING - AGNEW RD & GARRITY WAY (WEST OF)	APPROVED BY
		ACCOUNT No.		DATE: 01/07/2026
		DATE No. REVISION BY	DESIGNED BY: JTK 01/26	TRACING No. 12,444-D
			DRAWN BY: AMT 01/26	
			CHECKED BY: SAR 01/26	
			AS BUILT BY:	

SIGNING AND STRIPING NOTES:

- 1 REMOVE EXISTING STRIPING OR PAVEMENT MARKING AS INDICATED ON PLAN.
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- 16 INSTALL CONTINENTAL CROSSWALK PER 2025 CALTRANS STANDARD PLANS SHEET A24F.

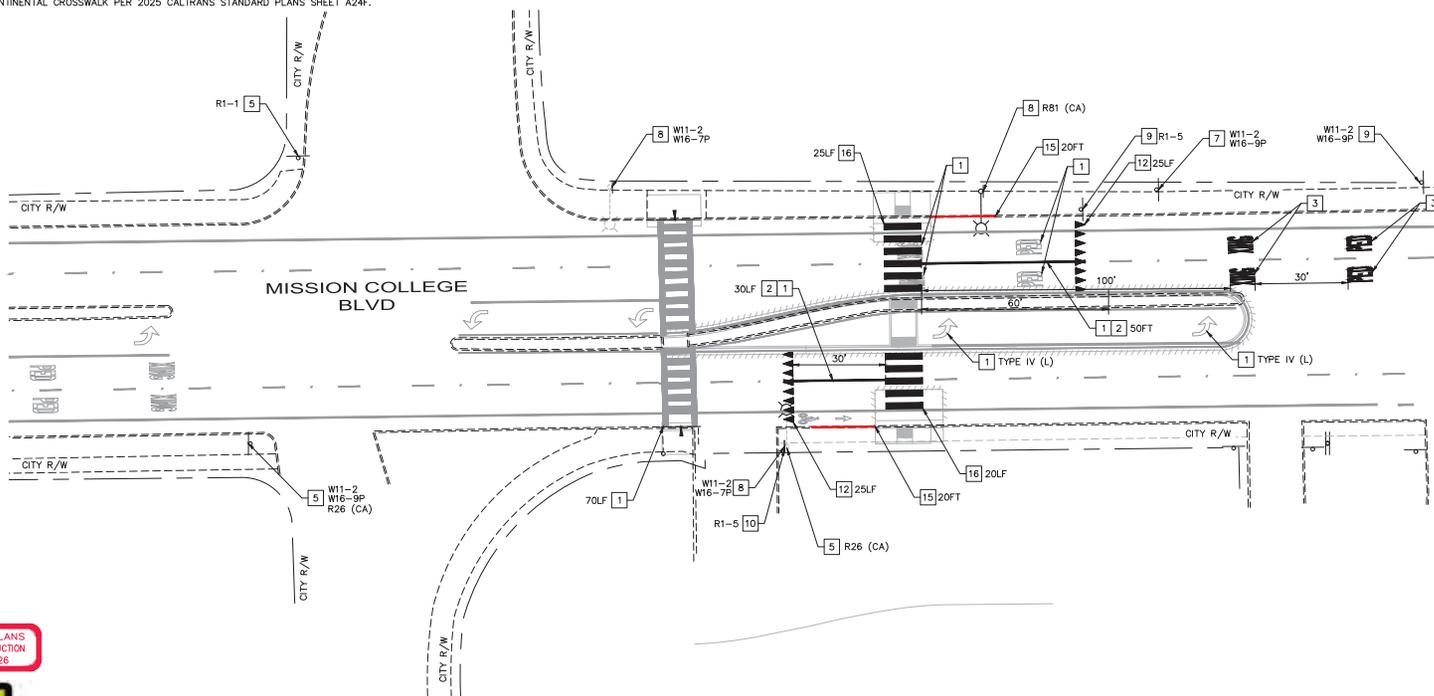
LEGEND:

- |— LIMITS OF STRIPING REMOVAL
- |— DIMENSIONING

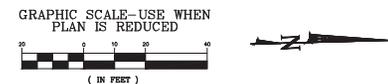
SIGNING SCHEDULE		
SIGN	INSTALL (EA)	REMOVE (EA)
W11-2	1	3
W16-7P	—	2
W16-9P	1	1
R1-5	2	—
RB1 (CA)	—	1
TOTAL	4	7

STRIPING SCHEDULE			
DETAIL	PATTERN OR LEGEND	INSTALL (LF)	REMOVE (LF)
9	LANE LINE	—	80
39	CHANNELIZING LINE	80	—
	12" LADDER CROSSWALK	—	70
	12" CONTINENTAL CROSSWALK	45	—
	YIELD LINE	50	—
	RED PAINT	40	—
	TOTAL	215	150

PAVEMENT MARKING SCHEDULE				
PATTERN OF LEGEND	INSTALL		REMOVE	
	NO.	TOTAL SQ. FT.	NO.	TOTAL SQ. FT.
TYPE IV (L) ARROW	—	—	2	30
"PED"	2	36	2	36
"XING"	2	42	2	42
TOTAL	8	156	10	186



65% DESIGN PLANS
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JANUARY 2026



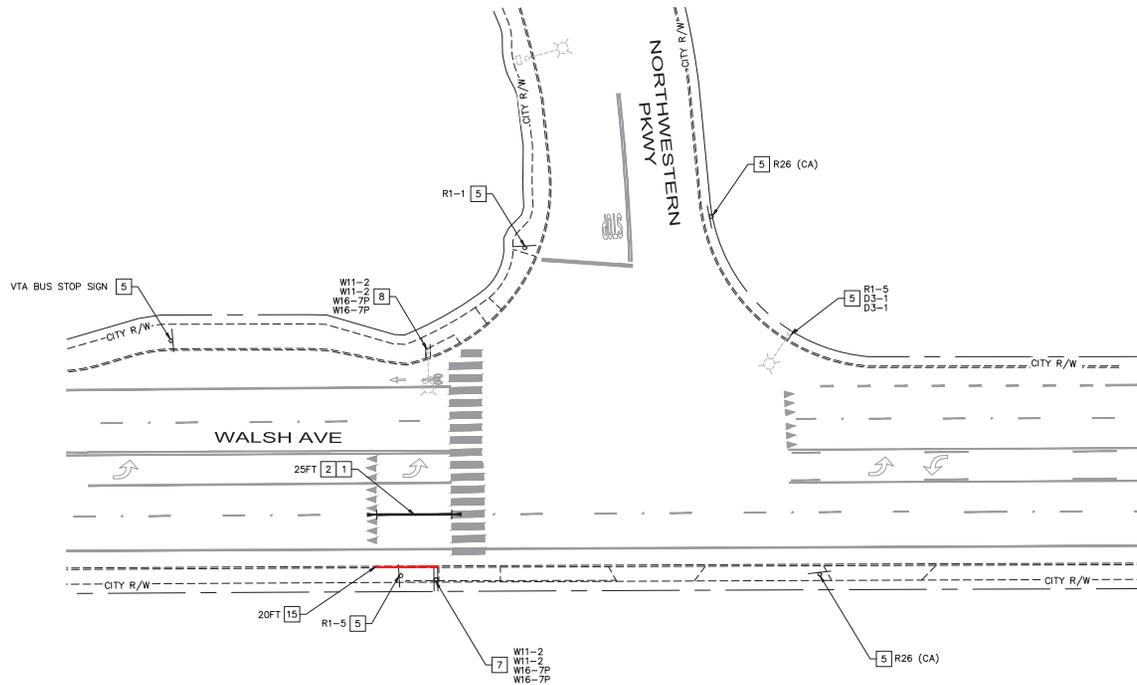
	<p>4637 Chabot Drive, Suite 200 Tel. No. (925) 398-4840 Pleasanton, California 94588</p>	PROJECT No. CE 24-25-07	CITY OF SANTA CLARA PUBLIC WORKS DEPARTMENT UNCONTROLLED CROSSWALKS IMPROVEMENTS - PHASE II SIGNING AND STRIPING - MISSION COLLEGE BLVD & OUR LADY'S WAY (SOUTH OF)
		ACCOUNT No. _____ DATE No. _____ REVISION BY _____	

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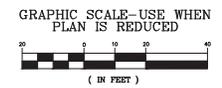
- +— LIMITS OF STRIPING REMOVAL
- +— DIMENSIONING



SIGNING SCHEDULE		
SIGN	INSTALL (EA)	REMOVE (EA)
W11-2	-	4
W16-7P	-	4
TOTAL	-	8

STRIPING SCHEDULE			
DETAIL	PATTERN OR LEGEND	INSTALL (LF)	REMOVE (LF)
①	LANE LINE	-	25
③9	CHANNELIZING LINE	125	-
	RED PAINT	20	-
	TOTAL	145	25

65% DESIGN PLANS
NOT FOR CONSTRUCTION
JANUARY 2026



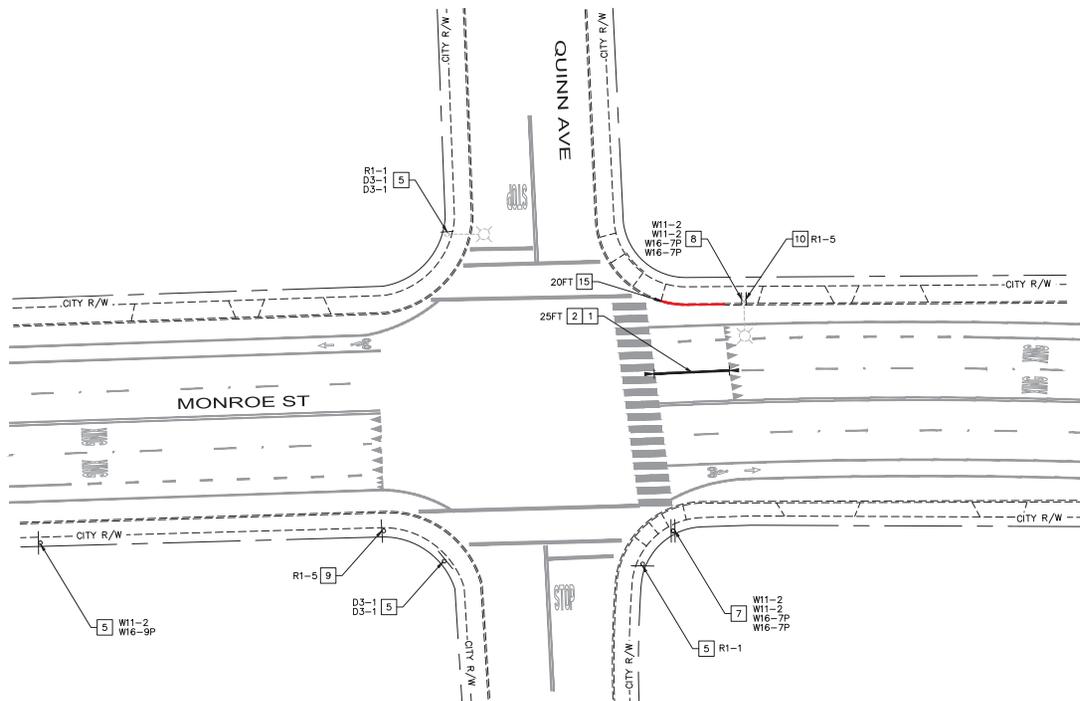
	<p>4637 Chabot Drive, Suite 200 Tel. No. (925) 398-4840 Pleasanton, California 94588</p>	PROJECT No. CE 24-25-07 ACCOUNT No.	CITY OF SANTA CLARA PUBLIC WORKS DEPARTMENT		APPROVED BY TITLE:
		DATE No. REVISION BY	UNCONTROLLED CROSSWALKS IMPROVEMENTS - PHASE II SIGNING AND STRIPING - WALSH AVE & NORTHWESTERN PKWY		DATE: 01/07/2026 INITIAL DATE DESIGNED BY: JTK 01/26 DRAWN BY: AMT 01/26 CHECKED BY: SAR 01/26 AS BUILT BY:

SIGNING AND STRIPING NOTES:

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- 15 INSTALL RED CURB PAINT AS INDICATED ON PLAN.

LEGEND:

- +— LIMITS OF STRIPING REMOVAL
- +— DIMENSIONING



SIGNING SCHEDULE		
SIGN	INSTALL (EA)	REMOVE (EA)
W11-2	-	4
W16-7P	-	4
R1-5	2	-
TOTAL	2	8

STRIPING SCHEDULE			
DETAIL	PATTERN OR LEGEND	INSTALL (LF)	REMOVE (LF)
①	LANE LINE	-	25
③⑨	CHANNELIZING LINE	105	-
	RED PAINT	20	-
	TOTAL	125	25

65% DESIGN PLANS
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JANUARY 2026



GRAPHIC SCALE—USE WHEN PLAN IS REDUCED



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Pleasanton, California 94588

PROJECT No. CE 24-25-07			
ACCOUNT No.			
DATE	No.	REVISION	BY

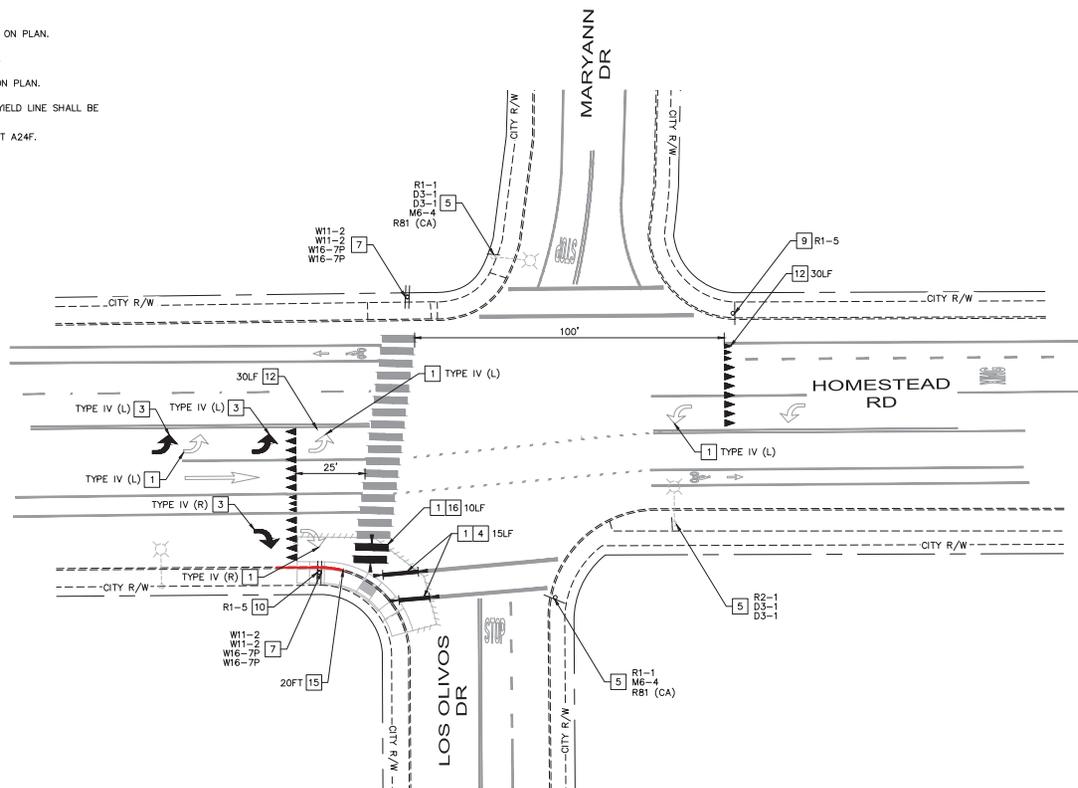
CITY OF SANTA CLARA		APPROVED BY	
PUBLIC WORKS DEPARTMENT		TITLE	
UNCONTROLLED CROSSWALKS IMPROVEMENTS - PHASE II		DATE	01/07/2026
SIGNING AND STRIPING - MONROE ST & QUINN AVE		INITIAL	DATE
		JTK	01/26
		AMT	01/26
		SAR	01/26
		AS BUILT BY	
		DWG No.	SS-07
		TRACING No.	12,444-D

SIGNING AND STRIPING NOTES:

- 1 REMOVE EXISTING STRIPING OR PAVEMENT MARKING AS INDICATED ON PLAN.
- 3 INSTALL PAVEMENT MARKING AS INDICATED ON PLAN. SEE CALTRANS STANDARD PLANS A24A THROUGH A24E FOR DETAILS.
- 4 INSTALL BASIC CROSSWALK PER 2025 CALTRANS STANDARD PLANS SHEET A24F.
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- 9 FURNISH AND INSTALL SIGN AND POLE. SIGN TYPE AS INDICATED ON PLAN.
- 10 FURNISH AND INSTALL SIGN ON EXISTING POLE. SIGN TYPE AS INDICATED ON PLAN.
- 12 INSTALL YIELD LINE PER 2025 CALTRANS STANDARD PLANS SHEET A24G. YIELD LINE SHALL BE SET 50' FROM EDGE OF CROSSWALK UNLESS OTHERWISE NOTED ON PLANS.
- 14 INSTALL LADDER CROSSWALK PER 2025 CALTRANS STANDARD PLANS SHEET A24F.
- 15 INSTALL RED CURB PAINT AS INDICATED ON PLAN.

LEGEND:

- +—+—+ LIMITS OF STRIPING REMOVAL
- +—+ DIMENSIONING

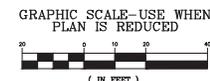


SIGNING SCHEDULE		
SIGN	INSTALL (EA)	REMOVE (EA)
W11-2	-	4
W16-7P	-	4
R1-5	2	-
TOTAL	2	8

STRIPING SCHEDULE			
DETAIL	PATTERN OR LEGEND	INSTALL (LF)	REMOVE (LF)
	YIELD LINE	60	-
	12" CONTINENTAL CROSSWALK	10	10
	BASIC CROSSWALK	15	15
	RED PAINT	20	-
TOTAL		105	130

PAVEMENT MARKING SCHEDULE				
PATTERN OF LEGEND	INSTALL		REMOVE	
	NO.	TOTAL SQ. FT.	NO.	TOTAL SQ. FT.
TYPE IV (L/R) ARROW	3	45	4	60
TOTAL	3	45	4	60

65% DESIGN PLANS
NOT FOR CONSTRUCTION
JANUARY 2026



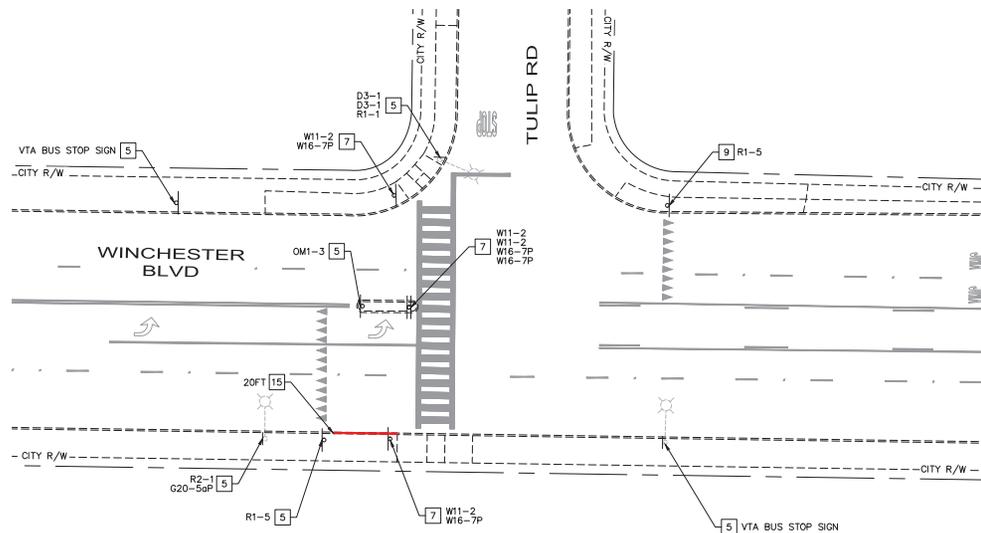
	<p>4637 Chabot Drive, Suite 200 Tel. No. (925) 398-4840 Pleasanton, California 94588</p>	PROJECT No. CE 24-25-07	CITY OF SANTA CLARA PUBLIC WORKS DEPARTMENT UNCONTROLLED CROSSWALKS IMPROVEMENTS - PHASE II SIGNING AND STRIPING - HOMESTEAD RD & LOS OLIVOS DR	APPROVED BY
		ACCOUNT No.		DATE
		DATE No. REVISION BY	DESIGNED BY JTK 01/26	INITIAL DATE
			DRAWN BY AMT 01/26	DATE
			CHECKED BY SAR 01/26	DATE
			AS BUILT BY	
				DWG No. SS-09
				TRACING No. 12,444-D

SIGNING AND STRIPING NOTES:

- 5 EXISTING SIGN AND POLE TO REMAIN. SIGN TYPE AS INDICATED ON PLAN.
- 7 REMOVE AND SALVAGE EXISTING SIGN AND POLE. SIGN TYPE AS INDICATED ON PLAN.
- 9 FURNISH AND INSTALL SIGN AND POLE. SIGN TYPE AS INDICATED ON PLAN.
- 15 INSTALL RED CURB PAINT AS INDICATED ON PLAN.

LEGEND:

- |— LIMITS OF STRIPING REMOVAL
- |— DIMENSIONING



SIGNING SCHEDULE		
SIGN	INSTALL (EA)	REMOVE (EA)
W11-2	-	4
W16-7P	-	4
R1-5	1	-
TOTAL	1	8

STRIPING SCHEDULE			
DETAIL	PATTERN OR LEGEND	INSTALL (LF)	REMOVE (LF)
	RED PAINT	20	-
TOTAL		20	-

65% DESIGN PLANS
NOT FOR CONSTRUCTION
JANUARY 2026



GRAPHIC SCALE—USE WHEN
PLAN IS REDUCED



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Pleasanton, California 94588

PROJECT No. CE 24-25-07			
ACCOUNT No.			
DATE	No.	REVISION	BY

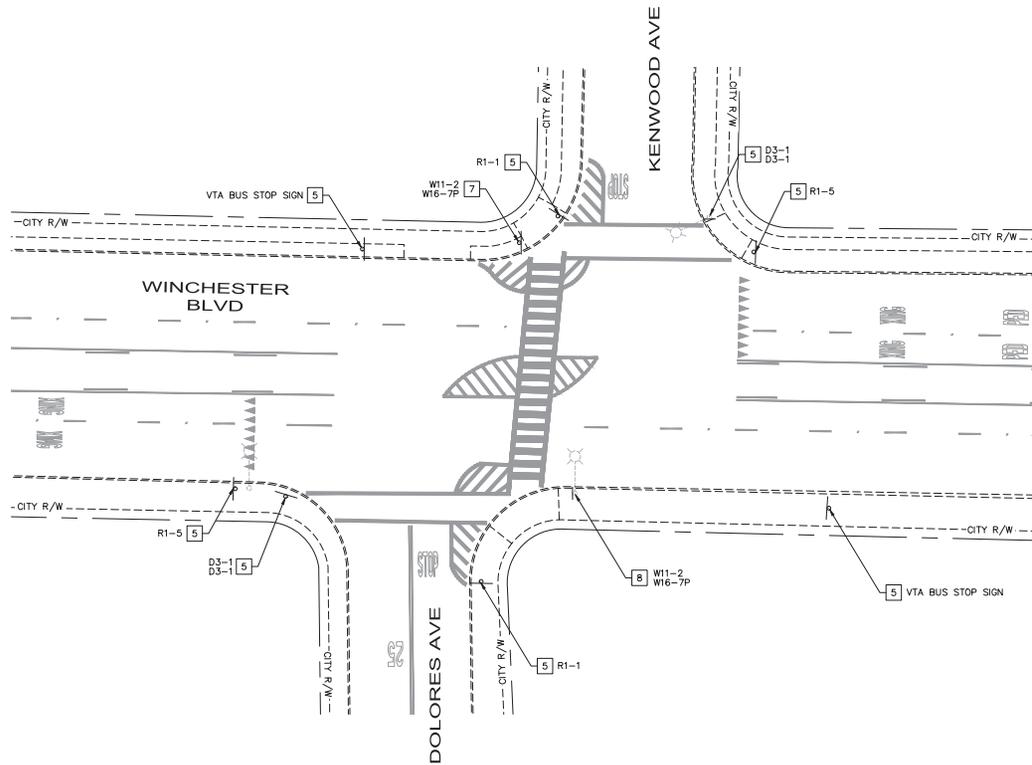
CITY OF SANTA CLARA PUBLIC WORKS DEPARTMENT		APPROVED BY	
UNCONTROLLED CROSSWALKS IMPROVEMENTS - PHASE II		TITLE	
SIGNING AND STRIPING - WINCHESTER BLVD & TULIP RD		DATE 01/07/2026	DWG No. SS-11
DESIGNED BY JTK	DATE 01/26	TRACING No. 12,444-D	
DRAWN BY AMT	DATE 01/26		
CHECKED BY SAR	DATE 01/26		
AS BUILT BY			

SIGNING AND STRIPING NOTES:

- 5 EXISTING SIGN AND POLE TO REMAIN. SIGN TYPE AS INDICATED ON PLAN.
- 7 REMOVE AND SALVAGE EXISTING SIGN AND POLE. SIGN TYPE AS INDICATED ON PLAN.
- 8 REMOVE AND SALVAGE EXISTING SIGN FROM EXISTING POLE. SIGN TYPE AS INDICATED ON PLAN.

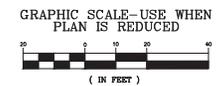
LEGEND:

- |— LIMITS OF STRIPING REMOVAL
- |— DIMENSIONING



SIGNING SCHEDULE		
SIGN	INSTALL (EA)	REMOVE (EA)
W11-2	-	2
W16-7P	-	2
TOTAL	-	4

65% DESIGN PLANS
NOT FOR CONSTRUCTION
JANUARY 2026

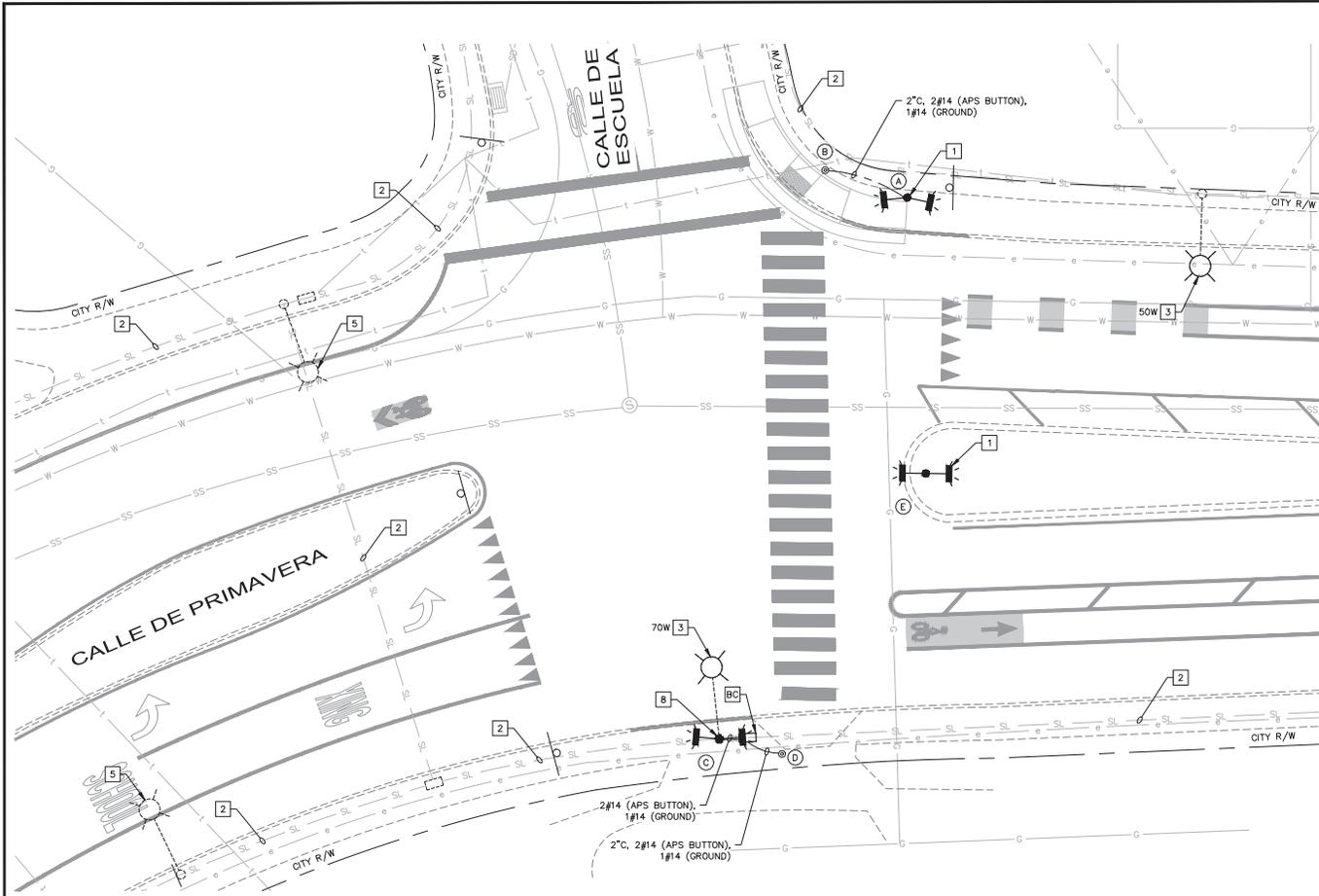


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Pleasanton, California 94588

PROJECT No. CE 24-25-07			
ACCOUNT No.			
DATE	No.	REVISION	BY

CITY OF SANTA CLARA PUBLIC WORKS DEPARTMENT		APPROVED BY	
UNCONTROLLED CROSSWALKS IMPROVEMENTS - PHASE II		TITLE	
SIGNING AND STRIPING - WINCHESTER BLVD & KENWOOD AVE		DATE 01/07/2026	DWG No. SS-12
DESIGNED BY JTK	DATE 01/26	TRACING No. 12,444-D	
DRAWN BY AMT	DATE 01/26		
CHECKED BY SAR	DATE 01/26		
AS BUILT BY			



- GENERAL NOTES (TS SHEETS ONLY)**
1. THE CONTRACTOR SHALL FURNISH AND INSTALL ALL MATERIALS, EQUIPMENT, LABOR, AND INCIDENTAL WORK FOR THE PROJECT.
 2. EXCEPT AS OTHERWISE SPECIFIED, ALL WORK AND MATERIALS SHALL CONFORM TO AND BE IN ACCORDANCE WITH THE APPLICABLE PROVISIONS OF THE CITY OF SANTA CLARA DESIGN STANDARDS, STANDARD SPECIFICATIONS, AND STANDARD PLANS; AND 2025 CALTRANS STANDARD PLANS AND SPECIFICATIONS, THESE PLANS, AND THE CONTRACT SPECIFICATIONS. IN CASE OF CONFLICT BETWEEN SAID DOCUMENTS, THE MOST STRINGENT REQUIREMENT SHALL APPLY AS DETERMINED BY THE CITY ENGINEER.
 3. LOCATION OF ALL UNDERGROUND AND ABOVE GROUND UTILITIES ARE APPROXIMATE. THE CONTRACTOR SHALL DETERMINE THE EXACT LOCATIONS AND VERIFY ALL CONDITIONS ON THE JOB SITE. DIG FOUNDATIONS UNTIL CLEAR OF OBSTRUCTIONS. PHONE UNDERGROUND SERVICE ALERT AT 811 OR (800) 227-2600. POTHOLES REQUIRED FOR UTILITIES SHOWN OR NOT SHOWN SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR AND SHALL BE INCLUDED IN THE CONTRACTOR BID PRICE. CONTRACTOR SHALL COORDINATE ALL PHASES OF CONSTRUCTION WITH AFFECTED AGENCIES AND UTILITY COMPANIES.
 4. FURNISH AND INSTALL ACCESSIBLE PEDESTRIAN SIGNAL PER MANUFACTURER'S RECOMMENDATION AND THE STATE STANDARD PLAN ES-5C "DETAIL A".
 5. ALL PUSH BUTTONS ARE TO BE MOUNTED SUCH THAT HEIGHT FROM SIDEWALK SURFACE AND CENTER OF PUSH BUTTON IS BETWEEN 40" AND 46" ABOVE NEW SIDEWALK SURFACE, AND PUSH BUTTON IS 10" MAXIMUM FROM NEAREST EDGE OF PEDESTRIAN WALKWAY. INSTALL ACCESSIBLE PEDESTRIAN SIGNAL ON EXTENSION, IF NECESSARY, TO MEET 10" REACH REQUIREMENT. APS PUSH BUTTONS SHALL BE NON-VIBRATING TYPE WITH R10-25 SIGN. WARNING MESSAGE SHALL BE "YELLOW LIGHTS ARE FLASHING" REPEATED TWICE.
 6. WHEN INSTALLING A NEW POLE OR CONDUIT IN AN EXISTING SIDEWALK, THE CONTRACTOR SHALL REMOVE AND REPLACE THE ENTIRE CONCRETE FLAG (PANEL) FROM SCORE LINE TO SCORE LINE WITHIN THE AREA OF CONSTRUCTION.

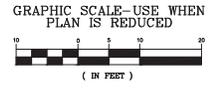
- CONSTRUCTION NOTES**
- [1] FURNISH AND INSTALL RRFB ASSEMBLY PER DETAIL 'A', SHEET TS-14.
 - [2] PROTECT EXISTING CONDUCTORS IN PLACE AND MAINTAIN EXISTING CIRCUIT.
 - [3] FURNISH AND INSTALL NEW LEOTEK GREENCOBRA LUMINAIRE OR APPROVED EQUAL ON EXISTING POLE. WATTAGE PER PLAN.
 - [5] EXISTING LUMINAIRE AND POLE TO REMAIN IN PLACE.
 - [8] FURNISH AND INSTALL RRFB ASSEMBLY PER DETAIL 'B', SHEET TS-14.
 - [BC] INSTALL PULL BOX INTO EXISTING CONDUIT.

POLE & EQUIPMENT SCHEDULE

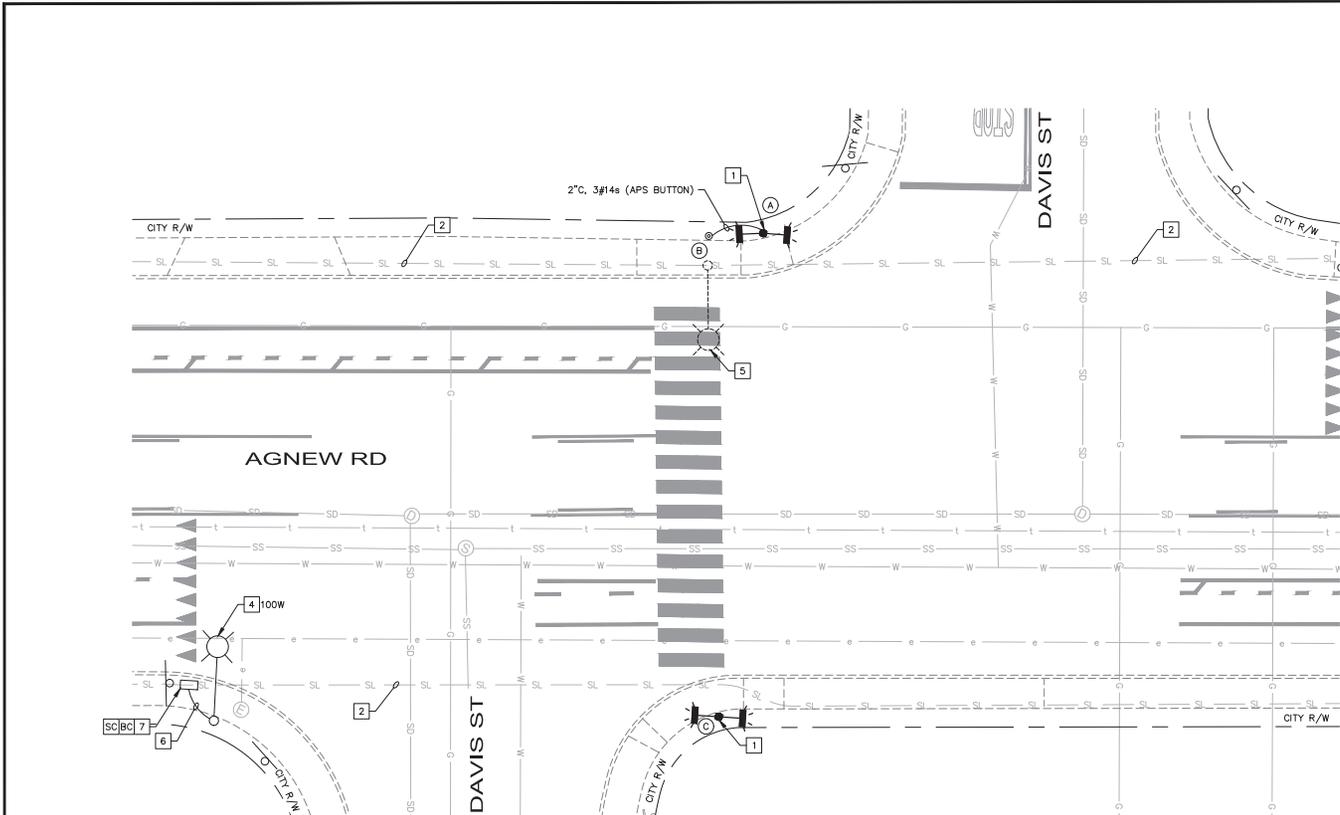
LOCATION	STANDARD		APS	FRONT SIDE SIGNS	BACK SIDE SIGNS
	TYPE	HEIGHT			
A	PELCO SPUN SCH 40 PB-5100	14'		S1-1 W16-7P	S1-1 W16-7P
B	APS POST	-	⊙	-	-
C	CALTRANS TYPE 15 POLE	30'	E	S1-1 W16-7P	S1-1 W16-7P
D	APS POST	-	⊙	-	-
E	PELCO SPUN SCH 40 PB-5100	14'		S1-1 W16-7P	S1-1 W16-7P

E - DENOTES EXISTING EQUIPMENT

65% DESIGN PLANS
NOT FOR CONSTRUCTION
JANUARY 2026



	<p>4637 Chabot Drive, Suite 200 Tel. No. (925) 398-4840 Pleasanton, California 94588</p>	PROJECT No. CE 24-25-07	CITY OF SANTA CLARA PUBLIC WORKS DEPARTMENT UNCONTROLLED CROSSWALKS IMPROVEMENTS - PHASE II RRFB - CALLE DE PRIMAVERA & CALLE DE ESCUELA	APPROVED BY TITLE
		ACCOUNT No.		DATE 01/07/2026
		DATE No. REVISION BY		DESIGNED BY JTK 01/26
				DRAWN BY AMT 01/26
				CHECKED BY SAR 01/26
				AS BUILT BY
				TRACING No. 12,444-D



GENERAL NOTES (TS SHEETS ONLY)

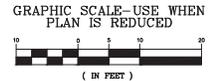
1. THE CONTRACTOR SHALL FURNISH AND INSTALL ALL MATERIALS, EQUIPMENT, LABOR, AND INCIDENTAL WORK FOR THE PROJECT.
2. EXCEPT AS OTHERWISE SPECIFIED, ALL WORK AND MATERIALS SHALL CONFORM TO AND BE IN ACCORDANCE WITH THE APPLICABLE PROVISIONS OF THE CITY OF SANTA CLARA DESIGN STANDARDS, STANDARD SPECIFICATIONS, AND STANDARD PLANS, AND 2025 CALTRANS STANDARD PLANS AND SPECIFICATIONS, THESE PLANS, AND THE CONTRACT SPECIFICATIONS. IN CASE OF CONFLICT BETWEEN SAID DOCUMENTS, THE MOST STRINGENT REQUIREMENT SHALL APPLY AS DETERMINED BY THE CITY ENGINEER.
3. LOCATION OF ALL UNDERGROUND AND ABOVE GROUND UTILITIES ARE APPROXIMATE. THE CONTRACTOR SHALL DETERMINE THE EXACT LOCATIONS AND VERIFY ALL CONDITIONS ON THE JOB SITE. DIG FOUNDATIONS UNTIL CLEAR OF OBSTRUCTIONS. PHONE UNDERGROUND SERVICE ALERT AT 811 OR (800) 227-2600. POT-HOLING REQUIRED FOR UTILITIES SHOWN OR NOT SHOWN SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR AND SHALL BE INCLUDED IN THE CONTRACTOR BID PRICE. CONTRACTOR SHALL COORDINATE ALL PHASES OF CONSTRUCTION WITH AFFECTED AGENCIES AND UTILITY COMPANIES.
4. FURNISH AND INSTALL ACCESSIBLE PEDESTRIAN SIGNAL PER MANUFACTURER'S RECOMMENDATION AND THE STATE STANDARD PLAN ES-5C "DETAIL A".
5. ALL PUSH BUTTONS ARE TO BE MOUNTED SUCH THAT HEIGHT FROM SIDEWALK SURFACE AND CENTER OF PUSH BUTTON IS BETWEEN 40" AND 46" ABOVE NEW SIDEWALK SURFACE, AND PUSH BUTTON IS 10" MAXIMUM FROM NEAREST EDGE OF PEDESTRIAN WALKWAY. INSTALL ACCESSIBLE PEDESTRIAN SIGNAL ON EXTENSION, IF NECESSARY, TO MEET 10" REACH REQUIREMENT. APS PUSH BUTTONS SHALL BE NON-VIBRATING TYPE WITH R10-25 SIGN. WARNING MESSAGE SHALL BE "YELLOW LIGHTS ARE FLASHING" REPEATED TWICE.
6. WHEN INSTALLING A NEW POLE OR CONDUIT IN AN EXISTING SIDEWALK, THE CONTRACTOR SHALL REMOVE AND REPLACE THE ENTIRE CONCRETE FLAG (PANEL) FROM SCORE LINE TO SCORE LINE WITHIN THE AREA OF CONSTRUCTION.

CONSTRUCTION NOTES

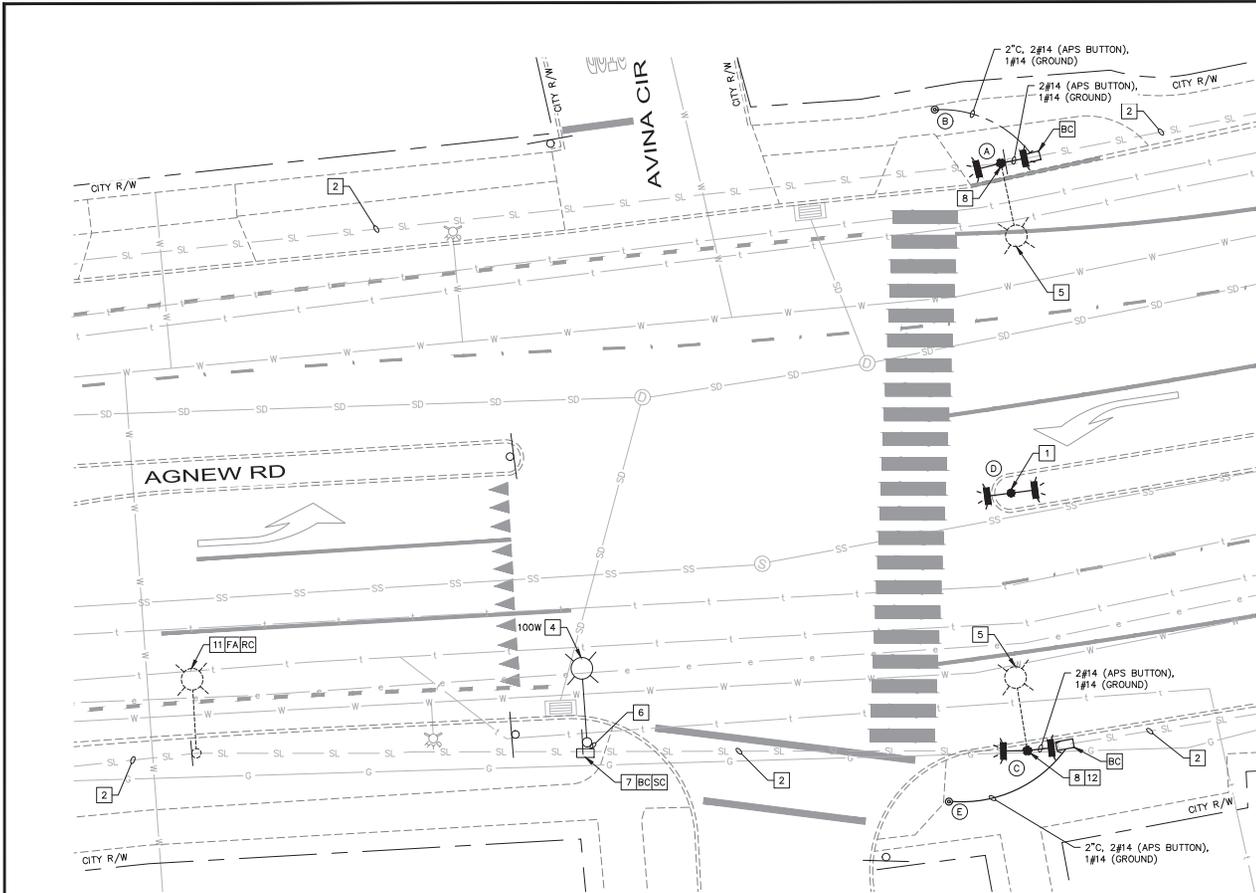
- 1 FURNISH AND INSTALL RRFB ASSEMBLY PER DETAIL 'A', SHEET TS-14.
- 2 PROTECT EXISTING CONDUCTORS IN PLACE AND MAINTAIN EXISTING CIRCUIT.
- 4 FURNISH AND INSTALL NEW LEOTREX GREENCOBRA LUMINAIRE OR APPROVED EQUAL ON NEW POLE. WATTAGE PER PLAN.
- 5 EXISTING LUMINAIRE AND POLE TO REMAIN IN PLACE.
- 6 FURNISH AND INSTALL 1 1/2" WITH 2#8 (APS) AND 1#8 (GROUND). CONTRACTOR SHALL VERIFY SIZE MATCHES EXISTING STREET LIGHT CONDUCTORS AND NOTIFY ENGINEER OF ANY DISCREPANCIES.
- 7 FURNISH AND INSTALL NO. 6 PULL BOX.
- BC INSTALL PULL BOX INTO EXISTING CONDUIT.
- SC SPlice NEW TO EXISTING CONDUCTORS.

POLE & EQUIPMENT SCHEDULE					
LOCATION	STANDARD		APS	FRONT SIDE SIGNS	BACK SIDE SIGNS
	TYPE	HEIGHT			
A	PELCO SPUN SCH 40 PB-5100	14'		S1-1 W16-7P	S1-1 W16-7P
B	APS POST	-	⦿	-	-
C	PELCO SPUN SCH 40 PB-5100	14'	⦿	S1-1 W16-7P	S1-1 W16-7P

65% DESIGN PLANS
NOT FOR CONSTRUCTION
JANUARY 2026



	<p>4637 Chabot Drive, Suite 200 Tel. No. (925) 398-4840 Pleasanton, California 94588</p>	PROJECT No. CE 24-25-07	CITY OF SANTA CLARA PUBLIC WORKS DEPARTMENT
		ACCOUNT No. _____ DATE No. _____ REVISION BY _____	APPROVED BY _____ TITLE _____ DATE 01/07/2026 DWG No. TS-02
		UNCONTROLLED CROSSWALKS IMPROVEMENTS - PHASE II RRFB - AGNEW RD & DAVIS ST	DESIGNED BY: JTK 01/26 DRAWN BY: AMT 01/26 CHECKED BY: SAR 01/26 AS BUILT BY: _____



GENERAL NOTES (TS SHEETS ONLY)

1. THE CONTRACTOR SHALL FURNISH AND INSTALL ALL MATERIALS, EQUIPMENT, LABOR, AND INCIDENTAL WORK FOR THE PROJECT.
2. EXCEPT AS OTHERWISE SPECIFIED, ALL WORK AND MATERIALS SHALL CONFORM TO AND BE IN ACCORDANCE WITH THE APPLICABLE PROVISIONS OF THE CITY OF SANTA CLARA DESIGN STANDARDS, STANDARD SPECIFICATIONS, AND STANDARD PLANS; AND 2025 CALTRANS STANDARD PLANS AND SPECIFICATIONS, THESE PLANS, AND THE CONTRACT SPECIFICATIONS. IN CASE OF CONFLICT BETWEEN SAID DOCUMENTS, THE MOST STRINGENT REQUIREMENT SHALL APPLY AS DETERMINED BY THE CITY ENGINEER.
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6. WHEN INSTALLING A NEW POLE OR CONDUIT IN AN EXISTING SIDEWALK, THE CONTRACTOR SHALL REMOVE AND REPLACE THE ENTIRE CONCRETE FLAG (PANEL) FROM SCORE LINE TO SCORE LINE WITHIN THE AREA OF CONSTRUCTION.

CONSTRUCTION NOTES

- 1 FURNISH AND INSTALL RRFB ASSEMBLY PER DETAIL 'A', SHEET TS-14.
- 2 PROTECT EXISTING CONDUCTORS IN PLACE AND MAINTAIN EXISTING CIRCUIT.
- 4 FURNISH AND INSTALL NEW LEOTREX GREENCOBRA LUMINAIRE OR APPROVED EQUAL ON NEW POLE. WATTAGE PER PLAN.
- 5 EXISTING LUMINAIRE AND POLE TO REMAIN IN PLACE.
- 6 FURNISH AND INSTALL 1 1/2" WITH 2#8 (APS) AND 1#8 (GROUND). CONTRACTOR SHALL VERIFY SIZE MATCHES EXISTING STREET LIGHT CONDUCTORS AND NOTIFY ENGINEER OF ANY DISCREPANCIES.
- 7 FURNISH AND INSTALL NO. 6 PULL BOX.
- 8 FURNISH AND INSTALL RRFB ASSEMBLY PER DETAIL 'B', SHEET TS-14.
- 11 SPLICE CONDUCTORS FOR EXISTING CIRCUIT TO MAINTAIN INTEGRITY.
- 12 PROTECT IN PLACE EXISTING SIGNS ON EXISTING LUMINAIRE POLE.
- BC INSTALL PULL BOX INTO EXISTING CONDUIT.
- FA FOUNDATION TO BE ABANDONED.
- RC EQUIPMENT OR MATERIAL TO BE REMOVED AND BECOME THE PROPERTY OF THE CONTRACTOR.
- SC SPLICE NEW TO EXISTING CONDUCTORS.

POLE & EQUIPMENT SCHEDULE					
LOCATION	STANDARD		APS	FRONT SIDE SIGNS	BACK SIDE SIGNS
	TYPE	HEIGHT			
A	CALTRANS TYPE 15 POLE	30'	E	S1-1 W16-7P	S1-1 W16-7P
B	APS POST	-	⊙	-	-
C	CALTRANS TYPE 15 POLE	30'	E	S1-1 W16-7P	S1-1 W16-7P
D	PELCO SPUN SCH 40 PB-S100	14'	-	S1-1 W16-7P	S1-1 W16-7P
E	APS POST	-	⊙	-	-

E - DENOTES EXISTING EQUIPMENT

GRAPHIC SCALE—USE WHEN PLAN IS REDUCED



65% DESIGN PLANS
NOT FOR CONSTRUCTION
JANUARY 2026



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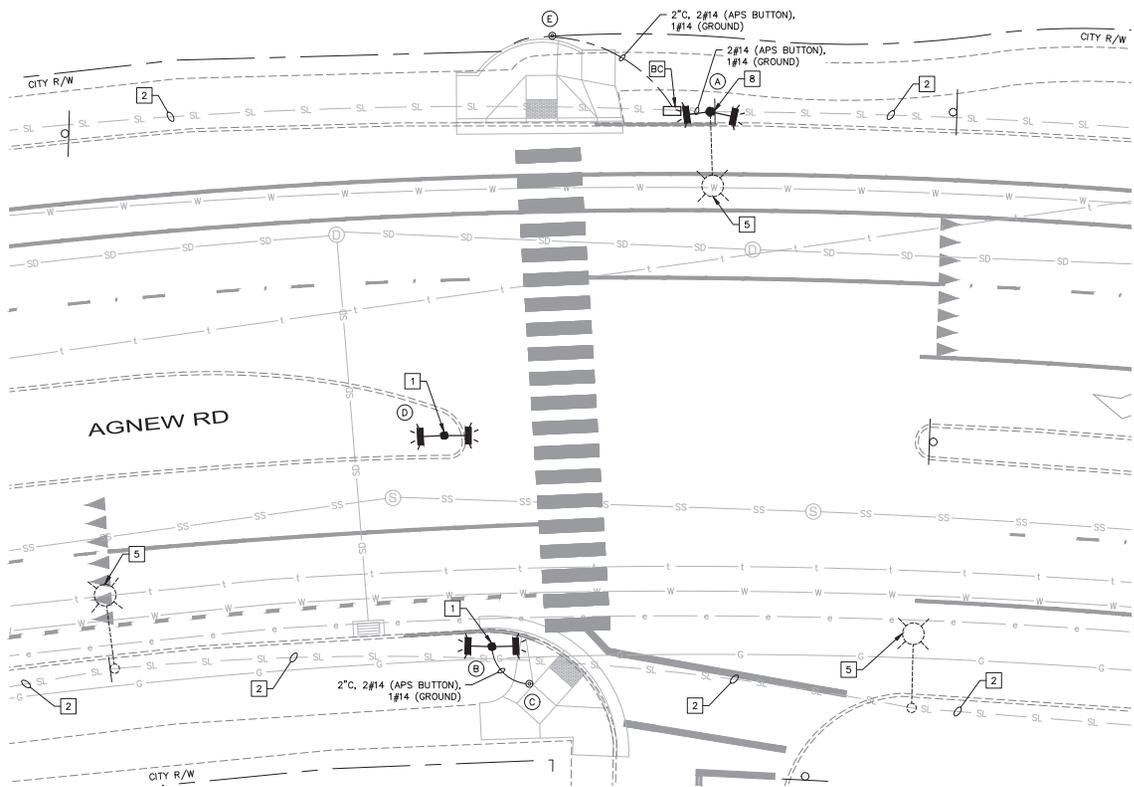
PROJECT No. CE 24-25-07

ACCOUNT No.			
DATE	No.	REVISION	BY

CITY OF SANTA CLARA
PUBLIC WORKS DEPARTMENT

UNCONTROLLED CROSSWALKS
IMPROVEMENTS - PHASE II
RRFB - AGNEW RD & AVINA CIR

APPROVED BY		TITLE		DWG No.
DATE	INITIAL	DATE	INITIAL	
01/07/2026	JTK	01/26	AMT	TS-03
DESIGNED BY	JTK	01/26		TRACING No. 12,444-D
DRAWN BY	AMT	01/26		
CHECKED BY	SAR	01/26		
AS BUILT BY				



GENERAL NOTES (TS SHEETS ONLY)

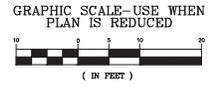
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6. WHEN INSTALLING A NEW POLE OR CONDUIT IN AN EXISTING SIDEWALK, THE CONTRACTOR SHALL REMOVE AND REPLACE THE ENTIRE CONCRETE FLAG (PANEL) FROM SCORE LINE TO SCORE LINE WITHIN THE AREA OF CONSTRUCTION.

CONSTRUCTION NOTES

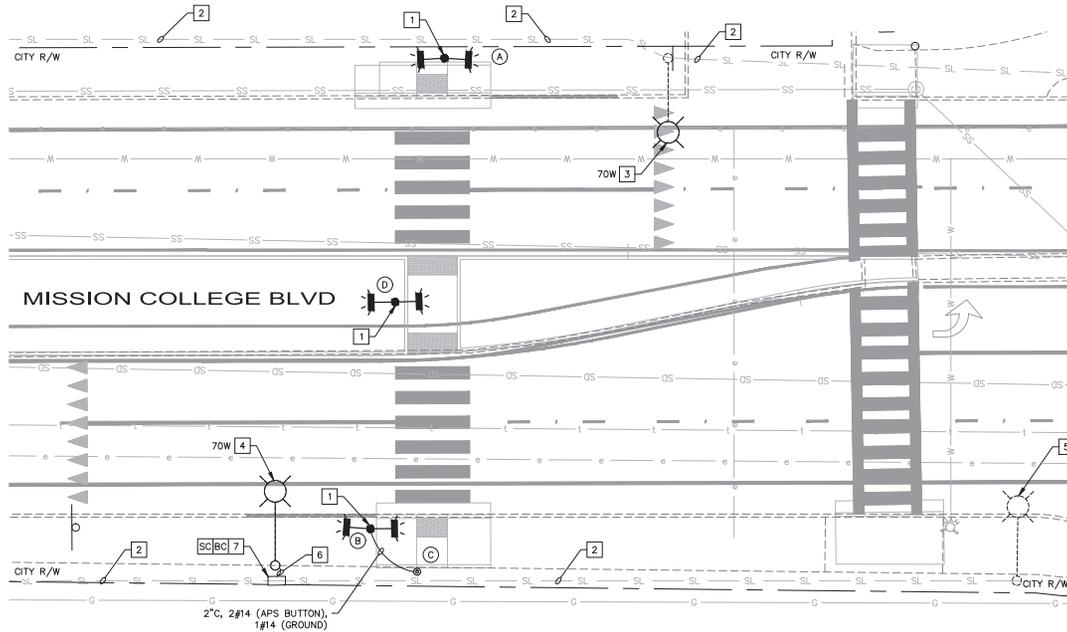
- 1 FURNISH AND INSTALL RRFB ASSEMBLY PER DETAIL 'A', SHEET TS-14.
- 2 PROTECT EXISTING CONDUCTORS IN PLACE AND MAINTAIN EXISTING CIRCUIT.
- 5 EXISTING LUMINAIRE AND POLE TO REMAIN IN PLACE.
- 8 FURNISH AND INSTALL RRFB ASSEMBLY PER DETAIL 'B', SHEET TS-14.
- BC INSTALL PULL BOX INTO EXISTING CONDUIT.

POLE & EQUIPMENT SCHEDULE					
LOCATION	STANDARD		APS	FRONT SIDE SIGNS	BACK SIDE SIGNS
	TYPE	HEIGHT			
A	PELCO SPUN SCH 40 PB-S100	14'	☉	S1-1 W16-7P	S1-1 W16-7P
B	PELCO SPUN SCH 40 PB-S100	14'	-	S1-1 W16-7P	S1-1 W16-7P
C	APS POST	-	☉	-	-
D	PELCO SPUN SCH 40 PB-S100	14'	-	S1-1 W16-7P	S1-1 W16-7P
E	APS POST	-	☉	-	-

65% DESIGN PLANS
NOT FOR CONSTRUCTION
JANUARY 2026



	<p>4637 Chabot Drive, Suite 200 Tel. No. (925) 398-4840 Pleasanton, California 94588</p>	PROJECT No. CE 24-25-07	CITY OF SANTA CLARA PUBLIC WORKS DEPARTMENT
		ACCOUNT No. _____ DATE No. _____ REVISION BY _____	APPROVED BY _____ TITLE _____ DATE 01/07/2026 DWG No. TS-04
		UNCONTROLLED CROSSWALKS IMPROVEMENTS - PHASE II	DESIGNED BY JTK 01/26 DRAWN BY AMT 01/26 CHECKED BY SAR 01/26 AS BUILT BY _____
		RRFB - AGNEW RD & GARRITY WAY (WEST OF)	TRACING No. 12,444-D



GENERAL NOTES (TS SHEETS ONLY)

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5. ALL PUSH BUTTONS ARE TO BE MOUNTED SUCH THAT HEIGHT FROM SIDEWALK SURFACE AND CENTER OF PUSH BUTTON IS BETWEEN 40" AND 46" ABOVE NEW SIDEWALK SURFACE, AND PUSH BUTTON IS 10" MAXIMUM FROM NEAREST EDGE OF PEDESTRIAN WALKWAY. INSTALL ACCESSIBLE PEDESTRIAN SIGNAL ON EXTENSION, IF NECESSARY, TO MEET 10" REACH REQUIREMENT. APS PUSH BUTTONS SHALL BE NON-VIBRATING TYPE WITH R10-25 SIGN. WARNING MESSAGE SHALL BE "YELLOW LIGHTS ARE FLASHING" REPEATED TWICE.
6. WHEN INSTALLING A NEW POLE OR CONDUIT IN AN EXISTING SIDEWALK, THE CONTRACTOR SHALL REMOVE AND REPLACE THE ENTIRE CONCRETE FLAG (PANEL) FROM SCORE LINE TO SCORE LINE WITHIN THE AREA OF CONSTRUCTION.

CONSTRUCTION NOTES

- 1 FURNISH AND INSTALL RRFB ASSEMBLY PER DETAIL 'A', SHEET TS-14.
- 2 PROTECT EXISTING CONDUCTORS IN PLACE AND MAINTAIN EXISTING CIRCUIT.
- 3 FURNISH AND INSTALL NEW LEOTEK GREENCOBRA LUMINAIRE OR APPROVED EQUAL ON EXISTING POLE. WATTAGE PER PLAN.
- 4 FURNISH AND INSTALL NEW LEOTEK GREENCOBRA LUMINAIRE OR APPROVED EQUAL ON NEW POLE. WATTAGE PER PLAN.
- 5 EXISTING LUMINAIRE AND POLE TO REMAIN IN PLACE.
- 6 FURNISH AND INSTALL 1 1/2" WITH 2#8 (APS) AND 1#8 (GROUND). CONTRACTOR SHALL VERIFY SIZE MATCHES EXISTING STREET LIGHT CONDUCTORS AND NOTIFY ENGINEER OF ANY DISCREPANCIES.
- 7 FURNISH AND INSTALL NO. 6 PULL BOX.
- BC INSTALL PULL BOX INTO EXISTING CONDUIT.
- SC SPLICE NEW TO EXISTING CONDUCTORS.

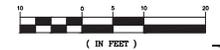
POLE & EQUIPMENT SCHEDULE

LOCATION	STANDARD		APS	FRONT SIDE SIGNS	BACK SIDE SIGNS
	TYPE	HEIGHT			
A	PELCO SPUN SCH 40 PB-5100	14'	☉	S1-1 W16-7P	S1-1 W16-7P
B	PELCO SPUN SCH 40 PB-5100	14'		S1-1 W16-7P	S1-1 W16-7P
C	APS POST	-	☉	-	-
D	PELCO SPUN SCH 40 PB-5100	14'	☉	S1-1 W16-7P	S1-1 W16-7P

65% DESIGN PLANS
NOT FOR CONSTRUCTION
JANUARY 2026



GRAPHIC SCALE—USE WHEN PLAN IS REDUCED



Kimley»Horn

4637 Chabot Drive, Suite 200 Tel. No. (925) 398-4840
Pleasanton, California 94588

PROJECT No. CE 24-25-07

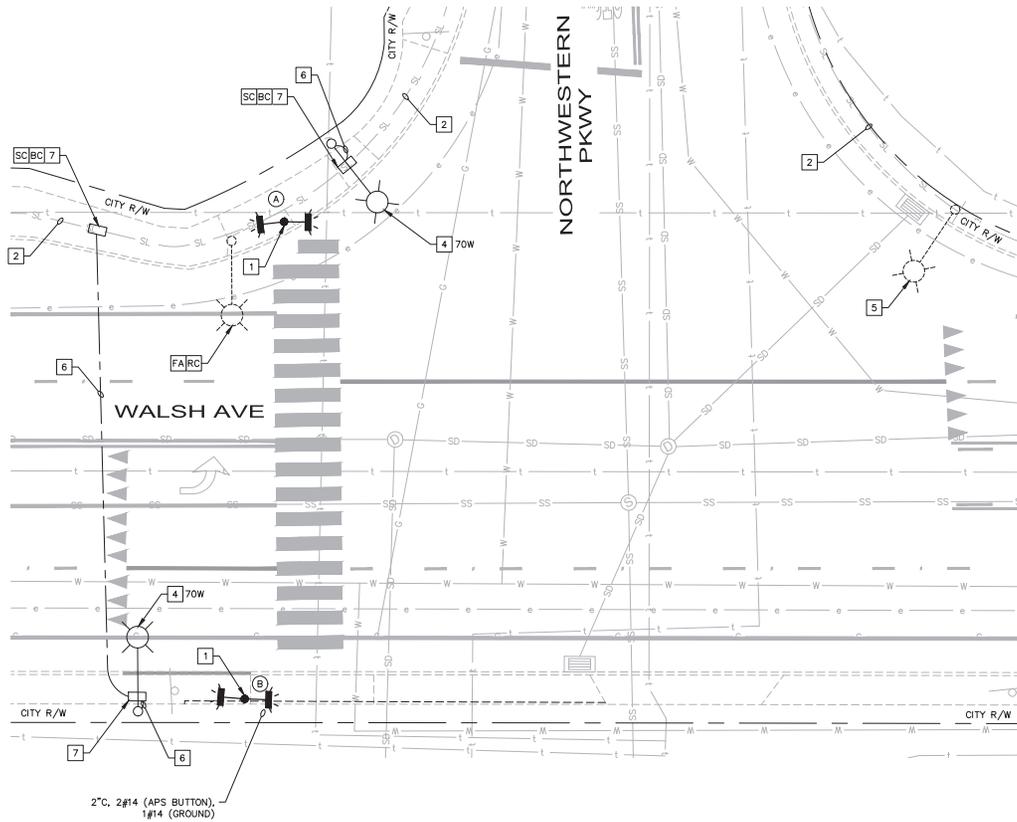
ACCOUNT No.

DATE No. REVISION BY

CITY OF SANTA CLARA
PUBLIC WORKS DEPARTMENT

UNCONTROLLED CROSSWALKS
IMPROVEMENTS - PHASE II
RRFB - MISSION COLLEGE BLVD & OUR LADY'S WAY
(SOUTH OF)

APPROVED BY		TITLE		DWG No. TS-05
DATE	01/07/2026			
DESIGNED BY	JTK	INITIAL	DATE	TRACING No. 12,444-D
DRAWN BY	AMT		01/26	
CHECKED BY	SAR		01/26	



GENERAL NOTES (TS SHEETS ONLY)

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2. EXCEPT AS OTHERWISE SPECIFIED, ALL WORK AND MATERIALS SHALL CONFORM TO AND BE IN ACCORDANCE WITH THE APPLICABLE PROVISIONS OF THE CITY OF SANTA CLARA DESIGN STANDARDS, STANDARD SPECIFICATIONS, AND STANDARD PLANS; AND 2025 CALTRANS STANDARD PLANS AND SPECIFICATIONS, THESE PLANS, AND THE CONTRACT SPECIFICATIONS. IN CASE OF CONFLICT BETWEEN SAID DOCUMENTS, THE MOST STRINGENT REQUIREMENT SHALL APPLY AS DETERMINED BY THE CITY ENGINEER.
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4. FURNISH AND INSTALL ACCESSIBLE PEDESTRIAN SIGNAL PER MANUFACTURER'S RECOMMENDATION AND THE STATE STANDARD PLAN ES-50 "DETAIL A".
5. ALL PUSH BUTTONS ARE TO BE MOUNTED SUCH THAT HEIGHT FROM SIDEWALK SURFACE AND CENTER OF PUSH BUTTON IS BETWEEN 40" AND 46" ABOVE NEW SIDEWALK SURFACE, AND PUSH BUTTON IS 10" MAXIMUM FROM NEAREST EDGE OF PEDESTRIAN WALKWAY. INSTALL ACCESSIBLE PEDESTRIAN SIGNAL ON EXTENSION, IF NECESSARY, TO MEET 10" REACH REQUIREMENT. APS PUSH BUTTONS SHALL BE NON-VIBRATING TYPE WITH R10-25 SIGN. WARNING MESSAGE SHALL BE "YELLOW LIGHTS ARE FLASHING" REPEATED TWICE.
6. WHEN INSTALLING A NEW POLE OR CONDUIT IN AN EXISTING SIDEWALK, THE CONTRACTOR SHALL REMOVE AND REPLACE THE ENTIRE CONCRETE FLAG (PANEL) FROM SCORE LINE TO SCORE LINE WITHIN THE AREA OF CONSTRUCTION.

CONSTRUCTION NOTES

- 1 FURNISH AND INSTALL RRFB ASSEMBLY PER DETAIL 'A', SHEET TS-14.
- 2 PROTECT EXISTING CONDUCTORS IN PLACE AND MAINTAIN EXISTING CIRCUIT.
- 4 FURNISH AND INSTALL NEW LEOTREK GREENCORRA LUMINAIRE OR APPROVED EQUAL ON NEW POLE. WATTAGE PER PLAN.
- 5 EXISTING LUMINAIRE AND POLE TO REMAIN IN PLACE.
- 6 FURNISH AND INSTALL 1 1/2" C WITH 2#8 (APS) AND 1#8 (GROUND). CONTRACTOR SHALL VERIFY SIZE MATCHES EXISTING STREET LIGHT CONDUCTORS AND NOTIFY ENGINEER OF ANY DISCREPANCIES.
- 7 FURNISH AND INSTALL NO. 6 PULL BOX.
- BC INSTALL PULL BOX INTO EXISTING CONDUIT.
- FA FOUNDATION TO BE ABANDONED.
- RC EQUIPMENT OR MATERIAL TO BE REMOVED AND BECOME THE PROPERTY OF THE CONTRACTOR.
- SC SPUCE NEW TO EXISTING CONDUCTORS.

POLE & EQUIPMENT SCHEDULE

LOCATION	STANDARD		APS	FRONT SIDE SIGNS	BACK SIDE SIGNS
	TYPE	HEIGHT			
A	PELCO SPUN SCH 40 PB-5100	14'		S1-1 W16-7P	S1-1 W16-7P
B	PELCO SPUN SCH 40 PB-5100	14'	☉	S1-1 W16-7P	S1-1 W16-7P

65% DESIGN PLANS
NOT FOR CONSTRUCTION
JANUARY 2026



GRAPHIC SCALE—USE WHEN
PLAN IS REDUCED



Kimley»Horn

4637 Chabot Drive, Suite 200 Tel. No. (925) 398-4840
Pleasanton, California 94588

PROJECT No. CE 24-25-07

ACCOUNT No.

DATE No. REVISION BY

CITY OF SANTA CLARA
PUBLIC WORKS DEPARTMENT

UNCONTROLLED CROSSWALKS
IMPROVEMENTS - PHASE II

RRFB - WALSH AVE & NORTHWESTERN PKWY

APPROVED BY

TITLE

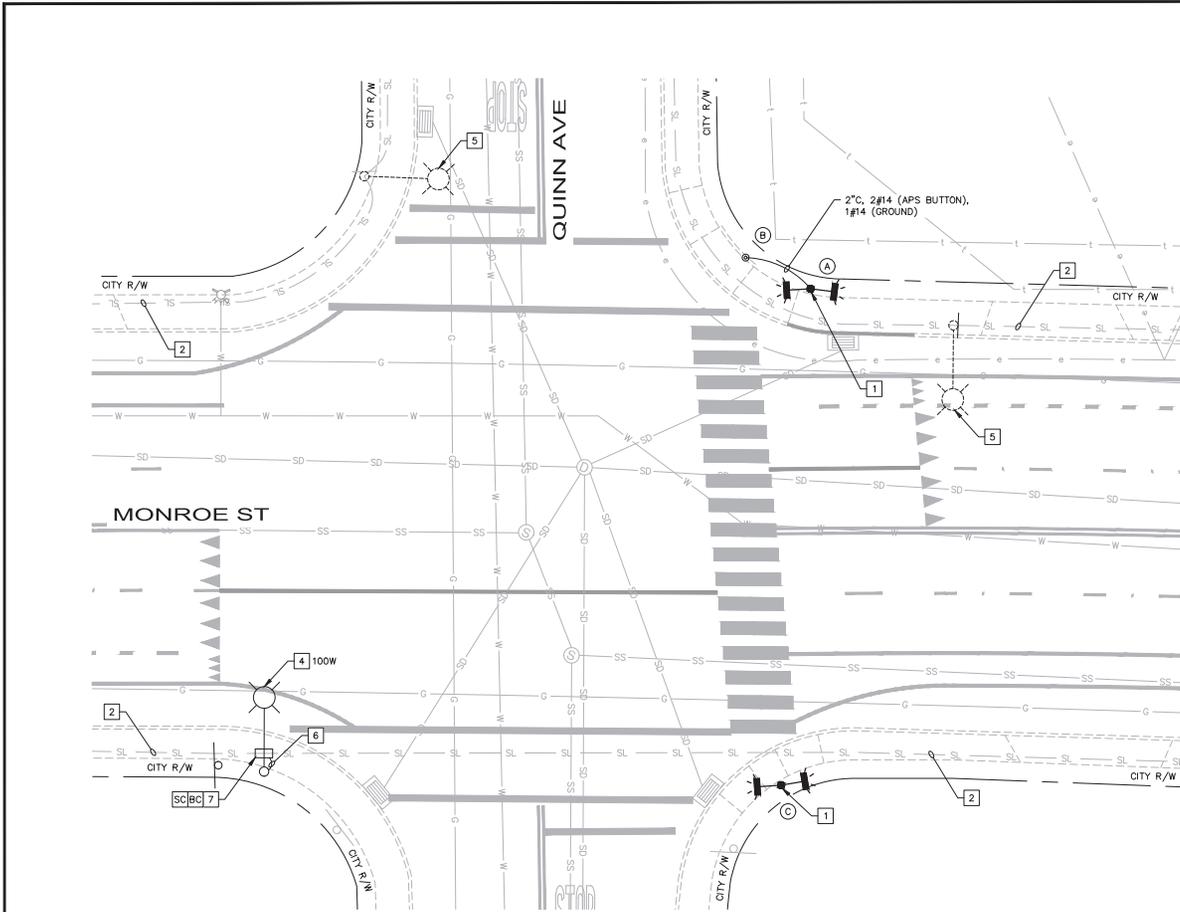
DATE 01/07/2026 DWG No. TS-06

DESIGNED BY JTK 01/26 TRACING No. 12,444-D

DRAWN BY AMT 01/26

CHECKED BY SAR 01/26

AS BUILT BY



GENERAL NOTES (TS SHEETS ONLY)

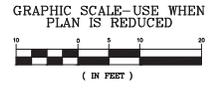
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4. FURNISH AND INSTALL ACCESSIBLE PEDESTRIAN SIGNAL PER MANUFACTURER'S RECOMMENDATION AND THE STATE STANDARD PLAN ES-SC "DETAIL A".
5. ALL PUSH BUTTONS ARE TO BE MOUNTED SUCH THAT HEIGHT FROM SIDEWALK SURFACE AND CENTER OF PUSH BUTTON IS BETWEEN 40" AND 46" ABOVE NEW SIDEWALK SURFACE, AND PUSH BUTTON IS 10" MAXIMUM FROM NEAREST EDGE OF PEDESTRIAN WALKWAY. INSTALL ACCESSIBLE PEDESTRIAN SIGNAL ON EXTENSION, IF NECESSARY, TO MEET 10" REACH REQUIREMENT. APS PUSH BUTTONS SHALL BE NON-VIBRATING TYPE WITH R10-25 SIGN. WARNING MESSAGE SHALL BE "YELLOW LIGHTS ARE FLASHING" REPEATED TWICE.
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CONSTRUCTION NOTES

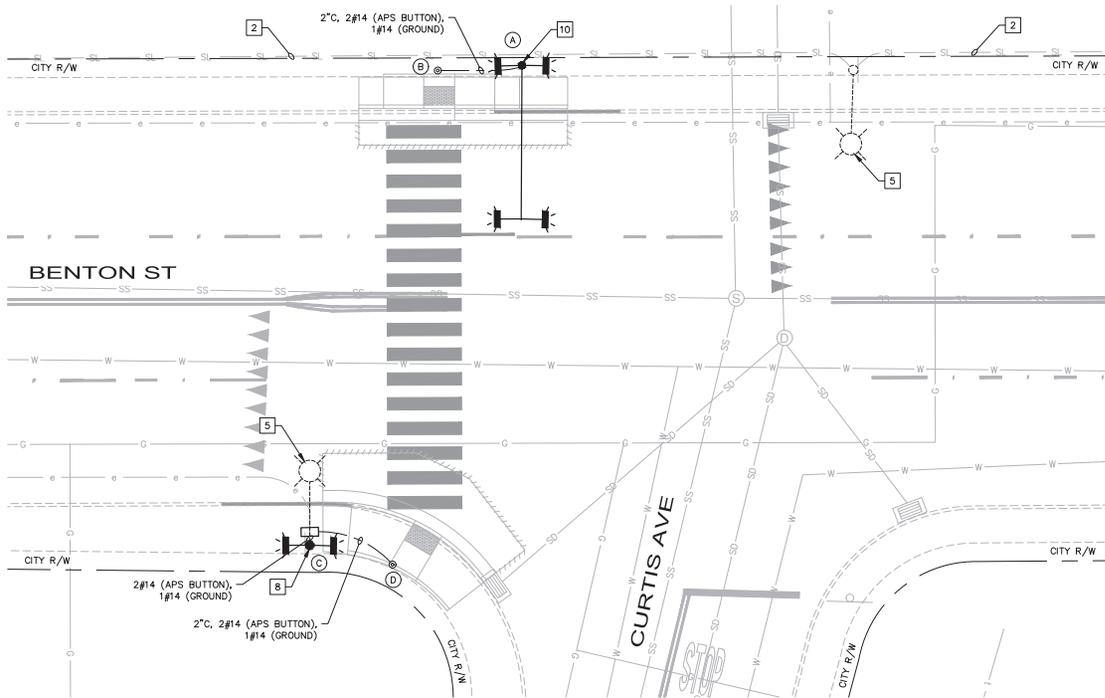
- 1 FURNISH AND INSTALL RFB ASSEMBLY PER DETAIL 'A', SHEET TS-14.
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- 4 FURNISH AND INSTALL NEW LEOTREK GREENCOBRA LUMINAIRE OR APPROVED EQUAL ON NEW POLE. WATTAGE PER PLAN.
- 5 EXISTING LUMINAIRE AND POLE TO REMAIN IN PLACE.
- 6 FURNISH AND INSTALL 1 1/2" x 2" WITH 2#6 (APS) AND 1#6 (GROUND). CONTRACTOR SHALL VERIFY SIZE MATCHES EXISTING STREET LIGHT CONDUCTORS AND NOTIFY ENGINEER OF ANY DISCREPANCIES.
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POLE & EQUIPMENT SCHEDULE					
LOCATION	STANDARD		APS	FRONT SIDE SIGNS	BACK SIDE SIGNS
	TYPE	HEIGHT			
A	PELCO SPUN SCH 40 PB-5100	14'		S1-1 W16-7P	S1-1 W16-7P
B	APS POST	-	●	-	-
C	PELCO SPUN SCH 40 PB-5100	14'	●	S1-1 W16-7P	S1-1 W16-7P

65% DESIGN PLANS
NOT FOR CONSTRUCTION
JANUARY 2026



	<p>4637 Chabot Drive, Suite 200 Tel. No. (925) 398-4840 Pleasanton, California 94588</p>	PROJECT No. CE 24-25-07	CITY OF SANTA CLARA PUBLIC WORKS DEPARTMENT UNCONTROLLED CROSSWALKS IMPROVEMENTS - PHASE II RFB - MONROE ST & QUINN AVE	APPROVED BY			
		ACCOUNT No.		TITLE	DATE	INITIAL	DATE
		DATE	No.	REVISION	BY	DESIGNED BY JTK 01/26	TRACING No. 12,444-D
						DRAWN BY AMT 01/26	
						CHECKED BY SAR 01/26	
						AS BUILT BY	44 52



GENERAL NOTES (TS SHEETS ONLY)

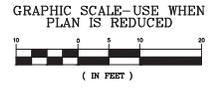
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CONSTRUCTION NOTES

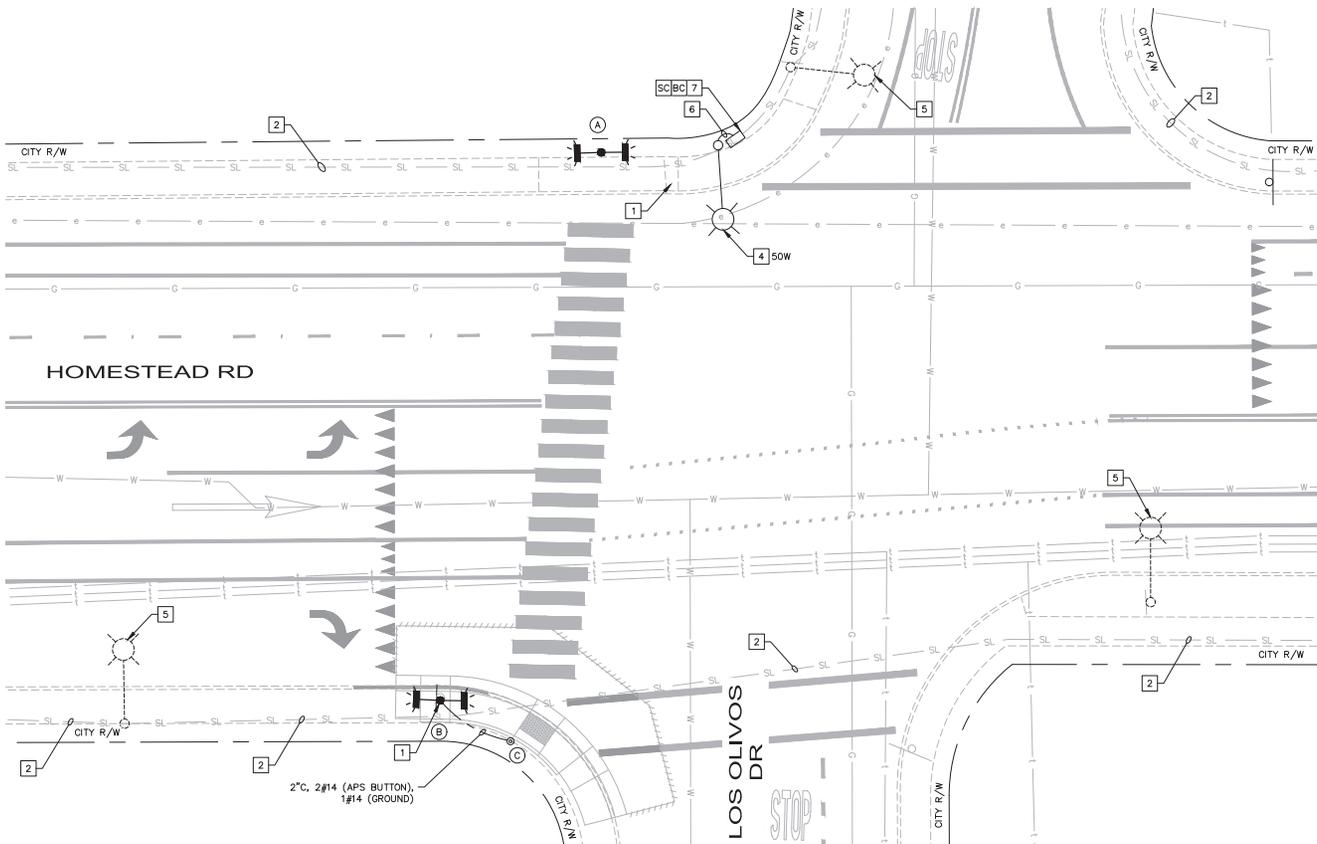
- 2 PROTECT EXISTING CONDUCTORS IN PLACE AND MAINTAIN EXISTING CIRCUIT.
- 5 EXISTING LUMINAIRE AND POLE TO REMAIN IN PLACE.
- 8 FURNISH AND INSTALL RRFB ASSEMBLY PER DETAIL 'B', SHEET TS-14.
- 10 FURNISH AND INSTALL RRFB ASSEMBLY PER DETAIL 'C', SHEET TS-15.

POLE & EQUIPMENT SCHEDULE					
LOCATION	STANDARD		APS	FRONT SIDE SIGNS	BACK SIDE SIGNS
	TYPE	HEIGHT			
A	CALTRANS 18-3-100	17'		S1-1 W16-7P	S1-1 W16-7P
B	APS POST	-	⊙	-	-
C	PELCO SPUN SCH 40 PB-S100	14'		S1-1 W16-7P	S1-1 W16-7P
D	APS POST	-	⊙	-	-

65% DESIGN PLANS
NOT FOR CONSTRUCTION
JANUARY 2026



	<p>4637 Chabot Drive, Suite 200 Tel. No. (925) 398-4840 Pleasanton, California 94588</p>	PROJECT No. CE 24-25-07	CITY OF SANTA CLARA PUBLIC WORKS DEPARTMENT	APPROVED BY
		ACCOUNT No.		TITLE
		DATE No. REVISION BY	UNCONTROLLED CROSSWALKS IMPROVEMENTS - PHASE II	DATE 01/07/2026
			RRFB - BENTON ST & CURTIS AVE	INITIAL DATE DWG No. TS-08
				DESIGNED BY JTK 01/26 TRACING No. 12,444-D
				DRAWN BY AMT 01/26
				CHECKED BY SAR 01/26
				AS BUILT BY



GENERAL NOTES (TS SHEETS ONLY)

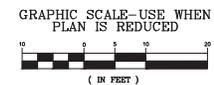
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- 7 FURNISH AND INSTALL NO. 6 PULL BOX.
- BC INSTALL PULL BOX INTO EXISTING CONDUIT.
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POLE & EQUIPMENT SCHEDULE					
LOCATION	STANDARD		APS	FRONT SIDE SIGNS	BACK SIDE SIGNS
	TYPE	HEIGHT			
A	PELCO SPUN SCH 40 PB-5100	14'		S1-1 W16-7P	S1-1 W16-7P
B	PELCO SPUN SCH 40 PB-5100	14'	⊙	S1-1 W16-7P	S1-1 W16-7P
C	APS POST	-	⊙	-	-

65% DESIGN PLANS
NOT FOR CONSTRUCTION
JANUARY 2026

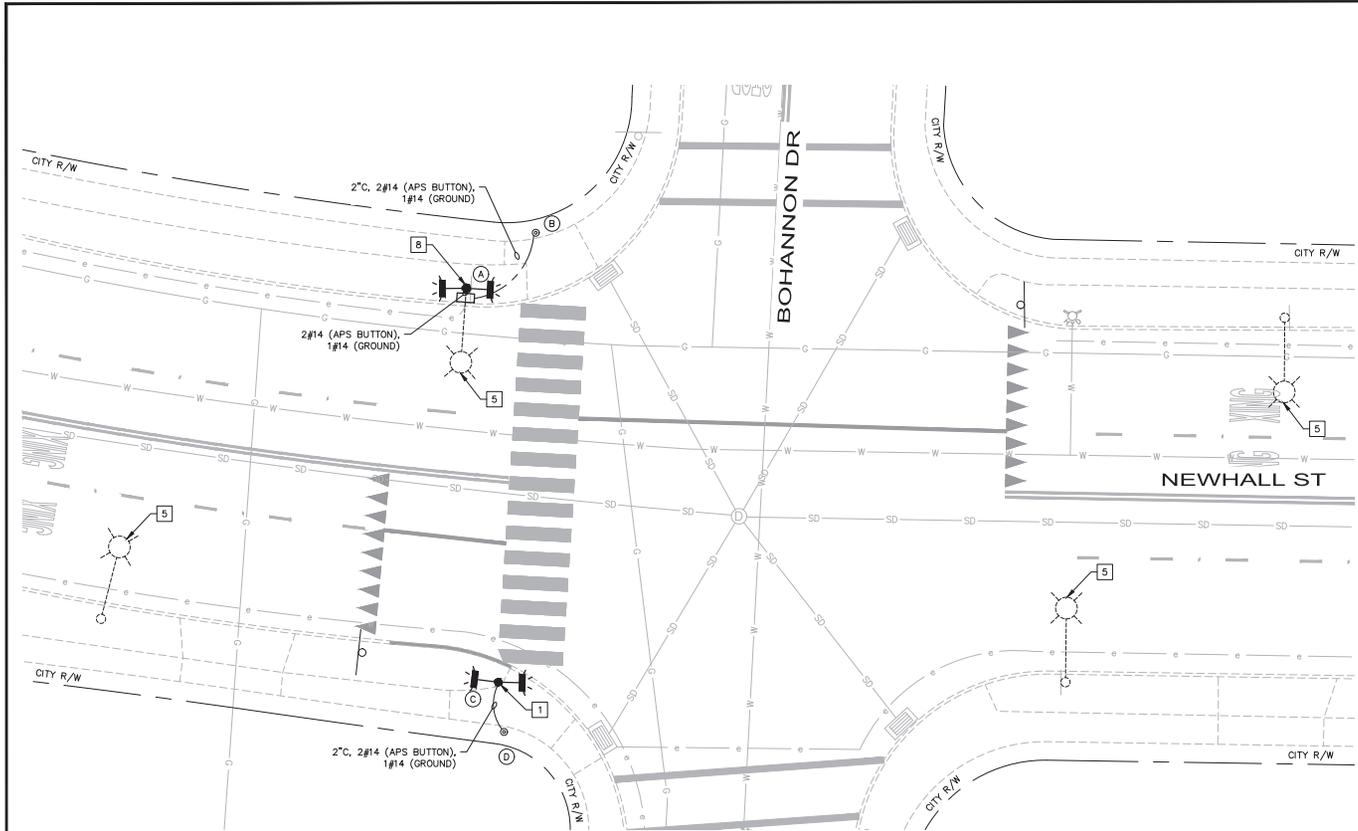


Kimley»Horn

4637 Chabot Drive, Suite 200 Tel. No. (925) 398-4840
Pleasanton, California 94588

PROJECT No. CE 24-25-07			
ACCOUNT No.			
DATE	No.	REVISION	BY

CITY OF SANTA CLARA		APPROVED BY	
PUBLIC WORKS DEPARTMENT		TITLE	
UNCONTROLLED CROSSWALKS IMPROVEMENTS - PHASE II		DATE 01/07/2026	DWG No. TS-09
DESIGNED BY	JTK	DATE	01/26
DRAWN BY	AMT	DATE	01/26
CHECKED BY	SAR	DATE	01/26
AS BUILT BY			
RRFB - HOMESTEAD RD & LOS OLIVOS DR		TRACING No. 12,444-D	



GENERAL NOTES (TS SHEETS ONLY)

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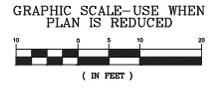
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- 8 FURNISH AND INSTALL RRFB ASSEMBLY PER DETAIL 'B', SHEET TS-14.

POLE & EQUIPMENT SCHEDULE					
LOCATION	STANDARD		APS	FRONT SIDE SIGNS	BACK SIDE SIGNS
	TYPE	HEIGHT			
A	CALTRANS TYPE 15 POLE E	30' E		S1-1 W16-7P	S1-1 W16-7P
B	APS POST	-	⊙	-	-
C	PELCO SPUN SCH 40 PB-S100	14'		S1-1 W16-7P	S1-1 W16-7P
D	APS POST	-	⊙	-	-

E - DENOTES EXISTING EQUIPMENT

65% DESIGN PLANS
NOT FOR CONSTRUCTION
JANUARY 2026



Kimley»Horn

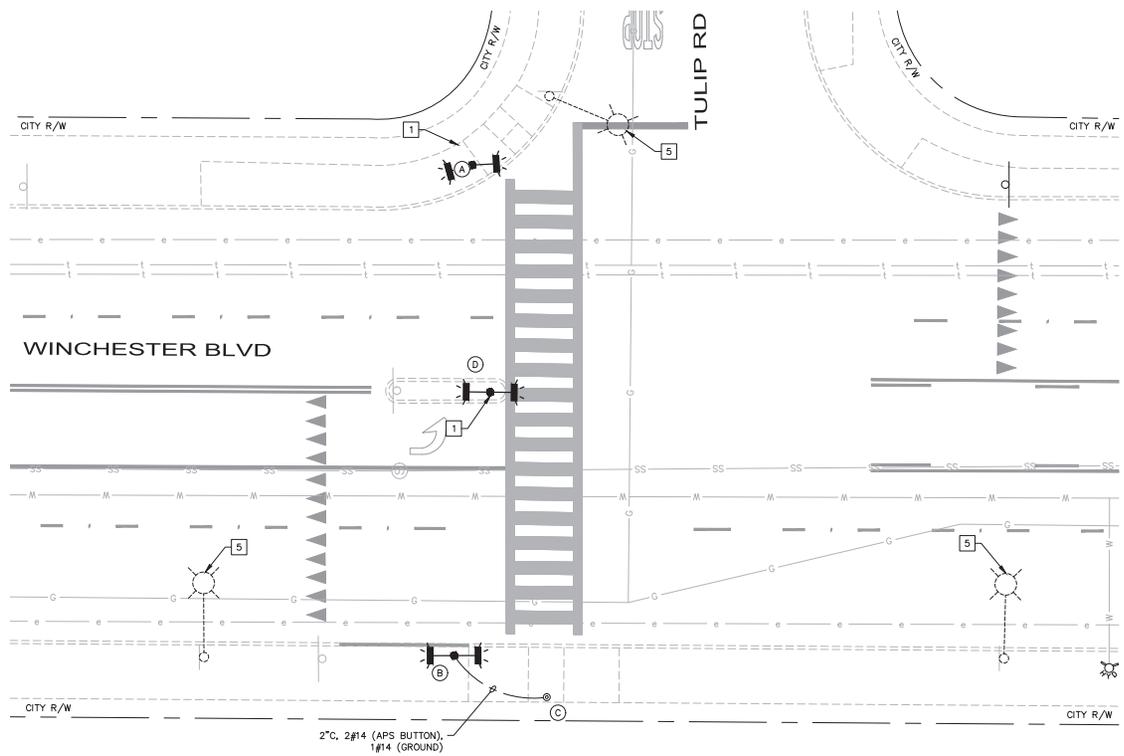
4637 Chabot Drive, Suite 200 Tel. No. (925) 398-4840
Pleasanton, California 94588

PROJECT No. CE 24-25-07			
ACCOUNT No.			
DATE	No.	REVISION	BY

CITY OF SANTA CLARA
PUBLIC WORKS DEPARTMENT

UNCONTROLLED CROSSWALKS
IMPROVEMENTS - PHASE II
RRFB - NEWHALL ST & BOHANNON DR

APPROVED BY		TITLE	
DATE 01/07/2026	INITIAL	DATE	DWG No. TS-10
DESIGNED BY JTK	01/26	DRAWN BY AMT	01/26
CHECKED BY SAR	01/26	TRACING No.	12,444-D
AS BUILT BY			



GENERAL NOTES (TS SHEETS ONLY)

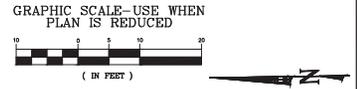
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4. FURNISH AND INSTALL ACCESSIBLE PEDESTRIAN SIGNAL PER MANUFACTURER'S RECOMMENDATION AND THE STATE STANDARD PLAN ES-5C "DETAIL A".
5. ALL PUSH BUTTONS ARE TO BE MOUNTED SUCH THAT HEIGHT FROM SIDEWALK SURFACE AND CENTER OF PUSH BUTTON IS BETWEEN 40" AND 46" ABOVE NEW SIDEWALK SURFACE, AND PUSH BUTTON IS 10" MAXIMUM FROM NEAREST EDGE OF PEDESTRIAN WALKWAY. INSTALL ACCESSIBLE PEDESTRIAN SIGNAL ON EXTENSION, IF NECESSARY, TO MEET 10" REACH REQUIREMENT. APS PUSH BUTTONS SHALL BE NON-VIBRATING TYPE WITH R10-25 SIGN. WARNING MESSAGE SHALL BE "YELLOW LIGHTS ARE FLASHING" REPEATED TWICE.
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CONSTRUCTION NOTES

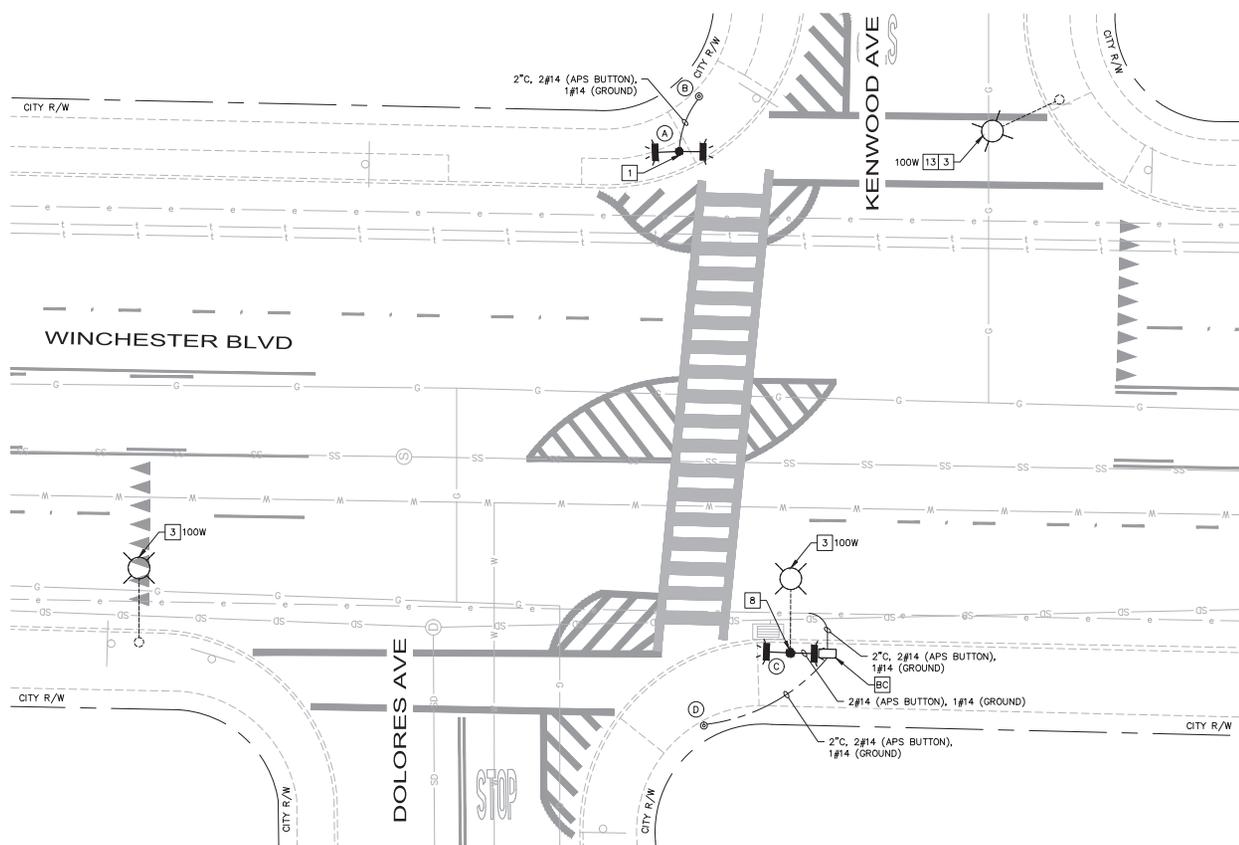
- 1 FURNISH AND INSTALL RRFB ASSEMBLY PER DETAIL 'A', SHEET TS-14.
- 5 EXISTING LUMINAIRE AND POLE TO REMAIN IN PLACE.

POLE & EQUIPMENT SCHEDULE					
LOCATION	STANDARD		APS	FRONT SIDE SIGNS	BACK SIDE SIGNS
	TYPE	HEIGHT			
A	PELCO SPUN SCH 40 PB-5100	14'	⊙	S1-1 W16-7P	S1-1 W16-7P
B	PELCO SPUN SCH 40 PB-5100	14'	⊙	S1-1 W16-7P	S1-1 W16-7P
C	APS POST	-	⊙	-	-
D	PELCO SPUN SCH 40 PB-5100	14'	⊙	S1-1 W16-7P	S1-1 W16-7P

65% DESIGN PLANS
NOT FOR CONSTRUCTION
JANUARY 2026



	<h2 style="margin: 0;">Kimley-Horn</h2> <p style="font-size: small; margin: 0;">4637 Chabot Drive, Suite 200 Tel. No. (925) 398-4840 Pleasanton, California 94588</p>	PROJECT No. CE 24-25-07	CITY OF SANTA CLARA PUBLIC WORKS DEPARTMENT
		ACCOUNT No. _____ DATE No. REVISION BY	APPROVED BY _____ TITLE _____ DATE 01/07/2026 DWG No. TS-11
		UNCONTROLLED CROSSWALKS IMPROVEMENTS - PHASE II	DESIGNED BY JTK 01/26 DRAWN BY AMT 01/26 CHECKED BY SAR 01/26 AS BUILT BY _____
		RRFB - WINCHESTER BLVD & TULIP DR	TRACING No. 12,444-D



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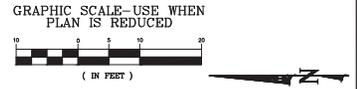
CONSTRUCTION NOTES

- 1** FURNISH AND INSTALL RRFB ASSEMBLY PER DETAIL 'A', SHEET TS-14.
- 3** FURNISH AND INSTALL NEW LEOTEK GREENCOBRA LUMINAIRE OR APPROVED EQUAL ON EXISTING POLE. WATTAGE PER PLAN.
- 8** FURNISH AND INSTALL RRFB ASSEMBLY PER DETAIL 'B', SHEET TS-14.
- 13** LUMINAIRE OWNED AND MAINTAINED BY CITY OF SAN JOSE.
- BC** INSTALL PULL BOX INTO EXISTING CONDUIT.

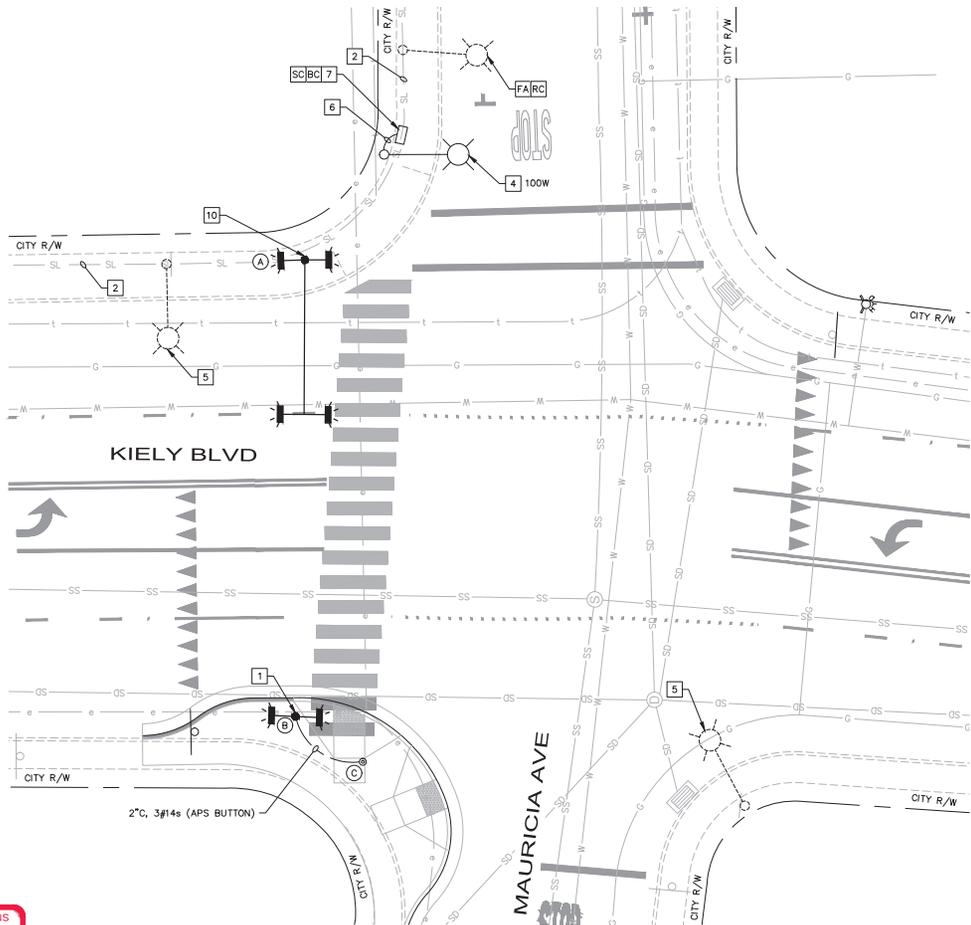
POLE & EQUIPMENT SCHEDULE					
LOCATION	STANDARD		APS	FRONT SIDE SIGNS	BACK SIDE SIGNS
	TYPE	HEIGHT			
A	PELCO SPUN SCH 40 PB-S100	14'		S1-1 W16-7P	S1-1 W16-7P
B	APS POST	-	●	-	-
C	CALTRANS TYPE 15 POLE	30'	E	S1-1 W16-7P	S1-1 W16-7P
D	APS POST	-	●	-	-

E -- DENOTES EXISTING EQUIPMENT

65% DESIGN PLANS
NOT FOR CONSTRUCTION
JANUARY 2026



	<p>4637 Chabot Drive, Suite 200 Tel. No. (925) 398-4840 Pleasanton, California 94588</p>	PROJECT No. CE 24-25-07	CITY OF SANTA CLARA PUBLIC WORKS DEPARTMENT UNCONTROLLED CROSSWALKS IMPROVEMENTS - PHASE II RRFB - WINCHESTER BLVD & KENWOOD AVE	APPROVED BY
		ACCOUNT No.		TITLE
		DATE	DATE 01/07/2026	DWG No. TS-12
		REVISION	INITIAL	DATE
			JTK	01/26
			AMT	01/26
			SAR	01/26
			CHECKED BY	DATE
			AS BUILT BY	DATE
				TRACING No. 12,444-D



GENERAL NOTES (TS SHEETS ONLY)

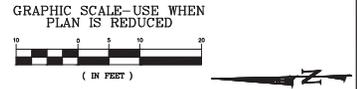
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CONSTRUCTION NOTES

- 1 FURNISH AND INSTALL RRB ASSEMBLY PER DETAIL 'A', SHEET TS-14.
 - 2 PROTECT EXISTING CONDUCTORS IN PLACE AND MAINTAIN EXISTING CIRCUIT.
 - 4 FURNISH AND INSTALL NEW LEOTREX GREENCOBRA LUMINAIRE OR APPROVED EQUAL ON NEW POLE. WATERLOG PER PLAN.
 - 5 EXISTING LUMINAIRE AND POLE TO REMAIN IN PLACE.
 - 6 FURNISH AND INSTALL 1 1/2" C WITH 2#8 (APS) AND 1#8 (GROUND). CONTRACTOR SHALL VERIFY SIZE MATCHES EXISTING STREET LIGHT CONDUCTORS AND NOTIFY ENGINEER OF ANY DISCREPANCIES.
 - 7 FURNISH AND INSTALL NO. 6 PULL BOX.
 - 10 FURNISH AND INSTALL RRB ASSEMBLY PER DETAIL 'C', SHEET TS-15.
- BC INSTALL PULL BOX INTO EXISTING CONDUIT.
 FA FOUNDATION TO BE ABANDONED.
 RC EQUIPMENT OR MATERIAL TO BE REMOVED AND BECOME THE PROPERTY OF THE CONTRACTOR.
 SC SPlice NEW TO EXISTING CONDUCTORS.

POLE & EQUIPMENT SCHEDULE					
LOCATION	STANDARD		APS	FRONT SIDE SIGNS	BACK SIDE SIGNS
	TYPE	HEIGHT			
A	CALTRANS 18-3-100	17'	☉	S1-1 W16-7P	S1-1 W16-7P
B	PELO SPUN SCH 40 FB-5100	14'		S1-1 W16-7P	S1-1 W16-7P
C	APS POST	-	☉	-	-

65% DESIGN PLANS
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			<h2 style="margin:0;">Kimley»Horn</h2> <p style="font-size: small; margin:0;">4637 Chabot Drive, Suite 200 Tel. No. (925) 398-4840 Pleasanton, California 94588</p>	PROJECT No. CE 24-25-07	<h3 style="margin:0;">CITY OF SANTA CLARA</h3> <p style="font-size: small; margin:0;">PUBLIC WORKS DEPARTMENT</p> <p style="margin:0;">UNCONTROLLED CROSSWALKS IMPROVEMENTS - PHASE II</p> <p style="margin:0;">RRFB - KIELY BLVD & MAURICIA AVE</p>	APPROVED BY
				ACCOUNT No.		DATE
				DATE	01/07/2026	DWG No. TS-13
				DESIGNED BY	JTK	01/26
				DRAWN BY	AMT	01/26
				CHECKED BY	SAR	01/26
				AS BUILT BY		

SIGN LEGEND:



R10-25



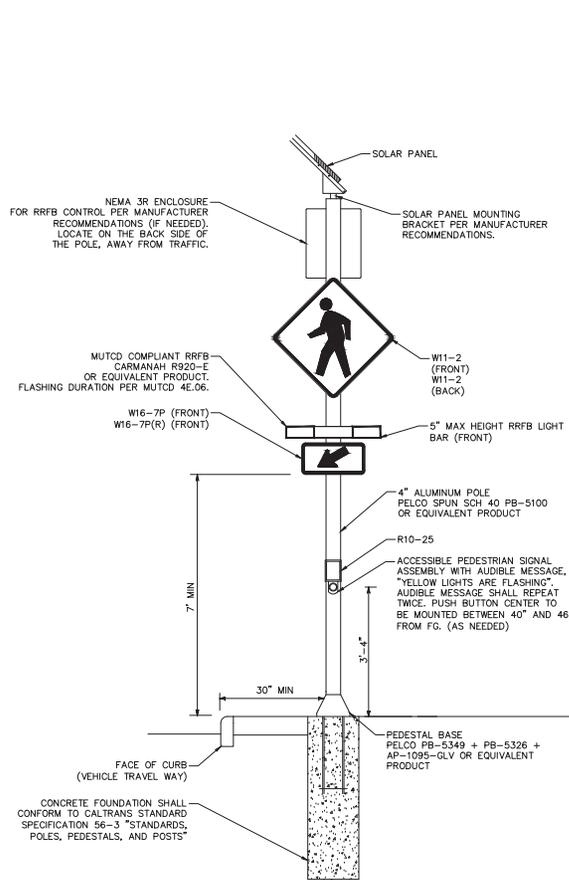
W11-2



W16-7P

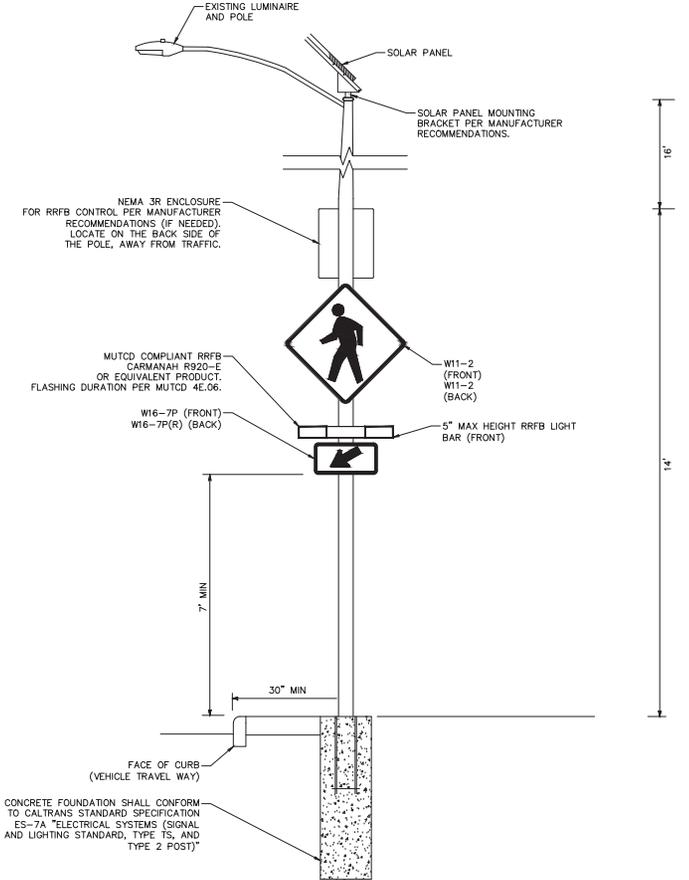


W16-7P(R)



DETAIL A
RECTANGULAR RAPID FLASHING BEACON
(RRFB) ASSEMBLY ON 1B POLE

NO SCALE



DETAIL B
EXISTING TYPE 15 POLE AND RECTANGULAR
RAPID FLASHING BEACON (RRFB) ASSEMBLY

NO SCALE

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PUBLIC WORKS DEPARTMENT

UNCONTROLLED CROSSWALKS
IMPROVEMENTS - PHASE II

RRFB DETAILS

APPROVED BY		TITLE		DWG No.
DATE	01/07/2026	DATE	TS-14	
DESIGNED BY	JTK	DATE	01/26	TRACING No. 12,444-D
DRAWN BY	AMT	DATE	01/26	
CHECKED BY	SAR	DATE	01/26	
AS BUILT BY				

SIGN LEGEND:



R10-25



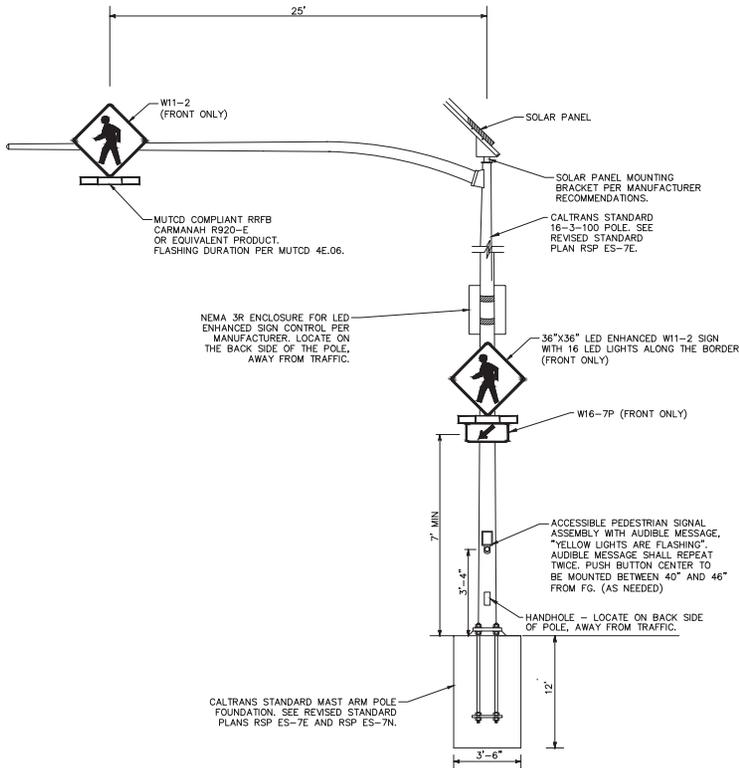
W11-2



W16-7P



W16-7P(R)



DETAIL C
RECTANGULAR RAPID FLASHING
BEACON (RRFB) ASSEMBLY ON
TYPE 18-3-100
NO SCALE

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DATE	No.	REVISION	BY

CITY OF SANTA CLARA		APPROVED BY	
PUBLIC WORKS DEPARTMENT		TITLE	
UNCONTROLLED CROSSWALKS IMPROVEMENTS - PHASE II		DATE 01/07/2026	DWG No. TS-15
DESIGNED BY JTK	DATE 01/26	TRACING No. 12,444-D	
DRAWN BY AMT	DATE 01/26		
CHECKED BY SAR	DATE 01/26		
AS BUILT BY			



Agenda Report

26-192

Agenda Date: 3/23/2026

REPORT TO BICYCLE AND PEDESTRIAN ADVISORY COMMITTEE

SUBJECT

Closing Bike Lanes, Trails, and Sidewalks (Garcia)

BACKGROUND

In July 2024, Bicycle and Pedestrian Advisory Committee (BPAC) Member Megas requested that BPAC discuss the procedure for closing sidewalks, bike lanes, and trails during various activities/events and how they are communicated to contractors and to the public (Attachment 1).

DISCUSSION

California Manual of Uniform Traffic Control Devices: The City utilizes the California Manual of Uniform Traffic Control Devices (CA MUTCD) when reviewing proposed roadway and/or trail closures. The CA MUTCD is issued by the California Department of Transportation to provide uniform standards and specifications for all official traffic control devices in California, in accordance with California Vehicle Code (CVC) 21400 and 21401. The City follows the CA MUTCD standards including Part 6, Temporary Traffic Control, which specifies standards for temporary traffic control around construction zones. The latest version of the CA MUTCD notes the following as guidance in Chapter 6F "Temporary Traffic Control Zone Signs":

Guidance:

Neither portable nor permanent sign supports should be located on sidewalks, bicycle facilities, or areas designated for pedestrians or bicyclists. Sign supports should be located to accommodate bicyclists in areas designated for their use. A minimum lateral width of 4 feet should be maintained for pedestrian pathways.

Roadway Closures: If construction blocks a bicycle lane, the CA MUTCD requires that traffic control includes signage or devices to accommodate bicycle travel.

If construction does not block a bicycle lane, staff reviews plans and permit applications to ensure traffic cones and signs do not block bicycle lanes. In early 2020, staff implemented new operating procedures to check plans and permits to ensure signs do not obstruct bicycle lanes or sidewalks. The City has the following requirement for traffic control plans (Attachment 2) used for construction activities:

Signage, channelizing devices, barriers and other equipment shall not be placed in active bike lanes or in locations that would block a bicyclist's path of travel (where possible). Where bike lanes must be closed, advanced warning signs and taper lengths shall be provided to allow bicyclists to safely merge into adjoining travel lane in advance of the bike lane closure. Bike lanes shall not be closed for construction activities unless the closure is documented and approved in a traffic control plan.

If construction impacts a City trail, the City will close the trail with barricades and signage at each entry point where the trail is closed for construction. A detour plan will be developed, and detour signage will be installed throughout the detour to guide bicyclists around the trail closure. A notification showing the detour is also placed at entry points where the trail access is closed. Additionally, advanced signage before the detour (typically at other trail entry points) is often utilized to provide advanced notice to trail users.

Signage Placement: Signs typically cannot be narrowed because the CA MUTCD requires a minimum size for visibility. A typical sign used for construction (i.e. "ROAD WORK AHEAD") must be a minimum of 30-inches by 30-inches and is recommended to be 36-inches by 36-inches.

The CA MUTCD requires signs to be placed at specific distances based on the speed limit of a roadway. If located within these distances, signs are encouraged to be installed on existing streetlights and posts.

Plan/Permit Review and Inspection: After approval of the plans or permits that result in lane (vehicle or bicycle) closures, a DPW inspector is assigned to inspect the project and work with the contractor to ensure permit compliance.

In summary, the City currently follows State standards to close bicycle lanes, and checks plans and permits and inspects projects to minimize signs obstructing bicycle lanes.

If members of the public find a sidewalk or bicycle facility blocked by construction signage or if there is a detour issue, they can report it to the MySantaClara App or submit an online request at SantaClaraCA.gov/mysantaclara for inspection and code enforcement evaluation.

Written by: Ralph Garcia, Senior Civil Engineer, Public Works

Reviewed by: Carol Shariat, Acting Transportation Manager, Public Works

Approved by: Michael Liw, Assistant Director/City Engineer, Public Works

ATTACHMENTS

1. Annual Work Plan Topic Request Form (Megas)
2. Standard Operating Procedure for Right Lane and Bike Lane Closures



**City of
Santa Clara**
The Center of What's Possible

DEPARTMENT OF PUBLIC WORKS - TRAFFIC ENGINEERING DIVISION

1500 Warburton Ave, Santa Clara, CA 95050

Tel: 408.615.3000 Fax: 408-983-0931 Email: engineering@santaclaraca.gov

Bicycle and Pedestrian Advisory Committee Annual Work Plan Topic Request Form

Requestor Name:

Betsy Megas

Date:

7/26/24

Proposed Topic Title:

Closing bike lanes, trails, and sidewalks

Background and Discussion: Please provide justification regarding how this topic pertains to the Bicycle and Pedestrian Advisory Committee's roles and responsibilities:

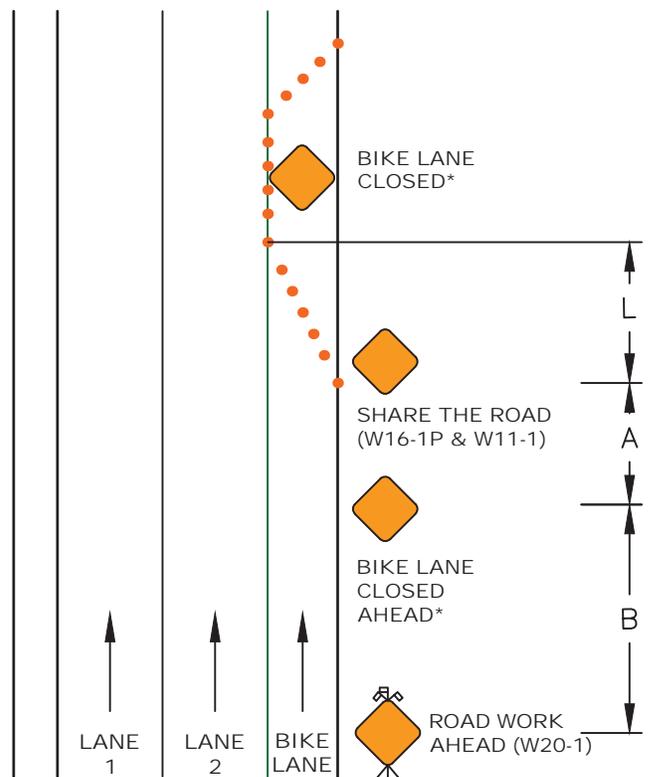
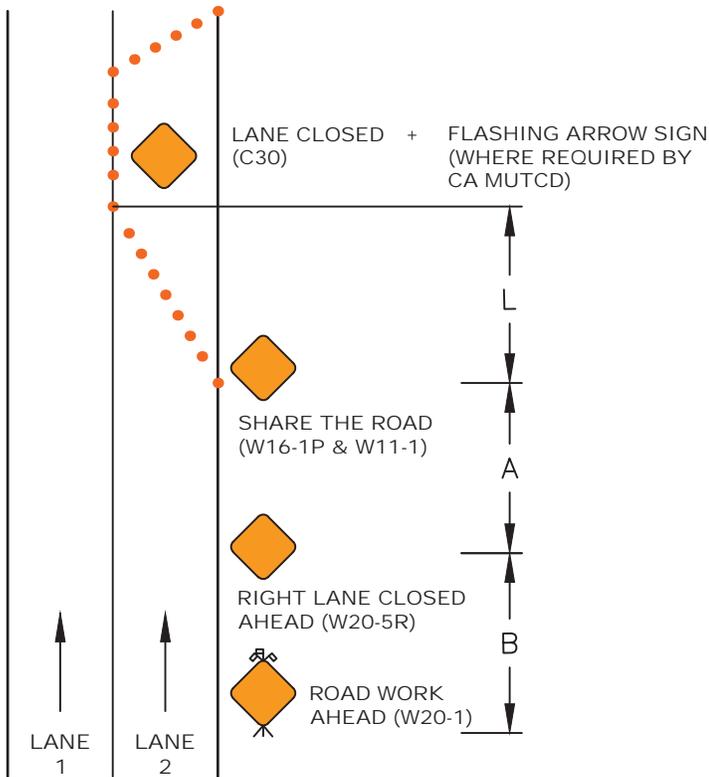
We work hard to get approval and find funding build bike lanes and trails, so it is frustrating and unsafe to find them blocked or closed. Construction and event signage often ends up in the bike lane, forcing bicyclists onto the sidewalk (technically illegal) or into lanes with fast-moving car traffic.

During the closure of the San Tomas Aquino Creek Trail in September 2023, there was poor communication of plans in advance, and details were missed, leaving a trail entry unintentionally open between the two ends of the closure, among other things.

I'd like to talk about standards for closing sidewalks, bike lanes, and trails and how they are communicated to construction contractors, ideally before the planned pavement work on the north end of the San Tomas Aquino Creek Trail.

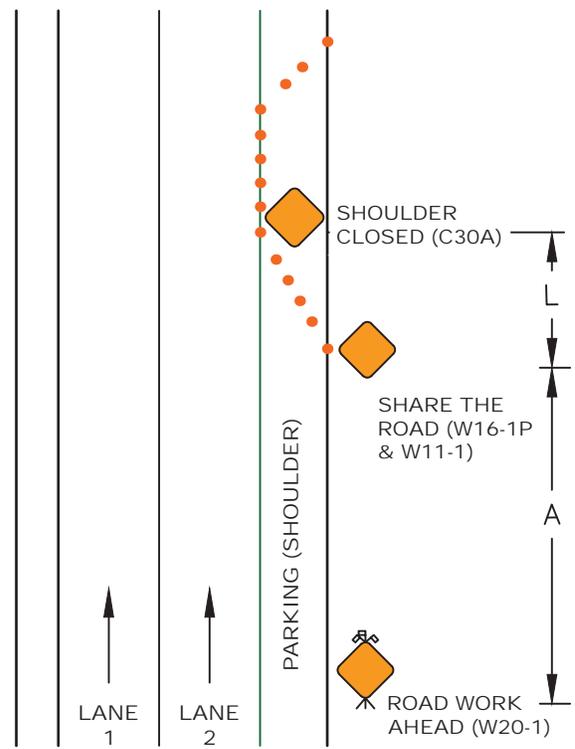
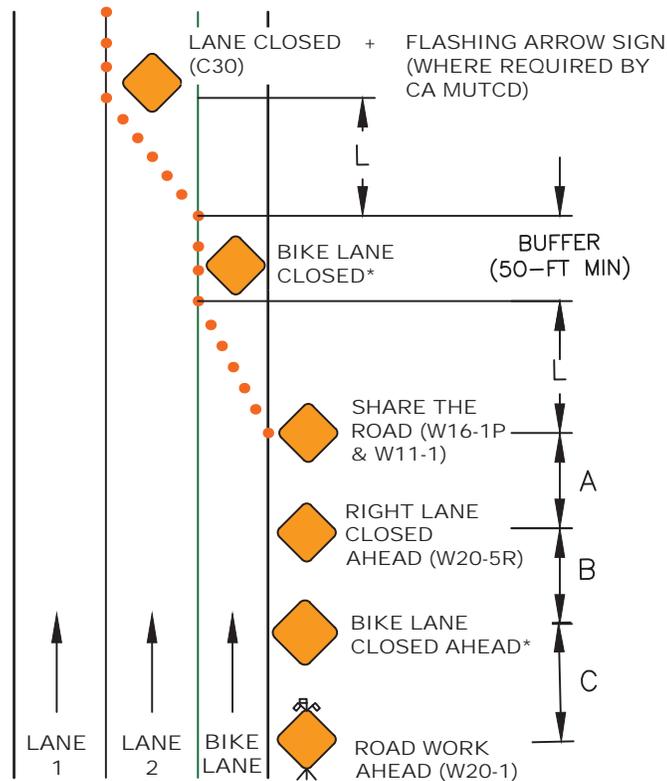
TYPICAL ROAD LANE CLOSURE

TYPICAL BIKE LANE CLOSURE



TYPICAL RIGHT LANE AND BIKE LANE CLOSURE

TYPICAL SHOULDER CLOSURE



NOTE: REFER TO ADDITIONAL NOTES ON THE REVERSE SIDE OF THIS STANDARD FOR MORE INFORMATION.

*REFER TO NOTE 6 FOR MODIFIED BIKE LANE SIGN(S).



DRAWN BY: N. DEEN
 CHECKED BY: C. SHARIAT
 APPROVED BY: M. LIW
 DATE: JAN 2020

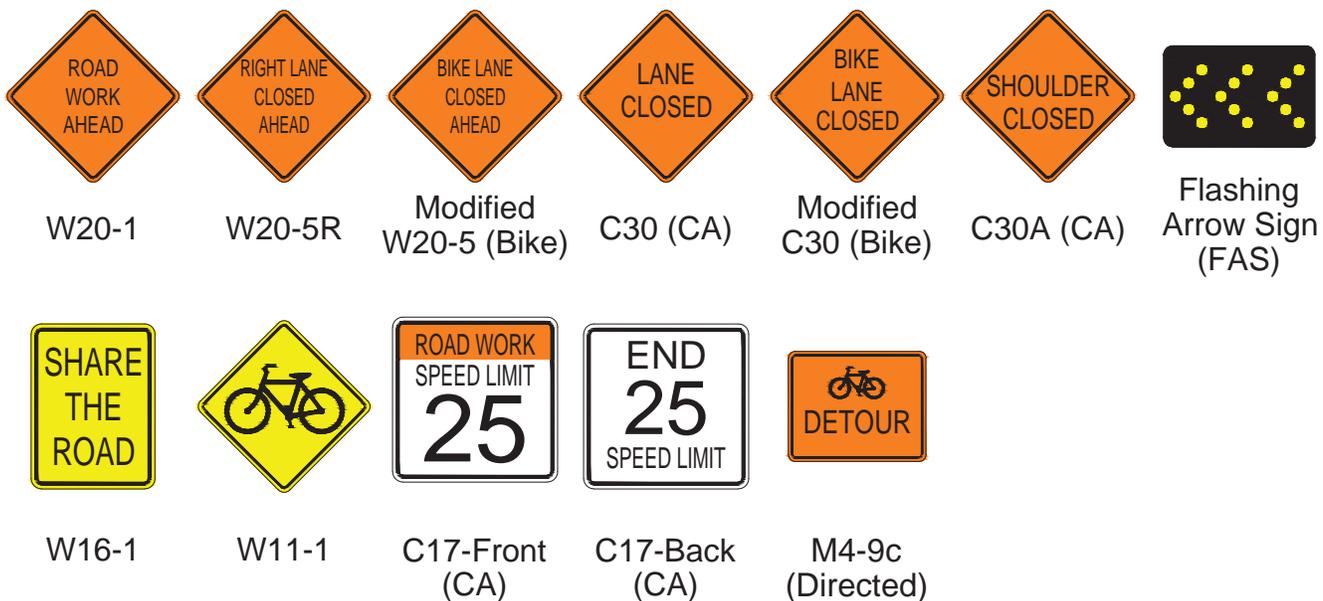
STANDARD OPERATING PROCEDURE FOR RIGHT LANE AND BIKE LANE CLOSURES

K:\Engineering\16-TED\Projects\Complete Streets\Complete Streets Forms\Measure B 3E\Standard Operating Bike Lane Closures (01-24-2020).dwg

NOTES:

1. REFER TO MOST RECENT CALIFORNIA MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (CA MUTCD) FOR THE RECOMMENDED ADVANCED WARNING SIGN SPACING (A, B, C) AND FORMULAS FOR DETERMINING TAPER LENGTH (L). TRAFFIC CONTROL PLAN SHOULD INDICATE TIME DURATION OF ALL BIKE LANE CLOSURES.
2. SIGNAGE, CHANNELIZING DEVICES, BARRIERS AND OTHER EQUIPMENT SHALL NOT BE PLACED IN ACTIVE BIKE LANES OR IN LOCATIONS THAT WOULD BLOCK BICYCLIST'S PATH OF TRAVEL (WHERE POSSIBLE). WHERE BIKE LANES MUST BE CLOSED, ADVANCED WARNING SIGNS AND TAPER LENGTHS SHALL BE PROVIDED TO ALLOW BICYCLISTS TO SAFELY MERGE INTO ADJOINING TRAVEL LANE IN ADVANCE OF THE BIKE LANE CLOSURE. BIKE LANES SHALL NOT BE CLOSED FOR CONSTRUCTION ACTIVITIES UNLESS THE CLOSURES IS DOCUMENTED AND APPROVED IN A TRAFFIC CONTROL PLAN.
3. ADVANCED WARNING SIGNS PLACEMENT SHOULD BE EVALUATED IN THE FOLLOWING ORDER:
 [OPTION 1: LANDSCAPING] PLACE SIGNS IN PUBLIC LANDSCAPING STRIP IF AVAILABLE
 [OPTION 2: PARKING] PLACE SIGNS IN PUBLIC PARKING OR SHOULDER LANE IF AVAILABLE
 [OPTION 3: SIDEWALK] PLACE SIGNS ON SIDEWALK ONLY WHEN 4' MIN PEDESTRIAN ACCESS IS MAINTAINED FOR ADA COMPLIANCE.
 [OPTION 4: OTHER] ANY LOCATION THAT ALLOWS 10' TRAVEL LANE AND 4' MIN PEDESTRIAN PATH.
 NOTE: SIGNS SHOULD BE LOCATED ON THE RIGHT-HAND SIDE OF THE ROADWAY WHERE EASILY RECOGNIZED AND UNDERSTOOD BY ROAD USERS. TEMPORARY TRAFFIC CONTROL SIGNS SHOULD NOT INTERFERE WITH VEHICULAR TRAFFIC, BICYCLE TRAFFIC, OR PEDESTRIAN MOVEMENTS (WHERE POSSIBLE).
4. LANE CLOSURE SHOULD BE EVALUATED IN THE FOLLOWING ORDER:
 [OPTION 1: NARROW BIKE LANE] WHERE POSSIBLE, MAINTAIN 4' MIN BICYCLE PATH FOR BICYCLISTS BY PLACING SIGNS IN THE BUFFER ZONE, OR IF BICYCLE LANE IS WIDE ENOUGH TO ACCOMMODATE.
 [OPTION 2: NARROW VEH LANE] NARROW ADJOINING TRAVEL LANE (10' MIN) AND PROVIDE 5' MIN TEMPORARY BIKE LANE.
 [OPTION 3: CLOSE VEH LANE] CLOSE THE ADJOINING TRAVEL LANE AND PROVIDING 5' MIN TEMPORARY BIKE LANE.
 [OPTION 4: DETOUR] DETOUR BICYCLE ACCESS, WITH ADEQUATE SPACE FOR BICYCLISTS AND APPROPRIATE DETOUR SIGNS INSTALLED, TO ALLOW BICYCLISTS TO BE ADEQUATELY DETOURED AROUND THE WORK ZONE. THE DETOUR ROUTE SHALL MINIMIZE OUT-OF-DIRECTION TRAVEL DISTANCE AND SIGNAGE SHOULD NOT BLOCK BICYCLE TRAVEL WAY.
4. TRAFFIC CONTROL DEVICES AND SIGNS SHALL NOT BE LEFT IN PLACE OUTSIDE OF WORK HOURS OR FOR LONGTERM DURATIONS WITHOUT CITY TRAFFIC ENGINEER APPROVAL.
5. TRAFFIC CONTROL PLANS THAT INCLUDE BIKE LANE CLOSURES ON HIGH SPEED ROADWAYS SHALL HAVE SPEED LIMITS REDUCED TO 30 MPH OR LESS.
6. MODIFIED W20-5 (BIKE) AND C30 (BIKE) SIGNS TO BE PROVIDED BY THE APPLICANT. SIGNS MAY BE SUBSTITUTED BY COMBINING W20-5 AND C30 (CA) WITH W11-1.

APPLICABLE ADVANCED WARNING SIGNS (REFER TO RECENT CA MUTCD FOR RECOMMENDED ADVANCED WARNING SIGNS):



DRAWN BY: N. DEEN
 CHECKED BY: C. SHARIAT
 APPROVED BY: M. LIW
 DATE: JAN 2020

**STANDARD OPERATING
 ADDITIONAL NOTES**

CITY OF SANTA CLARA

PAGE: 2



Agenda Report

26-194

Agenda Date: 3/23/2026

REPORT TO BICYCLE AND PEDESTRIAN ADVISORY COMMITTEE

SUBJECT

Trial Runs of New Bike and Pedestrian Infrastructure (Shariat)

BACKGROUND

In July 2023, resident Jane Casamajor requested that the Bicycle and Pedestrian Advisory Committee (BPAC) investigate the possibility of conducting a trial run of any new bicycle and pedestrian infrastructure as part of the approval process (Attachment 1).

Former BPAC Member Casamajor was interested in the type of project referred to as quick build. Quick build is a method of building bike and pedestrian safety improvements that can be temporary and built at a lower cost such as bike lanes, curb extensions, pedestrian crossings, or traffic calming measures. These projects are implemented to respond quickly to safety concerns or to conduct a pilot project to test out a bicycle or pedestrian design concept. These pilot projects can then be evaluated on their performance through feedback and collection of real-world traffic data. Based on the findings, design concepts can then be modified before more permanent infrastructure is installed. However, these quick build projects can also remain indefinitely, without the need to install more permanent infrastructure.

DISCUSSION

The City of Santa Clara has completed quick build projects in the past to address safety concerns raised by the community. In 2023, the City installed quick build plastic vertical delineators on portions of Monroe Street generally between South Drive/Marmon Court and San Juan Avenue to address safety concerns related to speeding and the existing roadway curve near Francis Avenue.

In addition, a portion of the Lafayette Street Class IV Bikeway and Pavement Rehabilitation Project (located between Laurelwood and Reed Street) is being completed as a pilot project. This project was brought to the BPAC for review and discussion in January 2023. The project calls for on-street parking removal on the west side of Lafayette Street between Memorex Drive and Parker Street. However, City Council directed staff to build the bike lane improvements as temporary improvements where parking will be removed. This temporary project will remain in effect for a year and will be brought back to Council to provide results of a required parking study in the adjacent area. At that time, Council can then direct staff to make this bike project permanent.

Finally, there is a funded Capital Improvement Program (CIP) project titled "Quick Build Bike and Pedestrian Safety Project." This project will be used to complete several quick build projects within Santa Clara based on identified need and to address community concerns.

Written by: Carol Shariat, Interim Transportation Manager, Public Works

Approved by: Michael Liw, Assistant Director/City Engineer, Public Works

ATTACHMENTS

1. Annual Work Plan Topic Request Form (Casamajor)



Bicycle and Pedestrian Advisory Committee Annual Work Plan Topic Request Form

Requestor Name:

Jane Casamajor

Date:

7/10/23

Proposed Topic Title:

Trial runs of new bike and pedestrian infrastructure

Background and Discussion: Please provide justification regarding how this topic pertains to the Bicycle and Pedestrian Advisory Committee's roles and responsibilities:

I would like to investigate the possibility of adding a trial run of any significant new bicycle and pedestrian infrastructure as part of the approval process. I understand that this is a labor intensive process and will require volunteers to make happen. However it seems to be a powerful tool both for gaining public support and for finding flaws that do not appear on renderings.



City of Santa Clara

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Agenda Report

26-181

Agenda Date: 3/23/2026

REPORT TO BICYCLE AND PEDESTRIAN ADVISORY COMMITTEE

SUBJECT

Valley Transportation Authority Bicycle and Pedestrian Advisory Committee Meetings Update (Megas)

DISCUSSION

Bicycle and Pedestrian Advisory Committee (BPAC) Chair Megas will provide a verbal update regarding recent Valley Transportation Authority Bicycle and Pedestrian Advisory Committee (VTA BPAC) meetings.

Written by: Ralph Garcia, Senior Civil Engineer, Public Works

Reviewed by: Carol Shariat, Acting Transportation Manager, Public Works

Approved by: Michael Liw, Assistant Director/City Engineer, Public Works

ATTACHMENT

1. Email from Chair Megas dated March 6, 2026
2. Santa Clara County Central Expressway Presentation



Re: March BPAC Documents_Due March 6th s

From: Betty Mega <dvortygirl@gmail.com>

Date: Fri 3/6/2026 11:40 PM

To: Carol Shariat <CShariat@SantaClaraCA.gov>

Hi Carol,

Could you please attach the Central Expressway presentation to this item in the packet? I think it might be of particular interest to the BPAC.

The VTA BPAC met on February 11 and will meet again on March 11. The full agenda packet for February is available on the VTA portal here:

<http://antaclaravta.igmp2.com/Citizen/FileOpen.aspx?Type=1&ID=4313&Inline=True> s

February

- We got an information item on the VTA SmartCommute app, designed to incentivize travel by transit, carpool, and active transportation with prize, monthly challenge, leaderboard, and even small payment. I encourage you all to try it and share your feedback: <http://www.vta.gov/smartcommute>
- We heard from Ben Aghegnehu of County Road that the County of Santa Clara's Circulation and Mobility Update, originally from 1995, is being updated. They are in the outreach phase right now. County Road maintains and operates the expressway, along with the roadway network in unincorporated areas in Santa Clara County. Please learn more, complete the survey, and find out where to visit a pop-up event here: <http://road.antaclaracounty.gov/circulation-and-mobility-element>
- We heard an update on the VTA's Strategic Plan.
- We heard an update from County Road about the pavement rehabilitation planned on Central Expressway between Bower and Scott in Santa Clara.
 - The presentation on this item is here: <http://antaclaravta.igmp2.com/Citizen/FileOpen.aspx?Type=4&ID=14329&MeetingID=4541>
- We reviewed the MTC Complete Street Checklist for VTA's Transit Reliability Improvement and Performance System (TRIPS). Basically, a county-scale transit signal priority project.

Take care,

Betty Mega s



ROADS & AIRPORTS DEPARTMENT



Central Expressway Pavement Rehabilitation Project
(Bowers Ave to Scott Blvd)





Project Limits
Bowers to Scott

NO SCALE

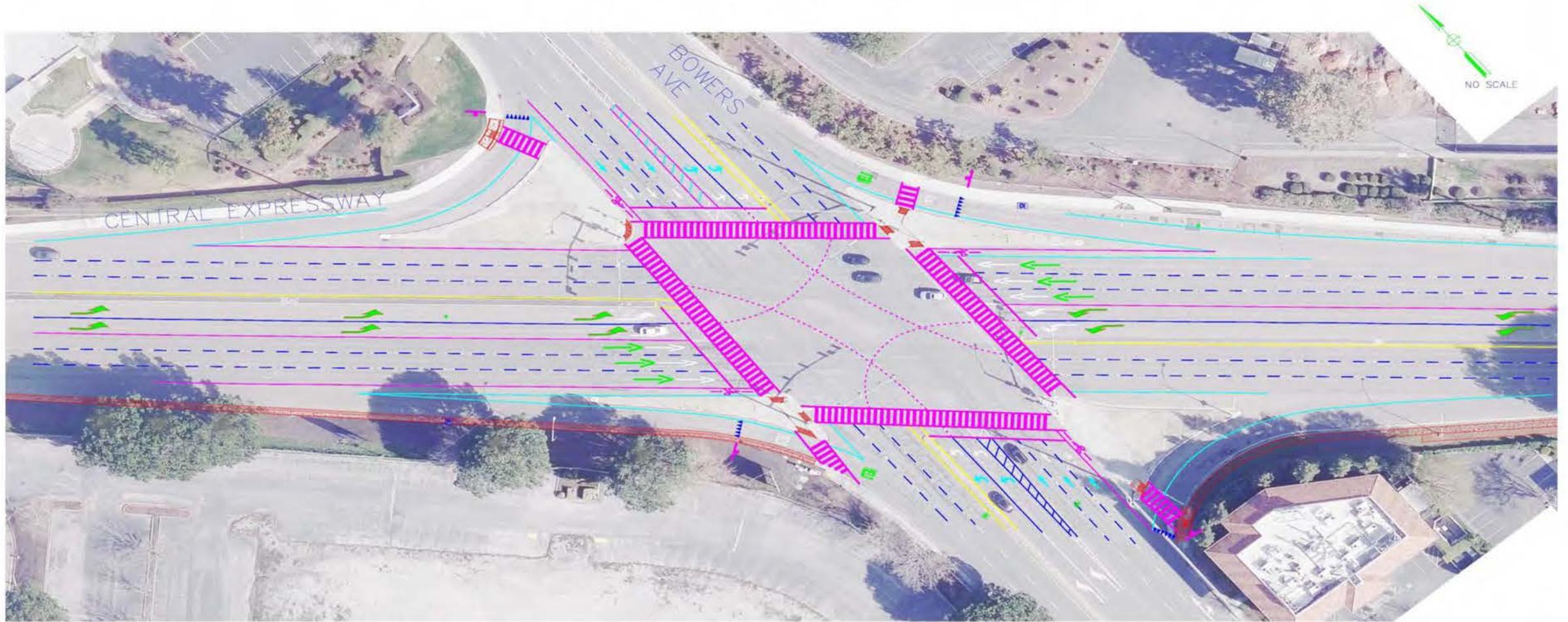
SCOPE OF WORK

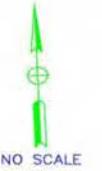
- * 6" partial depth recycling Bowers to Scott*
- *2" hot mix asphalt overlay Bowers to Scott*
- *Upgrading existing pedestrian facilities with current ADA standards, sidewalk, curb & gutters, etc.*
- *Installing new sidewalk between Bowers and Northwestern Pkwy*
- *Installing new sidewalk underneath Central Expwy, along San Tomas Expwy*
- *Squaring of a corner at NW corner of Scott Blvd*
- *2" mill and pave hot mix asphalt on Owen St and Connector roads to San Tomas Expwy*
- *Removing all existing guardrails and installing new Midwest guardrail system with current standards*

SCOPE OF WORK (Cont'd)

- *Installing new bioretention basin at NW corner of Scott Blvd*
- *All damaged ADA ramps, sidewalks, curb & gutters will be upgraded to current standard*
- *All signs will be removed and replaced with new*
- *Installing new stripes and markers throughout the project limits*
- * Electrical upgrades at Bowers and Scott intersections*
 - *Traffic signal improvements*
 - *Touchless pedestrian push buttons*
 - *Pedestrian sensors*
 - *Replacing new vehicular & bicycle detector loops*
- * Crackseal existing pavement from Wolfe to Bowers and Scott to De La Cruz (Completed)*

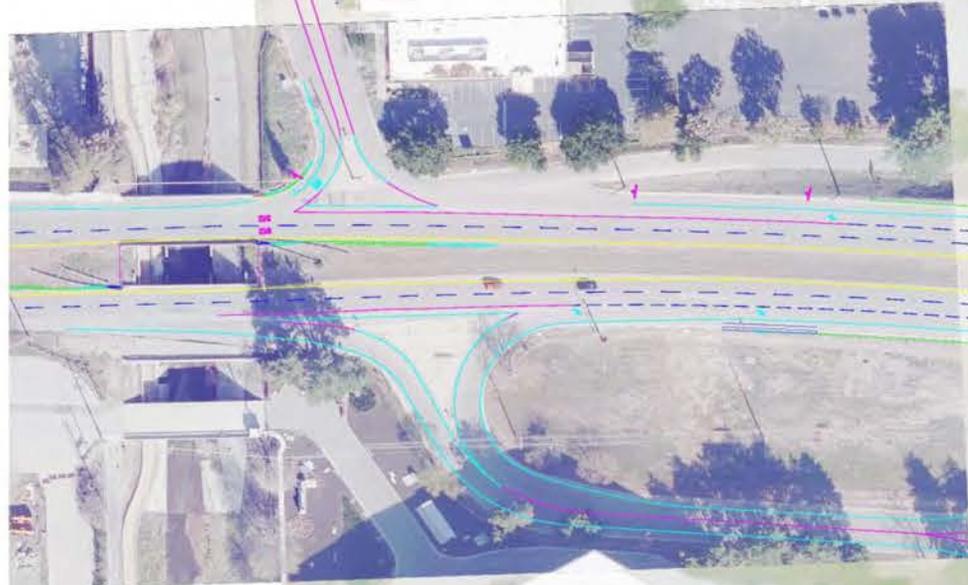
BOWERS AVE INTERSECTION IMPROVEMENTS







NO SCALE



OWEN ST

OWEN ST

OWEN ST

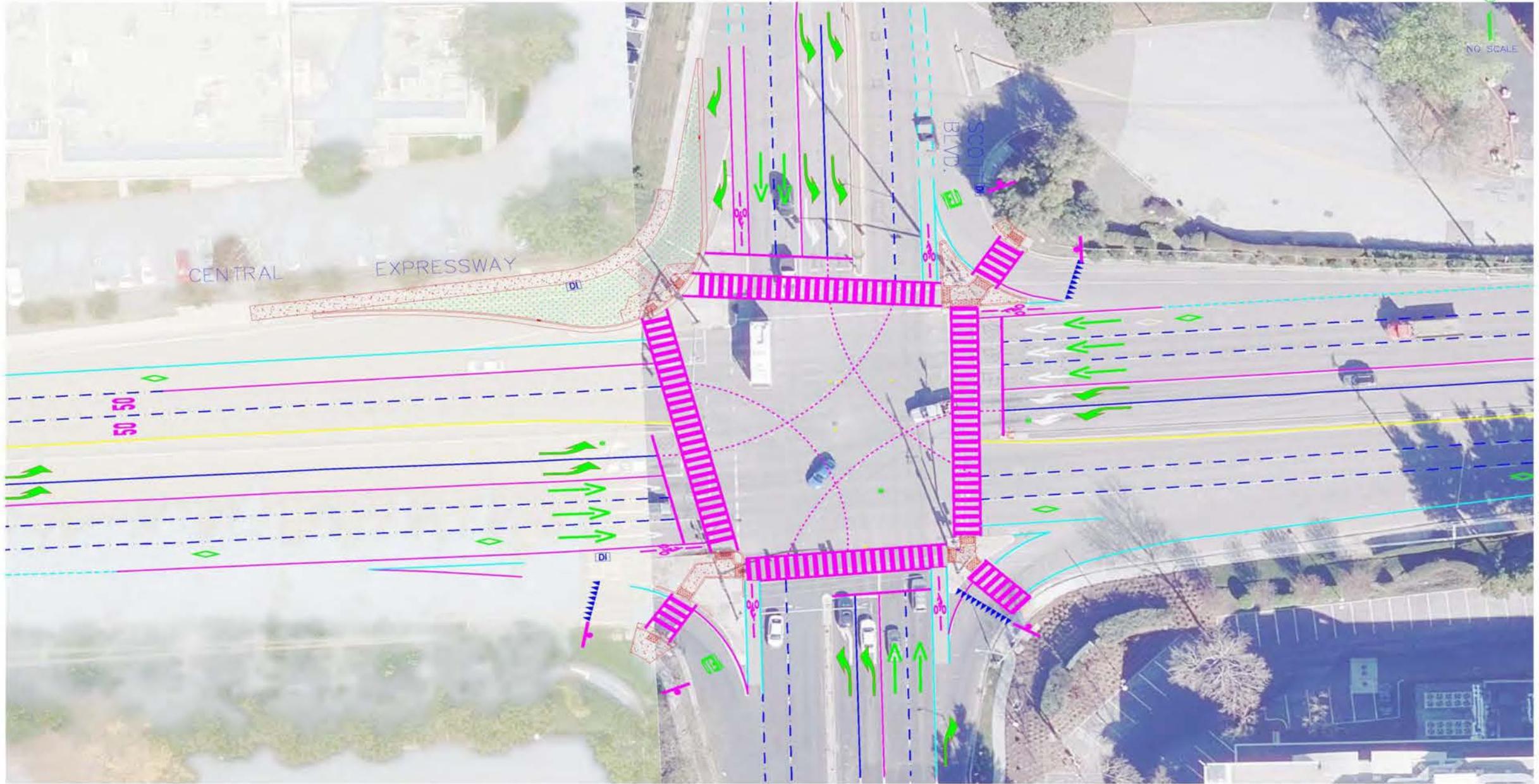
CENTRAL

EXPRESSWAY

LAWSON
LANE

SAN TOMAS
EXPRESSWAY

SCOTT BLVD INTERSECTION IMPROVEMENTS





City of Santa Clara

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Agenda Report

26-182

Agenda Date: 3/23/2026

REPORT TO BICYCLE AND PEDESTRIAN ADVISORY COMMITTEE

SUBJECT

Santa Clara Station Area Task Force Update (Shure)

DISCUSSION

Bicycle and Pedestrian Advisory Committee (BPAC) Member Shure will provide an update regarding recent Santa Clara Station Area Task Force meetings.

Written by: Ralph Garcia, Senior Civil Engineer, Public Works

Reviewed by: Carol Shariat, Acting Transportation Manager, Public Works

Approved by: Michael Liw, Assistant Director/City Engineer, Public Works



Agenda Report

26-183

Agenda Date: 3/23/2026

REPORT TO BICYCLE AND PEDESTRIAN ADVISORY COMMITTEE

SUBJECT

Ad-hoc Subcommittee Reports (Megas)

BACKGROUND

This agenda item gives current BPAC subcommittees an opportunity to provide an update on the progress and findings of the subcommittee.

BPAC subcommittees shall be considered ad-hoc committees.

An ad-hoc committee is a committee that is (1) advisory; and (2) composed solely of members of the legislative body and are less than a quorum. To ensure that the subcommittees supporting BPAC's work qualify as ad-hoc committees, the subcommittees should:

1. Be comprised solely of BPAC members;
2. Consist of less than a quorum;
3. Have a defined purpose and timeframe to accomplish said purpose;
4. Be advisory.

At the October 27, 2025 meeting, BPAC dissolved the previous subcommittees and created the ad-hoc Wayfinding Subcommittee.

DISCUSSION

The current BPAC ad-hoc subcommittees may provide an update to the BPAC:

- Wayfinding Subcommittee - Members Haro, Kratz, and Marinaro

The City Attorney's Office advised that all ad-hoc subcommittee progress reports should be consolidated into a single staff report rather than separate individual reports. This is noted for BPAC as it represents a change from the previous agenda format. This format change shortens BPAC agendas, reduces staff workload, and still allows all subcommittees to report on their progress.

Written by: Ralph Garcia, Senior Civil Engineer, Public Works

Reviewed by: Carol Shariat, Acting Transportation Manager, Public Works

Approved by: Michael Liw, Assistant Director/City Engineer, Public Works

ATTACHMENT

1. BPAC Wayfinding Subcommittee, minutes of meeting 3-5-2026
2. CSC BPAC Wayfinding Subcommittee-proposed routes

Meeting Minutes--City of Santa Clara BPAC Subcommittee on Wayfinding

Location: Pomeroy Green Cooperative clubhouse

Date: March 5, 2026

Time: 5 p.m. to 8:00 p.m.

Attendance:

Jonathan Marinaro (brought laptop with files for use at meeting)

Jorge Haro

Ken Kratz (recording secretary; provided bicycle map, plastic overlay, & pens)

Agenda Items:

Re-review of routes and designation of routes numbers.

Actions:

Re- reviewed proposed routes and end destinations. Selected primary routes for early implementation and proposed secondary routes for future consideration.

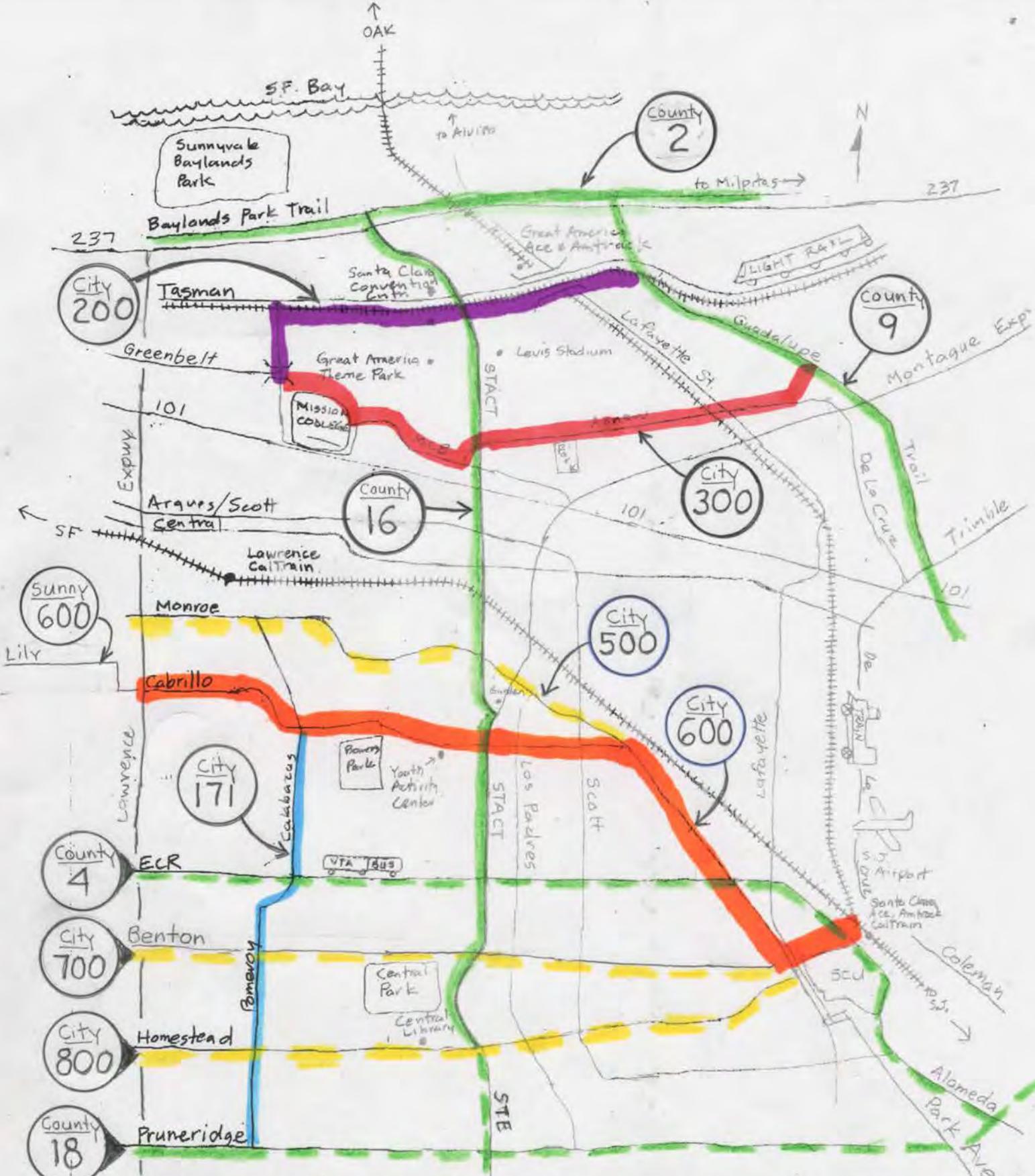
Reviewed other jurisdiction's routes that touch our city limits (Sunnyvale, route #600) or go through City (County, routes #2, #4, #9, & #18).

Reviewed and applied Santa Clara County's route numbering guidelines to proposed routes in City.

Mapped routes including route numbers for all primary and secondary routes.

Other Notes: None

Next Meeting: To be determined. Need to work on sign contents and placement for signs to be installed along the routes.



PROPOSED CSC WAYFINDING ROUTES

LEGEND:

- █ County
- █ City
- █ County
- █ City
- █ County
- █ City

- Existing/County
- Proposed/City
- Proposed/County
- Future/City

Route Designation Jurisdiction

BPAC Subcommittee
 date: 3/5/26
 dwg: Kratz
 1
 dwg# 1